



## Memorandum #2: Existing Conditions (FINAL)

Forest Grove Access Management Strategy  
OR-8 (TV Highway) from OR-47 to City Limits

December 3, 2025







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## Acronyms and Abbreviations

APM	ODOT Analysis Procedures Manual
ARTS	ODOT All Roads Transportation Safety Program
CRF	Crash Reduction Factor
EB	Eastbound
HDM	ODOT Highway Design Manual
NHS	National Highway System
NNN	National Network (Federal Designated Truck Route)
ODOT	Oregon Department of Transportation
OHP	Oregon Highway Plan
RRR	Reduction Review Route
SPIS	Safety Priority Index System
TSP	Transportation System Plan
WB	Westbound



# 1 Purpose

This memorandum summarizes the existing transportation system conditions of the segment of Oregon Route 8 (OR-8), also known as TV (Tualatin Valley) Highway, and Pacific Avenue, in Forest Grove between OR-47 / Quince Street and S 1<sup>st</sup> Avenue / city limits. This summary is based on field observations and data provided by the City and the Oregon Department of Transportation (ODOT). The information within this memorandum provides a baseline understanding of existing conditions of the corridor, access points within the corridor, and safety conditions to help inform project recommendations.

## 2 Existing Conditions

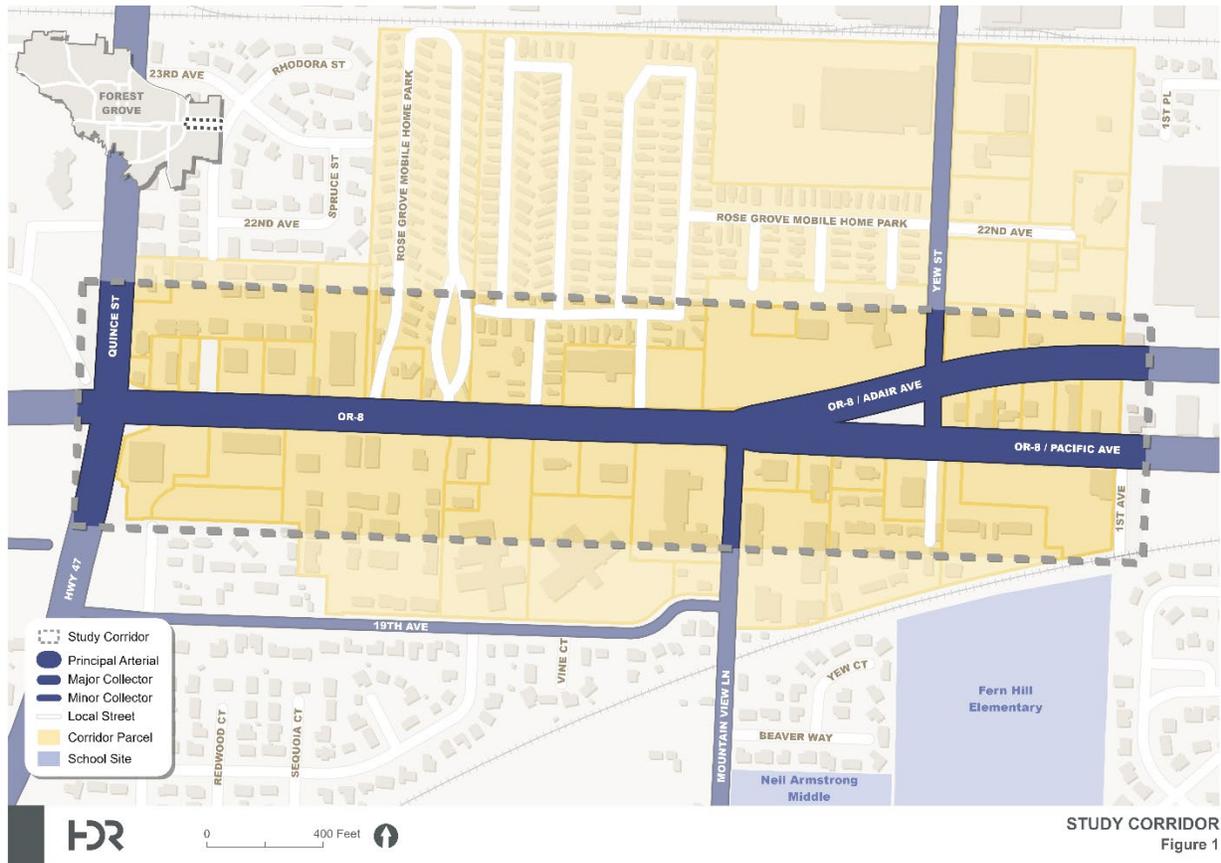
### 2.1 Study Area

OR-8 is a major east-west highway that connects OR-6 west of Gales Creek with US-26 just west of Portland (Figure 1). It traverses central Forest Grove before connecting to Cornelius just east of the study area. The study area is approximately two-thirds of a mile long and includes a 5-lane segment of OR-8 (Pacific Avenue) from OR-47 / Quince Street to Mountain View Lane and the section of OR-8 between Mountain View Lane and S 1<sup>st</sup> Avenue / city limits which consists of a one-way couplet with Adair Avenue carrying westbound traffic and Pacific Avenue eastbound traffic (Figure 2).

**Figure 1. OR-8 Near Rose Grove Mobile Home Community/First Community Credit Union, Looking East from North Sidewalk**



Figure 2. Study Corridor



Source: ODOT, ODOT TransGIS

### 2.1.1 Roadway Classification

OR-8 through the study corridor and OR-47 / Quince Street are classified federally as Principal Arterials, regionally as Major Arterials, and locally as Arterials. The Oregon Highway Plan (OHP) classifies OR 8 within the project area as a Statewide Highway. Major Collectors include Yew Street and Mountain View Lane. 19<sup>th</sup> Avenue, which is a parallel route to OR-8, is classified as a Minor Collector. These segments and relevant adjacent segments are shown in greater detail in Table 1.

Table 1. Study Corridor Roadway Characteristics

Street	Federal Functional Class.	Number of Lanes	Posted Speed	AADT (2023)
OR 8. (OR-47 / Quince St. – Mountain View Ln.)	Principal Arterial	5	40	34,888
Pacific Ave. (Mountain View Ln. – Yew St.)	Principal Arterial	2	40	16,228
Pacific Ave. (Yew St. – S 1 <sup>st</sup> Ave.)	Principal Arterial	2	40	15,873
Adair Ave. (Mountain View Ln. – Yew St.)	Principal Arterial	2	40	17,517
Adair Ave. (Yew St. – S 1 <sup>st</sup> Ave.)	Principal Arterial	2	40	17,870

Street	Federal Functional Class.	Number of Lanes	Posted Speed	AADT (2023)
OR-47 (south of OR-8)	Principal Arterial	3	45	15,202
OR-47 / Quince St. (north of OR-8)	Principal Arterial	3	25	15,845
Mountain View Ln.	Major Collector	2	20	3,309
Yew St. (Pacific Ave. to Adair Ave.)	Major Collector	2	25	-
19 <sup>th</sup> Ave.	Minor Collector	2	25	-

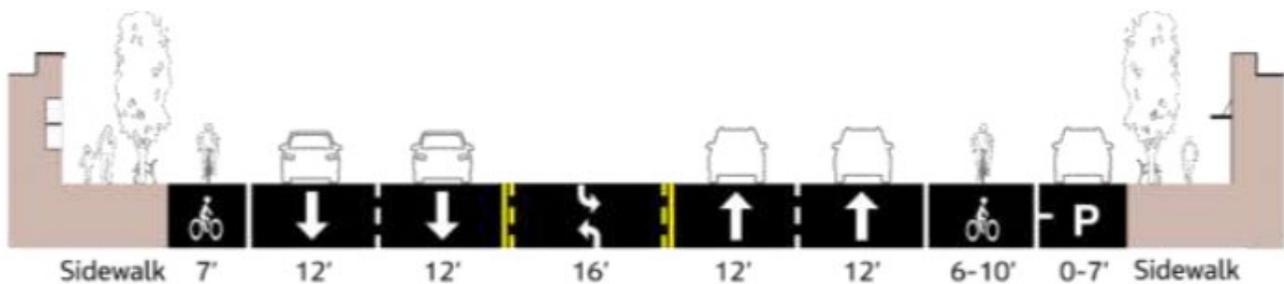
Source: ODOT, ODOT TransGIS, 2025 Google Earth  
 Note: AADT data is not available for Yew Street and 10<sup>th</sup> Avenue.

ODOT and regional agencies reviewed the Blueprint for Urban Design (BUD) Guidelines incorporated in ODOT’s Highway Design Manual and adopted land use contexts for the OR-8 (TV Highway) corridor assigning this section of TV Highway in Forest Grove a BUD land use context of Commercial Corridor. Appendix A shows the HDM Table 300-10 with Design Element Recommendations for Commercial Corridor that provides the design criteria for this context.

### 2.1.2 Roadway Configuration

Study corridor travel lanes are generally 12 feet wide with a 16-foot-wide center turn lane and 6 foot or wider bicycle lanes. The total width of the right-of-way in this section ranges from 77 to 88 feet curb to curb. Figure 3 shows the typical existing cross section for the study corridor developed for the East Forest Grove Safety Improvement Plan. Each section of the one-way couplet has two 12-foot travel lanes with six-foot bicycle lanes on the outer edge and intermittent 7- to 10-foot parking lanes along the inner edge. The total right-of-way width for each of these sections is approximately 40 feet.

Figure 3. Roadway Cross Section



Source: East Forest Grove Safety Improvement Plan, Jacobs

## 2.2 Pedestrian Network

The corridor generally has sidewalks that are at least five feet wide with recently updated ADA compliant curb ramps at the OR-8 / OR-47 / Quince Street and Yew Street / Adair Avenue intersections and marked crosswalks at signalized intersections. However, there are several sidewalk gaps and sidewalk deficiencies observed in the field that may impact the ability of pedestrians to navigate the corridor. Figure 4 shows several

locations ODOT has identified with sidewalk, curb ramp, and crossing needs. The following sections describe sidewalk, curb ramps and pedestrian crossing existing conditions in further detail.

Figure 4. Pedestrian Network



Source: ODOT, ODOT TransGIS, 2025 Google Earth

Note: Some ramp locations have been minimally adjusted to enhance visibility

### 2.2.1 Sidewalk Inventory

Sidewalks that meet the city minimum of five feet wide are present along much of OR-8 (Figure 5). However, the north side sidewalk along OR-8 west of Habitat for Humanity is only four feet wide. The HDM Table 300-10 included in Appendix A shows that a Commercial Corridor such as OR-8 should have minimum five feet-wide sidewalks with paved frontages and/or buffer zones that are at least one foot-wide for total minimum sidewalk width of six feet.

**Figure 5. Sidewalk - Pacific Avenue Looking West from South Sidewalk Near Karen's Korner**



Sidewalk gaps were identified at the following locations:

- East side of OR-47, south of OR-8
- North side of OR-8 in front of Habitat for Humanity (3731 Pacific Avenue, Figure 6)
- North and south side of Adair Avenue between Mountain View Lane and Yew Street (City project will complete construction of a sidewalk along the north side of this section anticipated to be completed by the end of this project)
- South side of Adair Avenue, east of Yew Street along Pouya Auto (Used car dealer at 4315 Pacific Avenue)
- South side of Pacific Avenue, between Yew Street and 1<sup>st</sup> Avenue: even though a sidewalk traverses private property, observations in the field indicate pedestrians are walking on the narrow roadway shoulder

Figure 6. Missing Sidewalk, OR-8 Westbound Near Habitat for Humanity



## 2.2.2 Curb Ramps

Most corners along the corridor have curb ramps (Figure 4), and curb ramps have been recently improved at the OR-8 / OR-47 / Quince Street and Yew Street / Adair Avenue intersections to meet ADA requirements. However, curb ramps remain which are considered to be in poor functional condition according to ODOT TransGIS. These ratings stem from issues such as slope, landing length, lip height, etc. Key curb ramp needs include:

### NEEDED AND MISSING RAMPS

- Adair Avenue / Yew Street: ADA ramps for east crossing have been recently updated, however, west crossing lacks ADA ramps and sidewalks.
- Pacific Avenue / Yew Street: ADA ramps at northeast corner have been recently updated, however, east crossing is missing receiving ramp in the south side of Pacific Avenue (Figure 7).

**Figure 7. Incomplete Unsignalized Pedestrian Crossing, Pacific Avenue at Yew Street, Looking West**



#### **CLOSED CROSSINGS**

The west side crossing is closed at the S 1<sup>st</sup> Avenue / Pacific Avenue intersection.

### **2.2.3 Pedestrian Crossings**

The study corridor features two signalized intersections: OR-8 / OR-47 / Quince Street, and OR-8 / Mountain View Lane. These intersections also feature signal-controlled, marked crosswalks with additional yield-controlled right turns at the OR-8 / OR-47 / Quince Street intersection. Unmarked pedestrian crossings are present along the east side of Yew Street where it crosses Pacific Avenue and Adair Avenue and the east side of S 1<sup>st</sup> Avenue crossing Pacific Avenue. ODOT's project will implement a pedestrian hybrid beacon with a median on OR-8 at the Rose Grove Mobile Home Park which is anticipated to be completed by the time of completion of this project. This new crossing will reduce the distance between pedestrian crossings in the corridor which currently is approximately 2,000 feet between OR-47 / Quince Street and Mountain View Lane crossings.

There is evidence of pedestrian travel crossing the railroad tracks south of S 1<sup>st</sup> Avenue connecting to the multi-use path leading to S Heather Street and Fern Hill Elementary School. This may be a factor in the locations children choose to cross OR-8.

## **2.3 Bicycle Network**

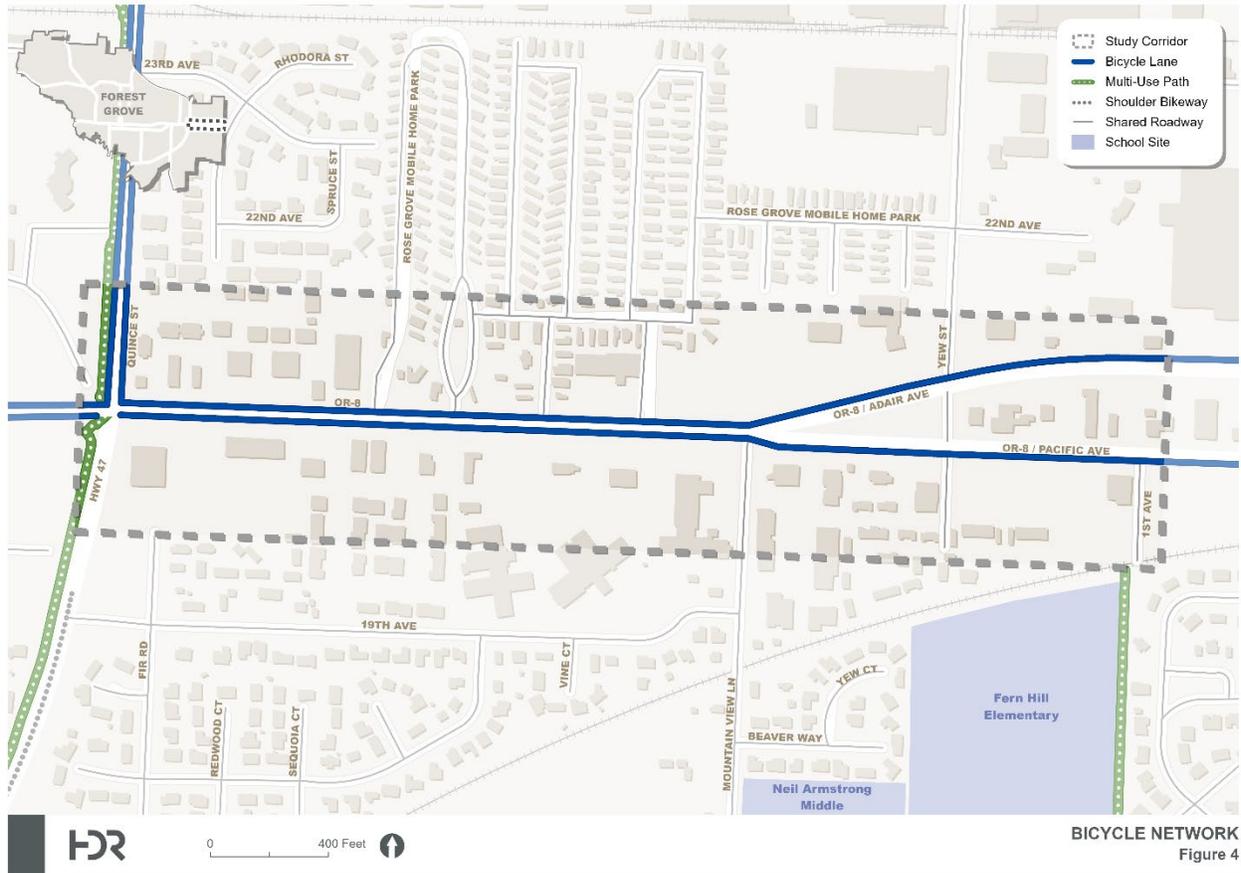
The bicycle network along and near the study corridor is largely complete with on-road facilities available on most streets (Figure 8, Figure 9). Conventional bicycle lanes are present on the outer edges of OR-8 throughout the corridor. A multi-use path is present on the west side of OR-47 north and south of OR-8. Shoulder bicycle lanes are present on the east side of OR-47 south of 10<sup>th</sup> Avenue. The majority of the remaining study corridor streets are classified as shared roadway facilities.

The East Forest Grove Safety Improvement Plan recommends wider, buffered bicycle lanes along the OR-8 corridor, although this has not been adopted into the Forest Grove Transportation System Plan (TSP).

**Figure 8. Bicycle Lane, OR-8 Looking West Near Rose Grove Mobile Home Community/Forest Grove Rehabilitation and Care Center**



Figure 9. Bicycle Network



Source: ODOT, ODOT TransGIS, Forest Grove TSP

## 2.4 Transit Network

OR-8 is the primary transit route for the City of Forest Grove (Figure 11). TriMet Bus Line 57 serves several stops along the study corridor (Figure 10). Service generally runs every 15 minutes from early morning to late evening, seven days a week. Line 57 is classified as a frequent service bus line by TriMet.

Ride Connection’s GroveLink is a community shuttle service that supplements TriMet’s Bus Line 57, covering most of the city with multiple routes. The East Loop and Employment Loop both serve the study corridor area. Service generally runs hourly on weekdays from morning to early evening. Stops within the study corridor provide connections to other service options such as Cornelius Link.

Figure 10. TriMet Bus Line 57 Stop at Yew Street



Figure 11. Transit Network



Source: TriMet, Ride Connection

## 2.5 Freight Network

OR-8 is a key freight route through Forest Grove (Figure 12). None of the study corridor sections are classified as Oregon Freight Routes in the Oregon Highway Plan (OHP), however, most major facilities are designated as part of the National Highway System, National Network, and Reduction Review Routes (Table 2). Reduction Review Routes is a term the OHP applies to protect the vertical and horizontal clearance of important freight routes. These routes and commercial and major freight parcels are shown in Figure 13. Commercial properties are generally more likely to be served by trucks and major freight parcels are those parcels with a freight-related land use code that are over 100,000 square feet.

Figure 12. Freight Traffic at OR-8 / OR-47 Intersection, Looking South from Quince Street



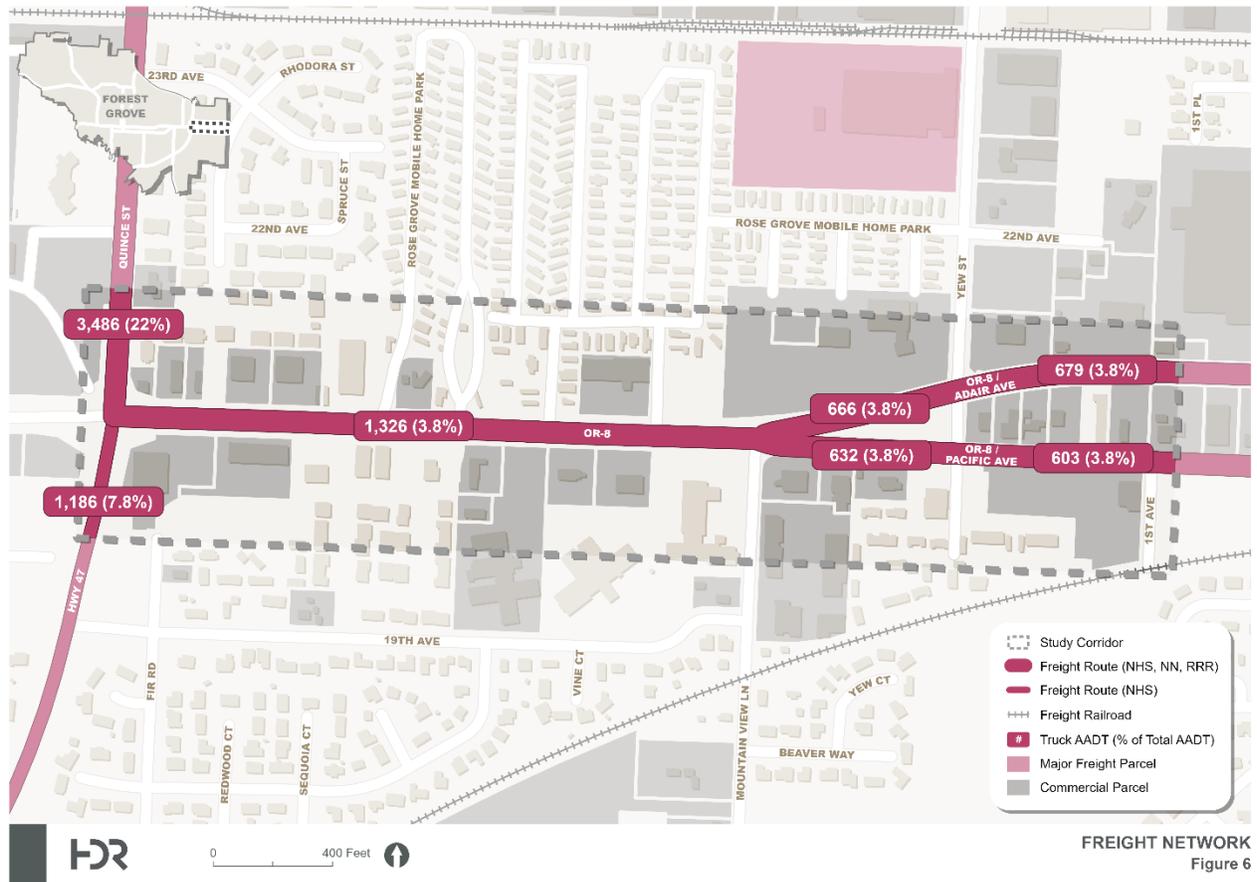
Table 2. Freight Summary

Section	Designations	Average Annual Daily Traffic (AADT)	Average Annual Daily Truck Traffic	Percent of All Traffic (AADT)
Pacific Ave. (OR-47 / Quince St. – Mountain View Ln.)	NHS, NN, RRR	34,888	1,326	3.8%
Pacific Ave. (Mountain View Ln. – Yew St.)	NHS, NN, RRR	16,628	632	3.8%
Pacific Ave. (Yew St. – S 1 <sup>st</sup> Ave.)	NHS, NN, RRR	15,873	603	3.8%
Adair Ave. (Mountain View Ln. – Yew St.)	NHS, NN, RRR	17,517	666	3.8%
Adair Ave. (Yew St. – S 1 <sup>st</sup> Ave.)	NHS, NN, RRR	17,870	679	3.8%
OR-47 (south of OR-8)	NHS	15,202	1,186	7.8%
OR-47 / Quince St. (north of OR-8)	NHS, NN, RRR	15,845	3,486	22.0%

Source: ODOT TransGIS, Oregon Highway Plan

KEY: National Highway System, NN - National Network (Federally Designated Truck Route), RRR – Reduction Review Route.

Figure 13. Freight Network



Source: ODOT TransGIS, Oregon Highway Plan

KEY: National Highway System, NN - National Network (Federally Designated Truck Route), RRR – Reduction Review Route. Major freight parcels are those with a freight-related land use code that are over 100,000 square feet in area.

### 3 Existing Driveway Inventory

The study corridor is characterized by closely spaced driveways including 31 total driveways. Between OR-47 / Quince Street and Mountain View Lane, 13 of the 15 driveways in this section have no restrictions and 2 driveways are restricted to right-in and right-out access. These two driveways are located within the influence area of the OR-8 / OR-47 / Quince Street intersection and are restricted by a traffic separator that extends 500 feet from the intersection. Between Mountain View Lane and 1<sup>st</sup> Avenue, there are 16 driveways that provide either right-in/right-out or left-in/left-out access due to the one-way couplet configuration of Adair Avenue and Pacific Avenue.

A Driveway Summary has been developed to summarize driveway relevant information provided by city and ODOT, and existing use observed in the field and is provided in Appendix B. Appendix B includes a roll plot to provide a visual reference for driveway location with identifying labels, property lines, and property addresses overlaying a

satellite imagery background. The Driveway Summary Table includes for each driveway: unique identifiers, property owners, property use, general access features, dimension to the nearest adjacent driveway, and Access Control status and other relevant information. Only one driveway is gated which is an emergency driveway to the Rose Grove Mobile Park (driveway #24). Per ODOT access management standards OR 8 is a statewide facility in urban areas, with posted speed of 40 mph and AADT higher than 5,000, the spacing standard is 800 feet. The distances between driveways in this section of OR 8 are generally lower than this spacing standard.

Turning movement counts at each driveway and at intersections in the corridor were collected in November 2024 to support access management alternatives analysis. These counts are summarized in Appendix C.

## 4 Safety

### 4.1 Crash Analysis

Methods from the ODOT Analysis Procedures Manual (APM)<sup>1</sup>, were used to identify the crash frequency, severity, type, and contributing factors at the study locations along OR-8. Crash rates for intersections and segments were estimated and compared to appropriate Statewide average crash rates and critical crash rates. ODOT also uses the Safety Priority Index System (SPIS)<sup>2</sup>, a scoring method to identify potential safety concerns on state highways based on frequency, fatal crashes, and crash rates in a three-year period. 2023 SPIS rankings were reviewed for study locations and reported in a later section.

This safety analysis was performed for 4 intersections and 3 corridor segments in the study area. Study intersections are listed in Table 3.

**Table 3. List of Study Intersections**

Intersection	Traffic Control
OR-8 / OR-47 / Quince St.	Signalized
OR-8 / Mountain View Ln.	Signalized
OR-8 (Adair Ave.) / Yew St.	Unsignalized
OR-8 (Pacific Ave.) / Yew St	Unsignalized

The most recent five-year crash data available (2019-2023) was obtained from ODOT’s Crash Analysis and Reporting Unit<sup>3</sup>. ODOT crash data is based on reportable motor vehicle traffic crashes. According to ODOT’s 2023 Motor Vehicle Traffic Crash Analysis and Code Manual<sup>4</sup>: “legally reportable motor vehicle traffic crashes involve a fatality,

<sup>1</sup> [https://www.oregon.gov/odot/Planning/Documents/APMv2\\_Ch4.pdf](https://www.oregon.gov/odot/Planning/Documents/APMv2_Ch4.pdf)

<sup>2</sup> <https://www.oregon.gov/odot/engineering/pages/spis-reports-on-state.aspx>

<sup>3</sup> <https://tvc.odot.state.or.us/tvc/>

<sup>4</sup> [https://www.oregon.gov/ODOT/Data/documents/CDS\\_Code\\_Manual.pdf](https://www.oregon.gov/ODOT/Data/documents/CDS_Code_Manual.pdf)

injury, or damage to personal property or any vehicle in excess of \$2,500.” The following sections provide summaries of the intersection and segment safety analysis.

### 4.1.1 Corridor Crashes

This section summarizes all the crashes that were reported along OR-8 between OR-47 / Quince Street and Mountain View Lane and the segments between Mountain View Lane and the eastern city limits. The summary provides crashes at 3 corridor segments: 1) OR-8 between OR-47 / Quince and Mountain View Lane, 2) Pacific Avenue between Mountain View Lane and city limits, and 3) Adair Avenue between Mountain View Lane and city limits and includes the crashes that occurred at the intersections within each corridor segment. A total of 182 crashes were reported on OR-8 between OR-47 / Quince Avenue and the eastern City limits in the most recent five-year crash data.

#### 4.1.1.1 Crash Frequency and Severity

The five-year crash summary with frequency and severity for corridor segments are shown in Table 4 and Figure 14. In summary:

- OR-8 between OR-47 / Quince Street and Mountain View Lane - the highest frequency of crashes occurred on this corridor segment, with one fatal injury crash and two serious injury crashes. The only fatality was reported at OR-8 / OR-47 / Quince Street signalized intersection due to the left turn movement crash. There were also two serious injury crashes in this segment, one was an angle crash at OR-8 / OR-47 / Quince Street and one left turning crash at 2<sup>nd</sup> Street driveway.
- Adair Avenue between Mountain View Lane and the eastern city limits - this corridor segment had the second highest frequency of crashes, with four serious injury crashes. Three of the crashes resulting in serious injuries occurred at Adair Avenue/Yew Street. The other serious injury involved a pedestrian crossing Adair Avenue at Mountain View Lane.
- Pacific Avenue between Mountain View Lane and the eastern city limits - this segment had the lowest crash frequency, with one serious injury crash involving a pedestrian crossing Pacific Avenue. There was another crash involving a pedestrian in this segment resulting in possible injury.

**Table 4. Summary of Five-Year (2019-2023) Crash Frequency by Severity at Corridor Segments**

Segments	Crash Severity					Total
	Fatal (K)	Serious Injury (A)	Minor Injury (B)	Possible Injury (C)	Property Damage Only (O)	
OR-47 / Quince St. to Mountain View Ln.	1	2	17	36	37	93
Mountain View Ln. (WB) to City Limits	0	4	19	16	29	68
Mountain View Ln. (EB) to City Limits	0	1	3	6	11	21
Total	1	7	39	58	77	182

Figure 14. Crashes - Severity



Source: ODOT.

Note: Some crash locations have been minimally adjusted to enhance visibility.

#### 4.1.1.2 Collision Type

Table 5 summarizes corridor segment crashes by collision type. The most frequent reported collision types were rear-end and turning movement. There was one reported segment crash involving bicyclists and three reported segment crashes involving pedestrians. Figure 15 shows pedestrian and bicycle related crashes and Figure 16 shows left turn crashes.

In summary:

- OR-8 between OR-47 / Quince Street and Mountain View Lane - The majority of reported crashes were rear-end or turning movement crashes with 54% of the turning movement crashes resulted from left turning movement. One right turn crash at Habitat of Humanity driveway involved a bicyclist and resulted in a possible injury.
- Adair Avenue (WB) between Mountain View Lane and the eastern city limits – the majority of crashes were angle crashes occurring at Yew Street. The pattern of angle crashes at Yew Street is resulting the serious injuries (3 serious injury crashes) and the other serious injury involved a pedestrian crossing at Mountain View Lane. 33% of the turning movement crashes resulted from left turning movement.

- Pacific Avenue (EB) between Mountain View Lane and the eastern city limits – the higher percentage of the crashes were rear end or turning movement. Two pedestrian crashes occurred in this segment; one at Yew Street that resulted in possibly injury and one between Yew Street and 1<sup>st</sup> Avenue that resulted in a serious injury.

**Table 5. Summary of Five-Year (2019-2023) Crash Frequency by Collision Type at Corridor Segments**

Segments	Collision Type							
	Angle	Head-On	Rear-End	Sideswipe-Overtaking	Turning Movement	Fixed or Other Object	Ped	Total
OR-47 / Quince St. to Mountain View Ln.	2	1	47	5	37*	1	0	93
(WB) Mountain View Ln. to City Limits	38	0	18	2	9	0	1	68
(EB) Mountain View Ln. to City Limits	1	0	8	2	8	0	2	21
Total	41	1	73	9	54	1	3	182

Note: \* - Crash involving a bicyclist

**Figure 15. Crashes - Pedestrian and Bicycle**



Source: ODOT.

Figure 16. Crashes - Left Turn



Source: ODOT.

Note: Some crash locations have been minimally adjusted to enhance visibility.

#### 4.1.1.3 Contributing Factors

Table 6 shows the distribution of reported crashes by contributing factors for study segments. Similar to the intersection crash trends, “Did not yield right-of-way” and “Followed too closely” were the major contributing factors for crashes on OR-8. The “Other” category included improper driving actions such as improper turn, improper change of lanes, non-motorist illegally in road, etc.

- The majority of reported crashes were contributed by driving behavior, particularly “Did not yield right-of-way” (36%), “Followed too closely” (19%), and “Failure to avoid vehicles ahead” (13%).
- OR-8 between OR-47 / Quince Street and Mountain View Lane - the fatal injury crash in this segment was attributed to the motorist not yielding the right-of-way. Two serious injury crashes were attributed to motorists disregarding traffic signals. One of these involved a pedestrian and resulted in a severe injury.
- OR-8 between Mountain View Lane (WB) and the eastern city limits - three serious injury crashes were attributed to motorists not yielding the right-of-way and one serious injury crash was attributed to a pedestrian disregarding traffic signals.

- OR-8 between Mountain View Lane (EB) and the eastern city limits – one serious injury crash was attributed to a non-motorist illegally in the roadway. One possible injury crash involved a pedestrian and was attributed to a motorist that did not yield right-of-way.
- Most crashes along this corridor occurred during daylight (71%), while 22% occurred under dark conditions. The crash resulting in a fatality at OR-8 between OR-47 / Quince Street occurred under dark conditions with street lighting. Two crashes occurred in dark conditions without street lighting on OR 8 (Adair Avenue) and Yew Street resulting in minor injury and possible injury. One other crash occurred in dark conditions involving a pedestrian who sustained a possible injury on OR8 (Pacific Avenue) at Yew Street. Reduced visibility at night can make it harder for drivers to detect pedestrians, obstacles, or roadway features in time to react. Inadequate or inconsistent lighting may also contribute to driver misjudgment and delayed response, increasing the likelihood of crashes.

**Table 6. Summary of Five-Year (2019-2023) Crash Frequency by Crash Contributing Factors at Corridor Segments**

Segments	Contributing Factor							Total
	Failed to Avoid Vehicle Ahead	Followed Too Closely	Disregarded Traffic Signal	Did Not Yield ROW	Passed Stop Sign	Inattention	Other	
OR-47 / Quince St. to Mountain View Ln.	13	22	5	29	0	5	19	93
Mountain View Ln. (WB) to City Limits	6	9	3	29	13	4	4	68
Mountain View Ln. (EB) to City Limits	4	4	0	7	0	2	4	21
Total	23	35	8	65	13	11	27	182

#### 4.1.1.4 Crash Rates

OR-8 corridor segments, which include intersection crashes within the study area, were analyzed to identify potential problem areas along the study corridor. The 2023 ODOT State Highway Crash Book<sup>5</sup> provided pre-defined highway segments and crash rates from OR-47 / Quince Street to Mountain View Lane and Mountain View Lane to the easter city limits. Crash rates for each of the last reported five years (between 2019-2023) were compared to the statewide average crash rate for similar facilities using Crash Rate Table II in the Crash Book.

This analysis identified all three segments as safety focus areas for further investigation as all corridor segments exceed the Statewide Average Crash rate. These findings are summarized in Table 7.

<sup>5</sup> [https://www.oregon.gov/odot/Data/Documents/Crash\\_Rate\\_Table\\_II\\_2019-2023.pdf](https://www.oregon.gov/odot/Data/Documents/Crash_Rate_Table_II_2019-2023.pdf)



**Table 7. Comparison of Crash Rates with the Statewide Crash Rates for Corridor Segments**

Segments	Start Milepoint	End Milepoint	Area Type	Segment Type	Observed Crash Rate	Statewide Average Crash Rate
OR-47 / Quince St. to Mountain View Ln.	17.48	17.88	Urban City	Principal Arterial	3.652	2.308
Mountain View Ln. (WB) to City Limits	17.22	17.48	Urban City	Principal Arterial	8.106	2.308
Mountain View Ln. (EB) to City Limits	17.22	17.48	Urban City	Principal Arterial	2.755	2.308
<b>Red-</b> indicates crash rates that exceed Statewide average crash rates.						

### 4.1.2 Intersection Crashes

The following sections provide crash analysis for intersections within the corridor segments. For this analysis, intersection-related crashes are defined as crashes that occurred within 250 feet of the intersection. Crashes at OR-8 / OR-47 / Quince Street also include crashes on OR-47 and Quince Street within 250 feet of the intersection. The 2023 SPIS ratings for OR-8 were obtained from ODOT Region 1 reports to screen for locations with SPIS ratings among the state’s top 10%. Three intersections in the study area -- OR-8 / OR-47 / Quince Street, OR-8 / Mountain View Lane, and Adair Avenue / Yew Street intersections -- are rated among the state’s top 10% SPIS sites and have been flagged as safety focus areas.

#### 4.1.2.1 Crash Frequency and Severity

Table 8 presents the crash frequency and severity at the study intersections for the five-year analysis period (2019-2023). Reported serious injury and fatal injury crashes include: one fatality (OR-8 / OR-47 / Quince Street) and five serious injury crashes at the study intersections (one at OR-8 / OR-47 / Quince Street, one at OR-8 / Mountain View Lane, and three at Adair Avenue / Yew Street). The crash at OR-8 / Mountain View Lane involved a pedestrian. There were no crashes involving bicyclists at any intersection. Table 8 shows the location of all study corridor crashes by severity.

**Table 8. Summary of Five-Year (2019-2023) Crash Frequency by Severity at Study Intersections**

Intersections	Crash Severity					
	Fatal (K)	Serious Injury (A)	Minor Injury (B)	Possible Injury (C)	Property Damage Only (O)	Total
OR-8 / OR-47 / Quince St.	1	1	8	22	20	52
OR-8 / Mountain View Ln.	0	1	4	11	8	24
Adair Ave. / Yew St.	0	3	17	11	24	55
Pacific Ave. / Yew St.	0	0	2	2	3	7
<b>Total</b>	<b>1</b>	<b>5</b>	<b>31</b>	<b>46</b>	<b>55</b>	<b>138</b>

In summary:

- The highest number of reported crashes (55 crashes) were observed at the Adair Avenue / Yew Street unsignalized intersection.
- The OR-8 / OR-47 / Quince Street signalized intersection had the second highest number of crashes (52 crashes). The only fatality reported in this intersection is due to the left turn movement crash.
- Adair Avenue / Yew Street had the most serious injury crashes (3 crashes). All were angle crashes with vehicles traveling on Adair Avenue crashing with vehicles crossing from Yew Street, 2 of them traveling in the southbound direction and 1 traveling in the northbound direction.
- The serious injury crash at OR-8 / Mountain View Lane involved a pedestrian crossing OR 8 and a vehicle disregarding traffic signal. The serious injury crash at OR-8 / OR-47 / Quince Street was an angle crash where one or more vehicles disregarded traffic signals.

#### 4.1.2.2 Collision Type

Table 9 shows crash frequency by collision type at study intersections. Overall, rear end and angle crashes are the most common collision types. Figure 15 shows pedestrian and bicycle related crashes and Figure 16 shows left turn crashes.

**Table 9. Summary of Five-Year (2019-2023) Crash Frequency by Collision Type at Study Intersections**

Intersections	Collision Type							Total
	Angle	Head-On	Rear-End	Sideswipe-Overtaking	Turning Movement	Fixed or Other Object	Ped	
OR-8 / OR-47 / Quince St.	2	0	30	1	18	1	0	52
OR-8 / Mountain View Ln.	0	0	21	1	1	0	1	24
Adair Ave. / Yew St.	38	0	8	2	7	0	0	55
Pacific Ave. / Yew St.	1	0	1	1	3	0	1	7
Total	41	0	60	5	29	1	2	138

In summary:

- At the Adair Avenue / Yew Street unsignalized intersection – the majority of the crashes are angle crashes with 38 of the 55 reported crashes angle crashes, eight were rear-end, and seven were turning movement crashes. Three angle crashes resulted in serious injuries.
- At the OR-8 / OR-47 / Quince Street signalized intersection - 30 of the 52 reported crashes were rear-end crashes and eighteen crashes were turning movement related. 78% of the turning movement-related crashes were left turn crashes. The fatal injury crash reported at this intersection involved a vehicle traveling southbound and turning left turn crashing with a vehicle traveling in the northbound direction.



- At the OR-8 / Mountain View Lane signalized intersections – the majority of crashes were rear-end crashes. The only turning movement-related crash in this intersection was a left turn crash.
- There were two reported crashes involving pedestrians. One of these crashes resulted in a serious injury of the pedestrian in wheelchair crossing OR-8 and hit by a vehicle traveling westbound (at OR-8 / Mountain View Lane) and one resulted in a possible injury of the pedestrian in wheelchair crossing Pacific Avenue and hit by a vehicle traveling eastbound and turning left (at Pacific Avenue / Yew Street).

#### 4.1.2.3 Contributing Factors

Table 10 shows crash frequency by contributing factors. The most frequent contributing factors were “Did not yield right-of-way”, “Failed to avoid vehicle ahead”, and “Followed too closely”. In summary:

- The majority of intersection crashes are reportedly related to driving behavior, including the factors: did not yield right-of-way (33%), failure to avoid vehicles ahead (17%), and followed too closely (17%). These factors are typical at signalized intersections experiencing congestion and drivers not expecting to stop at the signalized intersections or making risky turning movements at the same time opposing vehicles are entering the intersection.
- At the signalized intersections of OR-8 / OR-47 / Quince Street and OR-8 / Mountain View Lane, there are crash patterns of high number of rear end crashes, primarily contributed to motorists failing to avoid vehicle ahead or following too closely.
- At OR-8 / OR-47 / Quince Street, a crash trend of left turn crashes are occurring with vehicles failing to yield the ROW or disregarding the traffic signal.
- At Adair Avenue / Yew Street, a high number of angle crashes and left turn crashes with vehicles failing to yield the right of way or passing the stop sign at Yew Street.

**Table 10. Summary of Five-Year (2019-2023) Crash Frequency by Crash Contributing Factors at Study Intersections**

Intersections	Contributing Factor							Total
	Failed to Avoid Vehicle Ahead	Followed Too Closely	Disregarded Traffic Signal	Did Not Yield ROW	Passed Stop Sign	Inattention	Other	
OR-8 / OR-47 / Quince St.	13	13	5	14	0	2	5	52
OR-8 / Mountain View Ln.	6	7	2	1	0	4	4	24
Adair Ave. / Yew St.	3	4	1	27	13	3	4	55
Pacific Ave. / Yew St.	1	0	0	4	0	1	1	7
Total	23	24	8	46	13	10	14	138

#### 4.1.2.4 Crash Rates

The observed crash rate for intersections is a function of the number of crashes and the Annual Average Daily Traffic (AADT). Each intersection is grouped into a reference population based on intersection control. The crash rate (crashes per million entering vehicles) for each intersection was compared to two standards:

- A Critical Crash rate, which compares intersection performance to other similar intersections within the project area
- A statewide 90<sup>th</sup> percentile crash rate<sup>6</sup>, which is based on similar intersections throughout the state (obtained from ODOT’s APM, Exhibit 4-1).

Table 11 shows the crash rates for each study intersection for which crashes were recorded. Intersections that have observed crash rates greater than either the Critical or 90th percentile crash rate were flagged as safety focus areas for further consideration. From the comparative analysis of crash rates, it was determined that:

- At the signalized OR-8 / OR-47 / Quince Street and OR-8 / Mountain View Lane intersections – the observed crash rate was higher than Critical Crash Rate, but lower than the Statewide Crash Rate.
- At the unsignalized Adair Avenue / Yew Street intersection – the observed crash rate was higher than both Critical Crash Rate and the Statewide Crash Rate.
- At the unsignalized Pacific Avenue / Yew Street intersection, the signalized OR-8 / OR-47 / Quince Street and OR-8 / Mountain View Lane intersections – the observed crash rate was lower than both Critical Crash Rate and the Statewide Crash Rate.

**Table 11. Comparison of Crash Rates per Million Entering Vehicles (MEV) with Statewide Crash Rate and Critical Crash Rate**

Intersections	Intersection Type	Control Type	Observed Crash Rate	Critical Crash Rate	Statewide 90 <sup>th</sup> Percentile Crash Rate
OR-8 / OR-47 / Quince St.	4SG	Signalized	0.787	0.624	0.860
OR-8 / Mountain View Ln.	3SG	Signalized	0.456	0.403	0.509
Adair Ave. / Yew St.	4ST	Unsignalized	1.750	0.344	0.408
Pacific Ave. / Yew St.	4ST	Unsignalized	0.291	0.368	0.408
3SG: three-leg signalized; 3ST: three-leg minor stop-control; 4SG: four-leg signalized; 4ST: four-leg minor stop-control Red- indicates crash rates that exceed Statewide crash rates or critical crash rate.					

## 4.2 Potential Countermeasures and CRFs

This section summarizes the potential countermeasures and Crash Reduction Factors. Countermeasures are often implemented as strategies intended to reduce crash frequency or severity. The Oregon Department of Transportation All Roads Transportation Safety (ARTS) Program provides safety practitioners with a list of effective countermeasures that are appropriate improvements to many common safety

<sup>6</sup> [Analysis Procedure Manual \(APM Version 2\), Chapter 4-Safety, Oregon Department of Transportation](#)



issues. Each countermeasure is associated with a Crash Reduction Factor (CRFs). CRFs indicate the effectiveness of a countermeasure by providing the change (reduction) in crash frequency after the implementation of the countermeasure. Table 12 summarizes the potential countermeasures that could be considered when evaluating alternatives and CRFs.

**Table 12. Potential Countermeasures**

Target crashes	Control Type / Location	Potential Countermeasures			
		#	Description	CRF	Target Crash
Left turn crashes	Signalized OR-8 / OR-47 / Quince St.	I9	Replace urban permissive or protected/permissive left turns to protected only	99	Left turn crashes at All Severities
Pedestrian and Bicycle crashes	Signalized OR-8 / OR-47 / Quince St.	BP3	Install Urban Leading Pedestrian or Bicycle Interval at Signalized Intersections	37	Pedestrian and Bicycle Crashes at All Severities
	Signalized OR-8 / Mountain View Ln.				
Pedestrian crashes	Signalized OR-8 / Mountain View Ln.	BP8	Install pedestrian refuge island	31	Pedestrian crashes at All Severities
Pedestrian and Bicycle crashes	Unsignalized OR-8 / Yew St. (Both Directions)	BP2	Provide Intersection Illumination (Bike & Ped)	42	Nighttime Pedestrian and Bicycle Crashes at All Injury Severities
	City Limits to Mountain View Ln. (Both Directions)	BP11	Install Rectangular Rapid Flashing Beacon with Median (3-Lane or More Roadway)	56	Pedestrian Crashes at All Severities
Bicycle Crashes	City Limits to Mountain View Ln. (Both Directions)	BP6	Install Urban Green Bike Lanes at Conflict Points	39	Bicycle Crashes at All Severities
	Mountain View Ln to OR-47 / Quince St.	BP24	Install Buffered Bike Lanes	47	Bicycle Crashes at All Severities
Left Turn and Turning movement crashes	Unsignalized Adair Avenue / Yew St.	H22 H23	Install Urban Traffic Signal	67 angle -143 rear end	Angle & Rear End Crashes at All Severities
All crashes	OR8 between OR-47 / Quince St. and Mountain View Ln.	H38	Provide a Raised Median, Urban Multi-Lane Road	22	All Crashes at All Severities

# - ODOT Countermeasure Number; CRF - ODOT Crash Reduction Factor

## Appendix A: HDM Table 300-10

Table 300-10 Design Element Recommendations for Commercial Corridor

Realm	Design Element	Width Guidance
Pedestrian Realm	Frontage Zone	1'
	Pedestrian Zone <sup>9</sup>	8' to 5'
	Buffer Zone	5' to 0'
	Curb/Gutter <sup>1</sup>	2' to 0.5'
Transition Realm <sup>8</sup>	Separated Bicycle Lane Width (Curb Constrained Facility) <sup>2</sup>	8' to 7'
	On-Street Bicycle Lane Width (not including Buffer) <sup>2</sup>	6' to 5'
	Bicycle/Street Buffer (preferred for On-Street Lane) <sup>2</sup>	5' to 2'
	Right Side Shoulder (if travel lane directly adjacent to curb) <sup>3,5</sup>	4' to 0'
	On-Street Parking	N/A
Travelway Realm <sup>5</sup>	Travel Lane <sup>4,5</sup>	11' to 12'
	Right Turn Lane (including Shy Distances)	12' to 13'
	Left Turn Lane <sup>6</sup>	12' to 14'
	Left Side / Right Side Shy Distance <sup>3</sup>	1' to 0'
	Two-Way Left-Turn Lane <sup>6</sup>	12' to 14'
	Raised Median – No Turn Lane (including Shy Distances)	8' to 11'
	Left-Turn Lane with Raised Curb Median/Separator (including 16" separator & Shy Distance) <sup>7</sup>	14' to 16'

1 Where curb and gutter is used and on-street parking is provided or travel lane is directly adjacent to curb, gutter pan should be included in shoulder/shy or on-street parking measurement. Gutter pan should be included in travel lane, bicycle lane or turn lane measurements only where a smooth transition from gutter pan to roadway surface is provided.

2 Refer to Bicycle Facility Selection process (Section 306 and Part 900) to determine appropriate bicycle facility type. Consider raised bicycle lanes where appropriate. Except for right-turn channelizations, 5-foot on-street bicycle lane allowed only with a street buffer. When a raised buffer is used to protect the bicycle lane, the width should be 6' if parking is adjacent or if signs or other features are anticipated.

3 Overall shoulder width depends on other section elements. Elimination of shoulder width/lateral offset should only be considered in constrained locations and needs to be balanced with all cross-section and drainage needs. If the travel lane is next to a curb with a gutter (e.g., a 2-foot curb zone), the gutter typically serves as minimum right-side shoulder width. A wider shoulder may be needed to accommodate drainage based on hydrological analysis or other specific needs. At 35 mph and above, at a minimum, include a 1-foot shoulder/shy distance. ODOT Roadway Engineering Section | Highway Design Manual Cross Section Elements 300 January 2025 300-29

4 At 40 mph and above, a 12-foot lane is preferred; 10-foot lane width requires a formal design exception from the State Roadway Engineer. On freight- or transit-oriented streets, a 10-foot travel lane is generally not appropriate without a buffer zone or shoulder.

5 On Reduction Review Routes, comply with ODOT Freight Mobility Policies, ORS 366.215 and OAR 731-012. Element dimensions may need to be modified.

6 At 40 mph and above, a 14-foot lane is preferred.

7 At 40 mph and above, a 16-foot width is preferred.

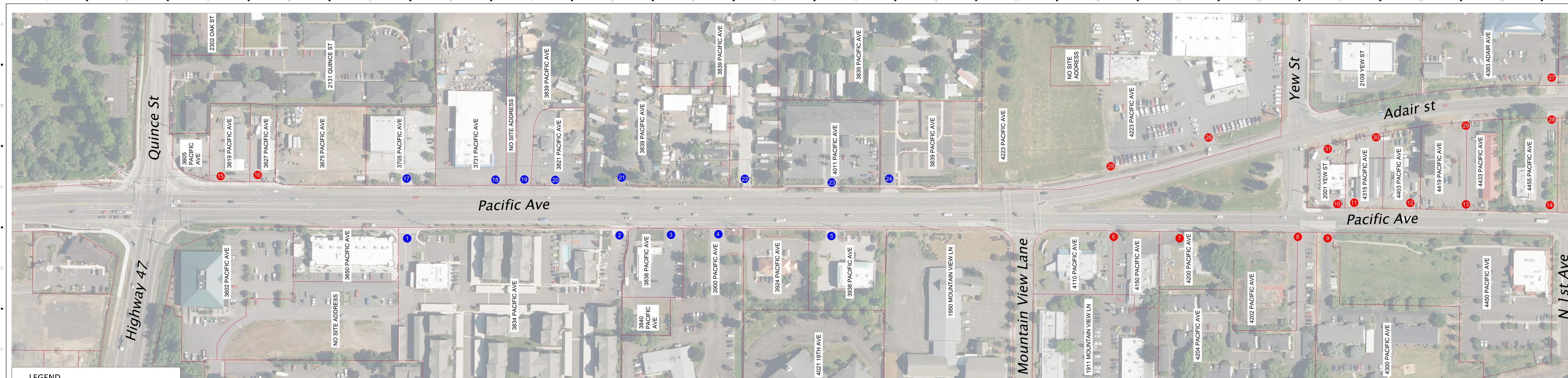
8 When painted buffers or vertical elements like curbing or flexible delineators are proposed to provide separation in a bicycle facility design, evaluate long-term maintenance needs and provide a solution to identified problems.

9 5-foot pedestrian zone requires a paved frontage zone and/or a paved buffer zone. Minimum sidewalk width is 6-feet.



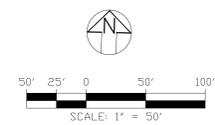
## Appendix B: Driveway Summary and Rollplot

#1	Driveway ID	Access Use	Nearest Driveway	Access Type	Alt Access Point	TLNO	Site Address	Property Owner	Owner Address 1	Owner City	Owner State	Owner Zip	Business/Residence	# of Businesses	Access Control Report	Notes
1	DW1	Shared	526' (DW-2)	Commercial/Residential	(Y)	1S305AB00400	3834 PACIFIC AVE	FORESTPLACE APARTMENTS LLC	27375 SW PARKWAY AVE	WILSONVILLE	OR	97070	US Market, Maytag Equipped Laundry, Forestplace Apartment Homes	3	None	
2	DW2	Shared	126' (DW-3)	Commercial	(Y)	1S305AB00501		STONEWOOD CENTER LLC	PO BOX 12807	SALEM	OR	97309				
3						1S305AB00503	3602 PACIFIC AVE	EASTON HARDWARE BUILDING LLC	PO BOX 308	FOREST GROVE	OR	97116	Ace Hardware	1		
4						1S305AB00504	3650 PACIFIC AVE	AC AVERY LLC	PO BOX 12807	SALEM	OR	97309	Bank of America, Papa Johns, Sherwin-Williams, and others	6		
5	DW-3	Shared	114' (DW-4)	Commercial	(N)	1S305AB00200	3838 PACIFIC AVE	FRYE, AARON	3838 PACIFIC AVE	FOREST GROVE	OR	97116	Fyzical Therapy & Balance Center	1	None	
6						1S305AB00300	3840 PACIFIC AVE	FRYE, AARON	3838 PACIFIC AVE	FOREST GROVE	OR	97116				
7	DW-4	Shared	114' (DW-3)	Commercial	(Y)	1S305AB00100	3900 PACIFIC AVE	NHI-REIT OF OREGON LLC	222 ROBERT ROSE DR	MURFREESBORO	TN	37129	Forest Grove Post Acute	1	None	
8	DW-5	Direct	285' (DW-4)	Commercial	(Y)	1S305AA04700	3938 PACIFIC AVE	AUTOZONE DEVELOPMENT CORP #2216	PO BOX 2198	MEMPHIS	TN	38101	AutoZone Auto Parts			
9						1S305AA04600	3924 PACIFIC AVE	FIRST COMMUNITY CREDIT UNION	200 N ADAMS ST	COQUILLE	OR	97423	First Community Credit Union			
10	DW-6	Shared	171' (DW-7)	Commercial	(Y)	1S305AA00300	4110 PACIFIC AVE	COLUMBIA STATE BANK	PO BOX 2156 MS OP-3300	TACOMA	WA	98401	Columbia Bank		None	
11						1S305AA00301	1911 MOUNTAIN VIEW LN	ZURBRUGG DEVELOPMENT CO	380 SE WASHINGTON ST	HILLSBORO	OR	97123	Mountain View Centre: Multiple business			
12						1S305AA00302	4150 PACIFIC AVE	ZURBRUGG DEVELOPMENT CO	380 SE WASHINGTON ST	HILLSBORO	OR	97123	Flying Pig Music			
13	DW-7	Direct	171' (DW-6)	Residential	(N)	1S305AA00200	4200 PACIFIC AVE	ALERY, JOSEPH & ALERY, CAROL	4200 PACIFIC AVE	FOREST GROVE	OR	97116			None	
14	DW-8	Direct	77' (DW-9)	Residential	(N)	1S305AA00101	4204 PACIFIC AVE	WASHINGTON MUTUAL	1191 2ND AVE	SEATTLE	WA	98111	Karen's Korner: Apartment Buildings		None	
15						1S305AA00100	4202 PACIFIC AVE	GREWAL, MOHANBIR S & GREWAL, NARINDER K	12643 NW MAJESTIC SEQUOIA WAY	PORTLAND	OR	97229				
16						1S305AA00101	4204 PACIFIC AVE	DIANNE GILBERT PROPERTIES I LLC	7831 SE LAKE RD STE 200	MILWAUKIE	OR	97267				
17	DW-9	Shared	77' (DW-8)	Residential	(Y)	1S304BB00500	4300 PACIFIC AVE	EAGLE CREST FG LLC	3225 LAVINA DR	FOREST GROVE	OR	97116	Eagle Crest: Apartment Buildings		None	
18						1S304BB00400	4450 PACIFIC AVE	AMBER WAVES LLC	386 S 27TH AVE	CORNELIUS	OR	97113				
19	DW-10	Direct	40' (DW-11)	Commercial	(Y)	1N333CC00600	2001 YEW ST	YEW 20715 LLC	PO BOX 711	DALLAS	TX	75221	7-Eleven	1	None	
20	DW-11	Direct	40' (DW-10), 129' (DW-31)	Commercial	(N)	1N333CC00700	4315 PACIFIC AVE	F & F PROPERTIES LLC	19435 DERRICK RD	CLOVERDALE	OR	97112				
21	DW-12	Direct	136' (DW-13)	Commercial	(N)	1N333CC00800	4403 PACIFIC AVE	F & F PROPERTIES LLC	19435 DERRICK RD	CLOVERDALE	OR	97112				
22	DW-13	Direct, Shared	212' (DW-14), 213' (DW-28)	Commercial	(N)	1N333CC00902	4419 PACIFIC AVE	CHOI, YOON SUH & CHOI, SOON JA	5710 SOUTHWOOD DR	LAKE OSWEGO	OR	97035	Subway	1	None	
23						1N333CC00901	4433 PACIFIC AVE	CENTRO PROPERTIES LLC	PO BOX 708	CORNELIUS	OR	97113	Centro Cultural	1		
24	DW-14	Direct	Outside study area	Commercial	(N)	1N333CC00900	4455 PACIFIC AVE	GRANDE ANCHORAGE LLC	1700 WASHINGTON ST	VANCOUVER	WA	98660	Taco Bell	1	None	
25	DW-15	Direct	89' (DW-16)	Commercial	(N)	1N332DC00901	3619 PACIFIC AVE	CK DESIGNS LP	2689 RED ARROW DR	LAS VEGAS	NV	89135	Under construction			
26	DW-16	Direct	89' (DW-15)	Commercial	(N)	1N332DC00902	3627 PACIFIC AVE	CK DESIGNS LP	2689 RED ARROW DR	LAS VEGAS	NV	89135	Under construction			
27						1N332DC01000	3605 PACIFIC AVE	CK DESIGNS LP	2689 RED ARROW DR	LAS VEGAS	NV	89135	Under construction			
28	DW-17	Direct	218' (DW-18)	Commercial	(N)	1N332DC00800	3705 PACIFIC AVE	O'REILLY AUTO ENTERPRISES LLC	PO BOX 9167	SPRINGFIELD	MO	65801	O'Reilly Auto Parts	1	Access controlled with reservation	
29						1N332DC00600	3675 PACIFIC AVE	EIG14T NOVA OR FOREST GROVE LLC	1695 TWELVE MILE RD STE #100	BERKLEY	MI	48072				
30	DW-18	Direct	66' (DW-19)	Commercial	(Y)	1N332DC00700	3731 PACIFIC AVE	WEST TUALITY HABITAT FOR HUMANITY	PO BOX 806	FOREST GROVE	OR	97116	Habitat For Humanity ReStore	1	Access controlled with reservation	
31						1N332DC00500	3731 PACIFIC AVE	WEST TUALITY HABITAT FOR HUMANITY	PO BOX 806	FOREST GROVE	OR	97116	Urgent Care			
32	DW-19	Direct	83' (DW-20)	Residential	(N)	1N332DC00300	3839 PACIFIC AVE	ROSE GROVE MOBILE HOME PARK LTD	717 VIA DE LA PAZ	PACIFIC PALISADES	CA	90272	Rose Grove Mobile Home Park		Access controlled with reservation	
33	DW-20	Direct	82' (DW-19)	Commercial	(N)	1N332DC00400	3821 PACIFIC AVE	PERTE, FLORIAN LIV TRUST	1171 SE BROOKWOOD AVE	HILLSBORO	OR	97123	Los Amigos Express		Access controlled with reservation	
34	DW-21	Direct	83' (DW-20)	Residential	(N)	1N332DC00300	3839 PACIFIC AVE	ROSE GROVE MOBILE HOME PARK LTD	717 VIA DE LA PAZ	PACIFIC PALISADES	CA	90272	Rose Grove Mobile Home Park		Access controlled with reservation	
35	DW-22	Direct	143' (DW-23)	Residential	(Y)	1N332D001301	3839 PACIFIC AVE	ROSE GROVE MOBILE HOME PARK, LTD	717 VIA DE LA PAZ	PACIFIC PALISADES	CA	90272	Rose Grove Mobile Home Park		Access controlled with reservation	
36	DW-23	Direct	143' (DW-24)	Commercial	(N)	1N332D001300	4011 PACIFIC AVE	BDS, LLC	61547 TAM MCARTHUR LOOP	BEND	OR	97702	Best Western University Inn & Suites		Access controlled with reservation	
37	DW-24	Shared	143' (DW-23)	Residential	(N)	1N332D001301	3839 PACIFIC AVE	ROSE GROVE MOBILE HOME PARK, LTD	717 VIA DE LA PAZ	PACIFIC PALISADES	CA	90272	Gated: Rose Grove Mobile Home Park		Access controlled with reservation	
38						1N332D001400	3839 PACIFIC AVE	ROSE GROVE MOBILE HOME PARK, LTD	717 VIA DE LA PAZ	PACIFIC PALISADES	CA	90272				
39	DW-25	Direct	556' (DW-24)	Commercial	(Y)	1N332D001500	4223 PACIFIC AVE	DOHERTY FORD INC	PO BOX 97	FOREST GROVE	OR	97116	Doherty Ford: car dealer		Access controlled with reservation	Parcel appears to have a temporary driveway visible in streetview.
40	DW-26	Direct	556' (DW-24)	Commercial	(Y)	1N332D001500	4223 PACIFIC AVE	DOHERTY FORD INC	PO BOX 97	FOREST GROVE	OR	97116	Discount Fireworks Superstore			
41						1N332D001503	4223 PACIFIC AVE	DOHERTY FORD INC	PO BOX 97	FOREST GROVE	OR	97116				
42	DW-27	Direct	Outside study area	Commercial	(N)	1N333CC01000	4383 ADAIR AVE	COOK, MICHAEL	4383 N ADAIR ST	FOREST GROVE	OR	97116			Access controlled with reservation	
43	DW-28	Direct	Outside study area	Commercial	(N)	1N333CC00900	4455 PACIFIC AVE	GRANDE ANCHORAGE LLC	1700 WASHINGTON ST	VANCOUVER	WA	98660	Taco Bell	1	Access controlled with reservation	
44	DW-29	Direct, Shared	211' (DW-14), 212' (DW-28)	Commercial	(N)	1N333CC00902	4419 PACIFIC AVE	CHOI, YOON SUH & CHOI, SOON JA	5710 SOUTHWOOD DR	LAKE OSWEGO	OR	97035	Subway	1	Access controlled with reservation	
45						1N333CC00901	4433 PACIFIC AVE	CENTRO PROPERTIES LLC	PO BOX 708	CORNELIUS	OR	97113	Forest Grove Inn	1		
46	DW-30	Direct	40' (DW-10), 129' (DW-31)	Commercial	(N)	1N333CC00700	4315 PACIFIC AVE	F & F PROPERTIES LLC	19435 DERRICK RD	CLOVERDALE	OR	97112			Access controlled with reservation	
47	DW-31	Direct	40' (DW-11)	Commercial	(Y)	1N333CC00600	2001 YEW ST	YEW 20715 LLC	PO BOX 711	DALLAS	TX	75221	7-Eleven	1	Restricted to Parcel by Statue	
Properties with access to study corridor via local streets																
49	n/a	via Yew St	n/a	Other	(N)	1N333CC01300		FOREST GROVE, CITY OF	1924 COUNCIL ST	FOREST GROVE	OR	97116				
50	n/a	via Yew St	n/a	Commercial	(Y)	1N332D001506		CHAUCER QALICB INC	2238 YEW ST	FOREST GROVE	OR	97116				
51	n/a	via Yew St	n/a	Commercial	(Y)	1N332D001506		CHAUCER FOODS	2238 YEW ST	FOREST GROVE	OR	97116				
52	n/a	via Yew St	n/a	Commercial	(Y)	1N332D001506		CHAUCER FOODS	2238 YEW ST	FOREST GROVE	OR	97116				
53	n/a	via Mountain View Ln	n/a	Residential	(Y)	1S305AA00501	1904 MOUNTAIN VIEW LN	ETHINGTON, ROGER & ETHINGTON, JUDY	2669 E VISTA VERDE CT	GILBERT	AZ	85298				
54	n/a	via 22nd Ave and Mountain View Ln	n/a	Commercial	(Y)	1S305AA00400	1950 MOUNTAIN VIEW LN	OREGON CONFERENCE ADVENTIST CHURCHES	19800 OATFIELD RD	GLADSTONE	OR	97027				
55	n/a	via Yew St	Outside study area	Commercial	(N)	1N333CC00500	2109 YEW ST	SUNSET DEVELOPMENT LLC	PO BOX 669	SCAPPOOSE	OR	97056				
56	n/a	via Quince St	Outside study area	Residential	(N)	1N332DC01100	2131 QUINCE ST	SENKO VILLA	2131 QUINCE ST	FOREST GROVE	OR	97116				
57	n/a	via Yew St	n/a	Residential	(N)	1N333CC00300	2205 YEW ST	VARGAS-CHACON, ELODIO & VARGAS, ROQUELINA NAVARRO DE	2205 YEW ST	FOREST GROVE	OR	97116				
58	n/a	via Yew St	n/a	Commercial	(N)	1N333CC00200	2221 YEW ST	RELCOM INC	2221 YEW ST	FOREST GROVE	OR	97116				
59	n/a	via Yew St	n/a	Commercial	(N)	1N333CC00101	2235 YEW ST	F DAVIS PROPERTIES LLC	6950 NW KANSAS CITY RD	FOREST GROVE	OR	97116				
60	n/a	via Yew St	Outside study area	Commercial	(N)	1N332D001506	2238 YEW ST	LINEAGE HCS MASTER RE LLC	46500 HUMBOLDT DR	NOVI	MI	48377				
61	n/a	via Quince St	Outside study area	Residential	(N)	1N332DC01101	2302 OAK ST	SENKO, SUSAN K TRUST	2135 QUINCE ST	FOREST GROVE	OR	97116				
62	n/a	via 19th Ave and Mountain View Ln	n/a	Commercial	(Y)	1S305AA00500	4021 19TH AVE	OREGON CONFERENCE ADVENTIST CHURCHES	19800 OATFIELD RD	GLADSTONE	OR	97027				
63	n/a	via 22nd Ave and Mountain View Ln	n/a	Residential	(N)	1N333CC01100	4308 22ND AVE	CONTRERAS, MARIA G GARCIA	4306 22ND AVE #4308	FOREST GROVE	OR	97116				
64	n/a	via 22nd Ave and Mountain View Ln	n/a	Residential	(N)	1N333CC01200	4350 22ND AVE	ESPARZA, PAOLINA & ESPARZA, ISMAEL	4348 22ND AVE	FOREST GROVE	OR	97116				
65	n/a	via 22nd Ave and Mountain View Ln	n/a	Residential	(Y)	1N333CC01301	4405 N ADAIR ST	VANDYKE, NORMAN A & VANDYKE, BETTY J	4405 ADAIR ST	FOREST GROVE	OR	97116				



**LEGEND**

- DRIVEWAY WITH FULL ACCESS #
- DRIVEWAY WITH RIGHT IN RIGHT OUT OR LEFT IN LEFT OUT ACCESS #



PACIFIC AVENUE & TUALATIN VALLEY HIGHWAY  
ACCESS MANAGEMENT STRATEGY

ODOT K21732  
FHWA FED AID #SA00(603)

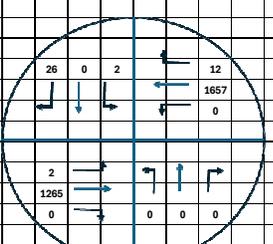
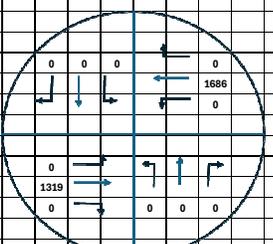
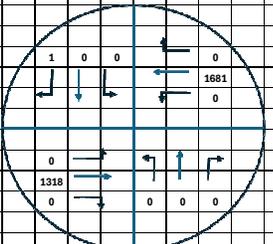
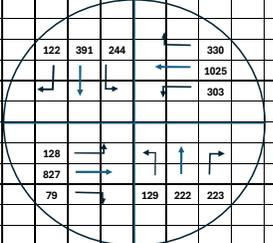
SCALE: 1:50    DRAWING NO.: XXX    CONTRACT NO.: PG-73000-0005655    SHEET NO.: 1 OF 1



## Appendix C: Driveway Turning Count Summary

Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Intersection	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
OR 47_OR 8	4:30 PM	14	13	26	0	23	24	12	0	9	73	12	0	24	101	29	0	360
	4:35 PM	15	21	20	0	24	30	10	0	13	61	13	0	28	92	35	0	362
	4:40 PM	10	23	23	0	26	30	15	0	9	71	5	0	31	80	32	0	355
	4:45 PM	13	21	23	0	22	35	14	0	7	72	5	0	17	85	29	0	343
	4:50 PM	11	24	18	0	14	38	7	0	9	68	5	0	19	91	26	0	330
	4:55 PM	11	19	15	0	16	37	14	0	7	65	5	0	31	69	28	0	317
	5:00 PM	9	22	10	0	12	36	6	0	11	66	7	0	29	62	20	0	290
	5:05 PM	7	14	8	0	23	33	3	0	16	63	5	0	34	80	25	0	311
	5:10 PM	13	15	15	0	20	26	10	0	18	84	6	0	22	100	20	0	349
	5:15 PM	7	18	15	0	17	30	12	0	8	70	7	0	21	91	27	0	323
	5:20 PM	4	17	27	0	20	37	8	0	11	70	5	0	21	98	35	0	353
5:25 PM	15	15	23	0	27	35	11	0	10	64	4	0	26	76	24	0	330	
<b>Total</b>		<b>129</b>	<b>222</b>	<b>223</b>	<b>0</b>	<b>244</b>	<b>391</b>	<b>122</b>	<b>0</b>	<b>128</b>	<b>827</b>	<b>79</b>	<b>0</b>	<b>303</b>	<b>1025</b>	<b>330</b>	<b>0</b>	<b>4023</b>
Pro Group Realtors Dwy_OR 8	4:30 PM	0	0	0	0	0	0	0	0	0	122	0	0	0	153	0	0	275
	4:35 PM	0	0	0	0	0	0	0	0	0	111	0	0	0	154	0	0	265
	4:40 PM	0	0	0	0	0	0	0	0	0	119	0	0	0	144	0	0	263
	4:45 PM	0	0	0	0	0	0	0	0	0	128	0	0	0	137	0	0	265
	4:50 PM	0	0	0	0	0	0	0	0	0	104	0	0	0	139	0	0	243
	4:55 PM	0	0	0	0	0	0	0	0	0	97	0	0	0	132	0	0	229
	5:00 PM	0	0	0	0	0	0	0	0	0	98	0	0	0	112	0	0	210
	5:05 PM	0	0	0	0	0	0	0	0	0	93	0	0	0	135	0	0	228
	5:10 PM	0	0	0	0	0	0	0	0	0	119	0	0	0	138	0	0	257
	5:15 PM	0	0	0	0	0	0	0	0	0	102	0	0	0	144	0	0	246
	5:20 PM	0	0	0	0	0	0	0	0	0	117	0	0	0	156	0	0	273
5:25 PM	0	0	0	0	0	0	1	0	0	108	0	0	0	137	0	0	246	
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>1318</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1681</b>	<b>0</b>	<b>0</b>	<b>3000</b>
The Auto Dealer Dwy_OR 8	4:30 PM	0	0	0	0	0	0	0	0	0	122	0	0	0	157	0	0	279
	4:35 PM	0	0	0	0	0	0	0	0	0	112	0	0	0	157	0	0	269
	4:40 PM	0	0	0	0	0	0	0	0	0	120	0	0	0	135	0	0	255
	4:45 PM	0	0	0	0	0	0	0	0	0	133	0	0	0	135	0	0	268
	4:50 PM	0	0	0	0	0	0	0	0	0	103	0	0	0	142	0	0	245
	4:55 PM	0	0	0	0	0	0	0	0	0	100	0	0	0	131	0	0	231
	5:00 PM	0	0	0	0	0	0	0	0	0	90	0	0	0	113	0	0	203
	5:05 PM	0	0	0	0	0	0	0	0	0	96	0	0	0	137	0	0	233
	5:10 PM	0	0	0	0	0	0	0	0	0	112	0	0	0	132	0	0	244
	5:15 PM	0	0	0	0	0	0	0	0	0	104	0	0	0	153	0	0	257
	5:20 PM	0	0	0	0	0	0	0	0	0	121	0	0	0	147	0	0	268
5:25 PM	0	0	0	0	0	0	0	0	0	106	0	0	0	147	0	0	253	
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1319</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1686</b>	<b>0</b>	<b>0</b>	<b>3005</b>
O'Reilly Dwy_OR 8	4:30 PM	0	0	0	0	0	0	4	0	0	118	0	0	0	140	2	0	264
	4:35 PM	0	0	0	0	0	0	2	0	0	106	0	0	0	152	2	0	262
	4:40 PM	0	0	0	0	0	0	2	0	0	117	0	0	0	127	1	0	247
	4:45 PM	0	0	0	0	0	0	3	0	0	131	0	0	0	133	0	0	267
	4:50 PM	0	0	0	0	1	0	2	0	0	98	0	0	0	149	0	0	250
	4:55 PM	0	0	0	0	0	0	2	0	1	93	0	0	0	123	1	0	220
	5:00 PM	0	0	0	0	0	0	1	0	0	83	0	0	0	135	1	0	220
	5:05 PM	0	0	0	0	0	0	3	0	0	90	0	0	0	131	0	0	224
	5:10 PM	0	0	0	0	0	0	3	0	0	106	0	0	0	122	1	0	232
	5:15 PM	0	0	0	0	0	0	2	0	0	98	0	0	0	159	2	0	261
	5:20 PM	0	0	0	0	0	0	1	0	0	118	0	0	0	140	1	0	260
5:25 PM	0	0	0	0	1	0	1	0	1	107	0	0	0	146	1	0	257	
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>26</b>	<b>0</b>	<b>2</b>	<b>1265</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1657</b>	<b>12</b>	<b>0</b>	<b>2964</b>



Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Driveway #1	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total	
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
US Market Dwy_OR 8	4:30 PM	0	0	4	0	0	0	0	0	0	118	6	0	5	140	0	0	273	
	4:35 PM	0	0	4	0	0	0	0	0	0	106	5	0	7	152	0	0	274	
	4:40 PM	0	0	6	0	0	0	0	0	0	117	6	0	3	127	0	0	259	
	4:45 PM	1	0	4	0	0	0	0	0	0	131	4	0	2	133	0	0	275	
	4:50 PM	0	0	11	0	0	0	0	0	0	98	7	0	2	149	0	0	267	
	4:55 PM	1	0	2	0	0	0	0	0	0	93	9	0	1	123	0	0	229	
	5:00 PM	1	0	4	0	0	0	0	0	0	83	5	0	0	135	0	0	228	
	5:05 PM	0	0	5	0	0	0	0	0	0	90	3	0	1	131	0	0	230	
	5:10 PM	0	0	3	0	0	0	0	0	0	106	4	0	3	122	0	0	238	
	5:15 PM	1	0	3	0	0	0	0	0	0	98	5	0	0	159	0	0	266	
	5:20 PM	1	0	3	0	0	0	0	0	0	118	4	0	7	140	0	0	273	
	5:25 PM	0	0	3	0	0	0	0	0	0	107	5	0	3	146	0	0	264	
	<b>Total</b>		<b>5</b>	<b>0</b>	<b>52</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1265</b>	<b>63</b>	<b>0</b>	<b>34</b>	<b>1657</b>	<b>0</b>	<b>0</b>	<b>3076</b>	
Habitat for Humanity Dwy_OR 8	4:30 PM	0	0	0	0	1	0	2	0	0	120	0	0	0	147	1	0	271	
	4:35 PM	0	0	0	0	0	0	0	0	0	113	0	0	0	159	0	0	272	
	4:40 PM	0	0	0	0	0	0	3	0	0	123	0	0	0	129	0	0	255	
	4:45 PM	0	0	0	0	0	0	2	0	0	133	0	0	0	136	1	0	272	
	4:50 PM	0	0	0	0	0	0	0	0	0	110	0	0	0	148	1	0	259	
	4:55 PM	0	0	0	0	2	0	1	0	0	96	0	0	0	123	1	0	223	
	5:00 PM	0	0	0	0	0	0	1	0	0	88	0	0	0	135	0	0	224	
	5:05 PM	0	0	0	0	1	0	0	0	0	96	0	0	0	131	0	0	228	
	5:10 PM	0	0	0	0	0	0	0	0	0	105	0	0	0	127	0	0	232	
	5:15 PM	0	0	0	0	0	0	0	0	0	104	0	0	0	159	0	0	263	
	5:20 PM	0	0	0	0	0	0	0	0	0	127	0	0	0	147	0	0	274	
	5:25 PM	0	0	0	0	0	0	1	0	0	118	0	0	0	149	0	0	268	
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>10</b>	<b>0</b>	<b>1333</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1690</b>	<b>4</b>	<b>0</b>	<b>3041</b>	
Rose Grove Dwy (C and D Row)_OR 8	4:30 PM	0	0	0	0	1	0	0	0	0	124	0	0	0	144	5	0	274	
	4:35 PM	0	0	0	0	0	0	1	0	0	111	0	0	0	157	2	0	271	
	4:40 PM	0	0	0	0	2	0	0	0	0	119	0	0	0	131	0	0	252	
	4:45 PM	0	0	0	0	0	0	2	0	0	137	0	0	0	133	0	0	272	
	4:50 PM	0	0	0	0	0	0	1	0	0	111	0	0	0	147	3	0	262	
	4:55 PM	0	0	0	0	0	0	2	0	0	101	0	0	0	122	2	0	227	
	5:00 PM	0	0	0	0	2	0	2	1	0	90	0	0	0	126	3	0	224	
	5:05 PM	0	0	0	0	0	0	3	0	1	97	0	0	0	134	2	0	237	
	5:10 PM	0	0	0	0	1	0	0	0	1	102	0	0	0	129	0	0	233	
	5:15 PM	0	0	0	0	0	0	1	0	1	103	0	0	0	156	5	0	266	
	5:20 PM	0	0	0	0	3	0	1	0	0	127	0	0	0	137	4	0	272	
	5:25 PM	0	0	0	0	0	0	0	0	0	119	0	0	0	143	0	0	262	
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9</b>	<b>0</b>	<b>13</b>	<b>1</b>	<b>3</b>	<b>1341</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1659</b>	<b>26</b>	<b>0</b>	
Los Amigos Dwy_OR 8	4:30 PM	0	0	0	0	0	0	1	0	0	124	0	0	0	149	0	0	274	
	4:35 PM	0	0	0	0	0	0	0	0	0	110	0	0	0	156	1	0	267	
	4:40 PM	0	0	0	0	0	0	0	0	1	120	0	0	0	133	0	0	254	
	4:45 PM	0	0	0	0	0	0	2	0	0	138	0	0	0	132	0	0	272	
	4:50 PM	0	0	0	0	0	0	0	0	1	112	0	0	0	150	0	0	263	
	4:55 PM	0	0	0	0	0	0	0	0	0	100	0	0	0	121	0	0	221	
	5:00 PM	0	0	0	0	0	0	0	0	0	92	0	0	0	129	2	0	223	
	5:05 PM	0	0	0	0	0	0	0	0	0	98	0	0	0	137	1	0	236	
	5:10 PM	0	0	0	0	0	0	0	0	0	102	0	0	0	128	1	0	231	
	5:15 PM	0	0	0	0	0	0	1	0	0	102	0	0	0	163	0	0	266	
	5:20 PM	0	0	0	0	0	0	0	0	1	126	0	0	0	143	0	0	270	
	5:25 PM	0	0	0	0	1	0	0	0	0	126	0	0	0	141	1	0	269	
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>3</b>	<b>1350</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1682</b>	<b>6</b>	<b>0</b>	

Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Driveway #2	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total	
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
Forest Place Dwy_OR 8	4:30 PM	0	0	1	0	0	0	0	0	0	120	0	0	1	149	0	0	271	
	4:35 PM	1	0	0	0	0	0	0	0	0	108	1	0	4	154	0	0	268	
	4:40 PM	1	0	1	0	0	0	0	0	0	116	0	0	1	131	0	0	250	
	4:45 PM	0	0	0	0	0	0	0	0	0	137	1	0	1	135	0	0	274	
	4:50 PM	0	0	0	0	0	0	0	0	0	112	0	0	0	146	0	0	258	
	4:55 PM	0	0	1	0	0	0	0	0	0	99	0	0	3	127	0	0	230	
	5:00 PM	0	0	2	0	0	0	0	0	0	87	0	0	0	132	0	0	221	
	5:05 PM	0	0	3	0	0	0	0	0	0	97	1	0	1	137	0	1	240	
	5:10 PM	0	0	1	0	0	0	0	0	0	101	1	0	0	132	0	0	235	
	5:15 PM	0	0	4	0	0	0	0	0	0	102	0	0	0	164	0	0	270	
	5:20 PM	1	0	0	0	0	0	0	0	0	125	0	0	1	145	0	0	272	
	5:25 PM	0	0	0	0	0	0	0	0	0	124	0	0	2	143	0	0	269	
<b>Total</b>		<b>3</b>	<b>0</b>	<b>13</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1328</b>	<b>4</b>	<b>0</b>	<b>14</b>	<b>1695</b>	<b>0</b>	<b>1</b>	<b>3058</b>		
Rose Grove Dwy (A and B Row)_OR 8	4:30 PM	0	0	0	0	0	0	0	0	1	120	0	0	0	149	0	0	270	
	4:35 PM	0	0	0	0	1	0	1	0	1	108	0	0	0	154	0	0	265	
	4:40 PM	0	0	0	0	0	0	1	0	3	116	0	0	0	131	1	0	252	
	4:45 PM	0	0	0	0	1	0	0	0	2	137	0	0	0	135	3	0	278	
	4:50 PM	0	0	0	0	0	0	1	0	0	112	0	0	0	146	2	0	261	
	4:55 PM	0	0	0	0	0	0	0	0	0	99	0	0	0	127	2	0	228	
	5:00 PM	0	0	0	0	0	0	0	0	1	87	0	0	0	132	2	0	222	
	5:05 PM	0	0	0	0	0	0	0	0	1	97	0	0	0	137	1	1	237	
	5:10 PM	0	0	0	0	1	0	1	0	0	101	0	0	0	132	1	0	236	
	5:15 PM	0	0	0	0	0	0	1	0	0	102	0	0	0	164	3	0	270	
	5:20 PM	0	0	0	0	3	0	0	0	1	125	0	0	0	145	1	0	275	
	5:25 PM	0	0	0	0	1	0	0	0	0	124	0	0	0	143	0	0	268	
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>7</b>	<b>0</b>	<b>5</b>	<b>0</b>	<b>10</b>	<b>1328</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1695</b>	<b>16</b>	<b>1</b>	<b>3062</b>	
Driveway #3	4:30 PM	0	0	1	0	0	0	0	0	0	118	0	0	1	157	0	0	277	
	4:35 PM	0	0	0	0	0	0	0	0	0	108	0	0	0	148	0	0	256	
	4:40 PM	0	0	1	0	0	0	0	0	0	118	2	0	0	136	0	0	257	
	4:45 PM	1	0	1	0	0	0	0	0	0	131	0	0	0	136	0	0	269	
	4:50 PM	0	0	1	0	0	0	0	0	0	117	0	0	0	149	0	0	267	
	4:55 PM	1	0	0	0	0	0	0	0	0	90	0	0	0	126	0	0	217	
	5:00 PM	1	0	0	0	0	0	0	0	0	92	0	0	0	132	0	0	225	
	5:05 PM	1	0	0	0	0	0	0	0	0	100	0	0	0	138	0	0	239	
	5:10 PM	0	0	0	0	0	0	0	0	0	101	0	0	0	138	0	0	239	
	5:15 PM	0	0	0	0	0	0	0	0	0	107	0	0	0	166	0	0	273	
	5:20 PM	0	0	0	0	0	0	0	0	0	130	0	0	0	136	0	0	266	
	5:25 PM	0	0	0	0	0	0	0	0	0	124	0	0	0	146	0	0	270	
<b>Total</b>		<b>4</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1336</b>	<b>2</b>	<b>0</b>	<b>1</b>	<b>1708</b>	<b>0</b>	<b>0</b>	<b>3055</b>	
Driveway #4	4:30 PM	1	0	2	0	0	0	0	0	0	119	0	0	1	157	0	0	280	
	4:35 PM	0	0	0	0	0	0	0	0	0	104	1	0	0	147	0	0	252	
	4:40 PM	0	0	1	0	0	0	0	0	0	117	0	0	0	136	0	0	254	
	4:45 PM	0	0	0	0	0	0	0	0	0	130	0	0	0	145	0	0	275	
	4:50 PM	2	0	1	0	0	0	0	0	0	115	2	0	1	144	0	0	265	
	4:55 PM	0	0	0	0	0	0	0	0	0	92	1	0	1	116	0	0	210	
	5:00 PM	0	0	0	0	0	0	0	0	0	93	0	0	1	129	0	0	223	
	5:05 PM	1	0	2	0	0	0	0	0	0	96	1	0	0	138	0	0	238	
	5:10 PM	2	0	0	0	0	0	0	0	0	99	0	0	0	133	0	0	234	
	5:15 PM	0	0	0	0	0	0	0	0	0	99	0	4	0	162	0	0	265	
	5:20 PM	1	0	0	0	0	0	0	0	0	121	2	0	0	137	0	0	261	
	5:25 PM	0	0	2	0	0	0	0	0	0	122	0	0	0	150	0	0	274	
<b>Total</b>		<b>7</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1307</b>	<b>7</b>	<b>4</b>	<b>4</b>	<b>1694</b>	<b>0</b>	<b>0</b>	<b>3031</b>	

Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Driveway #22	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
		4:30 PM	0	0	0	0	3	0	0	0	0	119	0	0	0	157	6	
4:35 PM	0	0	0	0	4	0	1	0	1	104	0	0	0	147	0	0	257	
4:40 PM	0	0	0	0	6	0	4	0	3	117	0	0	0	136	3	0	269	
4:45 PM	0	0	0	0	2	0	1	0	0	130	0	0	0	145	7	0	285	
4:50 PM	0	0	0	0	1	0	3	0	0	115	0	0	0	144	2	0	265	
4:55 PM	0	0	0	0	1	0	3	0	0	92	0	0	0	116	2	0	214	
5:00 PM	0	0	0	0	2	0	4	0	2	93	0	0	0	129	6	0	236	
5:05 PM	0	0	0	0	4	0	1	0	1	96	0	0	0	138	1	0	241	
5:10 PM	0	0	0	0	2	0	1	0	1	99	0	0	0	133	3	0	239	
5:15 PM	0	0	0	0	0	0	2	0	1	99	0	4	0	162	7	0	275	
5:20 PM	0	0	0	0	1	0	2	0	4	121	0	0	0	137	1	0	266	
5:25 PM	0	0	0	0	5	0	3	0	3	122	0	0	0	150	2	0	285	
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>31</b>	<b>0</b>	<b>25</b>	<b>0</b>	<b>16</b>	<b>1307</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>1694</b>	<b>40</b>	<b>0</b>	<b>3117</b>

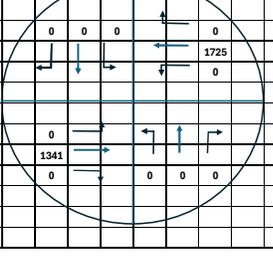
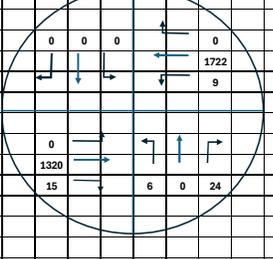
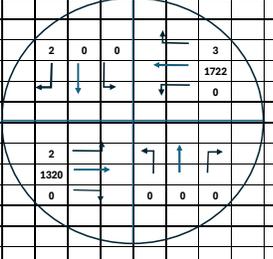
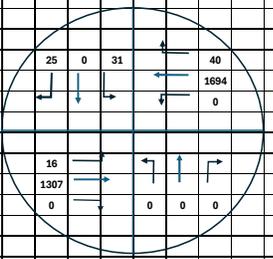
Driveway #23	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
		4:30 PM	0	0	0	0	0	0	0	0	0	121	0	0	0	156	0	
4:35 PM	0	0	0	0	0	0	0	0	0	109	0	0	0	144	0	0	253	
4:40 PM	0	0	0	0	0	0	0	0	0	122	0	0	0	140	0	0	262	
4:45 PM	0	0	0	0	0	0	0	0	1	124	0	0	0	147	0	0	272	
4:50 PM	0	0	0	0	0	0	0	0	0	118	0	0	0	149	0	0	267	
4:55 PM	0	0	0	0	0	0	0	0	1	91	0	0	0	118	0	0	210	
5:00 PM	0	0	0	0	0	0	0	0	0	95	0	0	0	133	1	0	229	
5:05 PM	0	0	0	0	0	0	0	0	0	97	0	0	0	143	0	0	240	
5:10 PM	0	0	0	0	0	0	1	0	0	98	0	0	0	134	1	0	234	
5:15 PM	0	0	0	0	0	0	0	0	0	97	0	0	0	173	1	0	271	
5:20 PM	0	0	0	0	0	0	1	0	0	126	0	0	0	137	0	0	264	
5:25 PM	0	0	0	0	0	0	0	0	0	122	0	0	0	148	0	0	270	
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>2</b>	<b>1320</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1722</b>	<b>3</b>	<b>0</b>	<b>3049</b>

Driveway #5	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
		4:30 PM	0	0	1	0	0	0	0	0	0	121	1	0	1	156	0	
4:35 PM	2	0	2	0	0	0	0	0	0	109	1	0	3	144	0	0	261	
4:40 PM	1	0	2	0	0	0	0	0	0	122	1	0	0	140	0	0	266	
4:45 PM	0	0	2	0	0	0	0	0	0	124	4	0	0	147	0	0	277	
4:50 PM	0	0	4	0	0	0	0	0	0	118	2	0	2	149	0	0	275	
4:55 PM	0	0	1	0	0	0	0	0	0	91	1	0	0	118	0	0	211	
5:00 PM	1	0	2	0	0	0	0	0	0	95	0	0	0	133	0	0	231	
5:05 PM	1	0	2	0	0	0	0	0	0	97	1	0	0	143	0	0	244	
5:10 PM	0	0	3	0	0	0	0	0	0	98	1	0	1	134	0	0	237	
5:15 PM	0	0	2	0	0	0	0	0	0	97	0	0	1	173	0	0	273	
5:20 PM	1	0	2	0	0	0	0	0	0	126	2	0	1	137	0	0	269	
5:25 PM	0	0	1	0	0	0	0	0	0	122	1	0	0	148	0	0	272	
<b>Total</b>		<b>6</b>	<b>0</b>	<b>24</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1320</b>	<b>15</b>	<b>0</b>	<b>9</b>	<b>1722</b>	<b>0</b>	<b>0</b>	<b>3096</b>

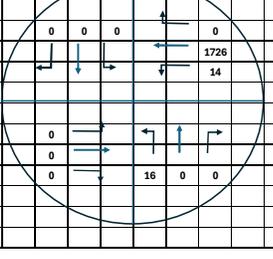
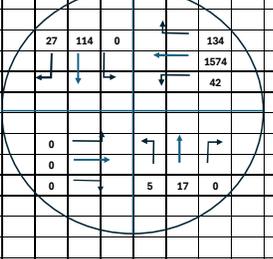
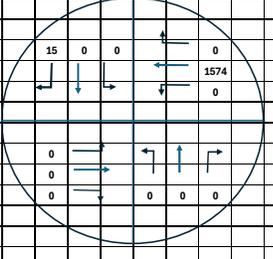
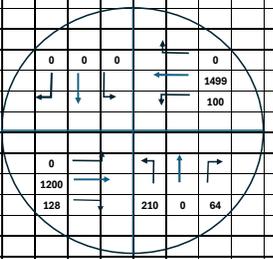
  

Driveway #24	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
		4:30 PM	0	0	0	0	0	0	0	0	0	130	0	0	0	162	0	
4:35 PM	0	0	0	0	0	0	0	0	0	110	0	0	0	146	0	0	256	
4:40 PM	0	0	0	0	0	0	0	0	0	120	0	0	0	132	0	0	252	
4:45 PM	0	0	0	0	0	0	0	0	0	130	0	0	0	150	0	0	280	
4:50 PM	0	0	0	0	0	0	0	0	0	124	0	0	0	149	0	0	273	
4:55 PM	0	0	0	0	0	0	0	0	0	83	0	0	0	117	0	0	200	
5:00 PM	0	0	0	0	0	0	0	0	0	97	0	0	0	136	0	0	233	
5:05 PM	0	0	0	0	0	0	0	0	0	94	0	0	0	137	0	0	231	
5:10 PM	0	0	0	0	0	0	0	0	0	115	0	0	0	137	0	0	252	
5:15 PM	0	0	0	0	0	0	0	0	0	104	0	0	0	171	0	0	275	
5:20 PM	0	0	0	0	0	0	0	0	0	122	0	0	0	141	0	0	263	
5:25 PM	0	0	0	0	0	0	0	0	0	112	0	0	0	147	0	0	259	
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1341</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1725</b>	<b>0</b>	<b>0</b>	<b>3066</b>



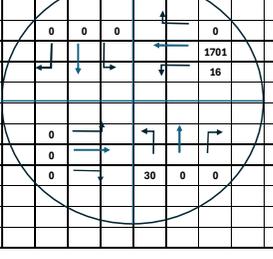
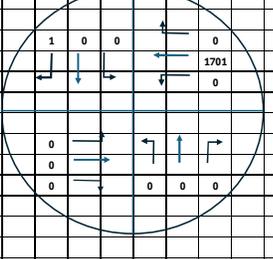
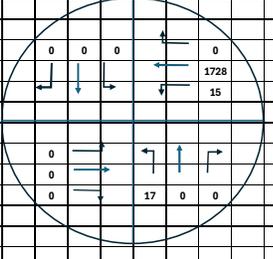
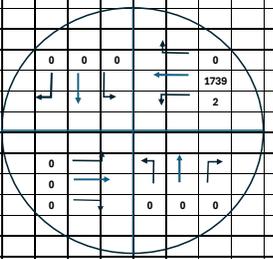
Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Intersection	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
		Mountain View Ln. OR 8/Adair Ave	4:30 PM	11	0	9	0	0	0	0	0	0	115	17	0	10	155	
4:35 PM	19		0	11	0	0	0	0	0	0	90	16	0	11	129	0	1	277
4:40 PM	18		0	8	0	0	0	0	0	0	113	9	0	9	116	0	0	273
4:45 PM	18		0	2	0	0	0	0	0	0	113	17	0	8	129	0	0	287
4:50 PM	20		0	4	0	0	0	0	0	0	109	8	0	9	130	0	0	280
4:55 PM	16		0	8	0	0	0	0	0	0	71	13	0	6	99	0	1	214
5:00 PM	19		0	3	0	0	0	0	0	0	86	6	0	10	115	0	1	240
5:05 PM	19		0	5	0	0	0	0	0	0	87	13	0	10	125	0	0	259
5:10 PM	15		0	4	0	0	0	0	0	0	99	10	0	8	116	0	0	252
5:15 PM	21		0	2	0	0	0	0	0	0	99	8	0	4	144	0	0	278
5:20 PM	18		0	4	0	0	0	0	0	0	110	10	0	6	118	0	0	266
5:25 PM	16		0	4	0	0	0	0	0	0	108	1	0	9	123	0	0	261
<b>Total</b>			<b>210</b>	<b>0</b>	<b>64</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1200</b>	<b>128</b>	<b>0</b>	<b>100</b>	<b>1499</b>	<b>0</b>	<b>4</b>	<b>3205</b>
Ford Dwy. Adair Ave	4:30 PM	0	0	0	0	0	0	4	0	0	0	0	0	137	0	0	141	
	4:35 PM	0	0	0	0	0	0	2	0	0	0	0	0	135	0	0	137	
	4:40 PM	0	0	0	0	0	0	0	0	0	0	0	0	118	0	0	118	
	4:45 PM	0	0	0	0	0	0	3	0	0	0	0	0	139	0	0	142	
	4:50 PM	0	0	0	0	0	0	2	0	0	0	0	0	138	0	0	140	
	4:55 PM	0	0	0	0	0	0	0	0	0	0	0	0	112	0	0	112	
	5:00 PM	0	0	0	0	0	0	1	0	0	0	0	0	122	0	0	123	
	5:05 PM	0	0	0	0	0	0	1	0	0	0	0	0	136	0	0	137	
	5:10 PM	0	0	0	0	0	0	0	0	0	0	0	0	139	0	0	139	
	5:15 PM	0	0	0	0	0	0	1	0	0	0	0	0	131	0	0	132	
	5:20 PM	0	0	0	0	0	0	1	0	0	0	0	0	132	0	0	133	
	5:25 PM	0	0	0	0	0	0	0	0	0	0	0	0	135	0	0	135	
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>15</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1574</b>	<b>0</b>	<b>0</b>	<b>1589</b>
Yew St. Adair Ave	4:30 PM	0	1	0	0	0	12	5	0	0	0	0	0	7	138	14	0	177
	4:35 PM	0	3	0	0	0	12	2	0	0	0	0	0	1	132	7	0	157
	4:40 PM	1	2	0	0	0	16	3	0	0	0	0	0	2	120	3	0	147
	4:45 PM	1	0	0	0	0	9	0	0	0	0	0	0	3	138	9	0	160
	4:50 PM	0	2	0	0	0	10	2	0	0	0	0	0	2	137	9	0	162
	4:55 PM	0	1	0	0	0	5	2	0	0	0	0	0	1	112	10	0	131
	5:00 PM	0	2	0	0	0	8	1	0	0	0	0	0	1	125	14	0	151
	5:05 PM	1	2	0	0	0	8	1	0	0	0	0	0	3	132	18	0	165
	5:10 PM	1	2	0	0	0	9	2	0	0	0	0	0	6	132	11	0	163
	5:15 PM	0	1	0	0	0	4	3	0	0	0	0	0	5	136	17	0	166
	5:20 PM	1	0	0	0	0	8	5	0	0	0	0	0	7	132	10	0	163
	5:25 PM	0	1	0	0	0	13	1	0	0	0	0	0	4	140	12	0	171
	<b>Total</b>		<b>5</b>	<b>17</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>114</b>	<b>27</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>42</b>	<b>1574</b>	<b>134</b>	<b>0</b>	<b>1913</b>
7-Eleven Dwy. Adair St	4:30 PM	4	0	0	0	0	0	0	0	0	0	0	0	156	0	0	160	
	4:35 PM	1	0	0	0	0	0	0	0	0	0	0	0	1	141	0	0	143
	4:40 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	124	0	0	125
	4:45 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	145	0	0	147
	4:50 PM	1	0	0	0	0	0	0	0	0	0	0	0	1	144	0	0	146
	4:55 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	122	0	0	123
	5:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	3	138	0	0	142
	5:05 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	153	0	0	155
	5:10 PM	2	0	0	0	0	0	0	0	0	0	0	0	1	148	0	0	151
	5:15 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	155	0	0	156
	5:20 PM	2	0	0	0	0	0	0	0	0	0	0	0	3	146	0	0	151
	5:25 PM	2	0	0	0	0	0	0	0	0	0	0	0	1	154	0	0	157
	<b>Total</b>		<b>16</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>14</b>	<b>1726</b>	<b>0</b>	<b>0</b>	<b>1756</b>



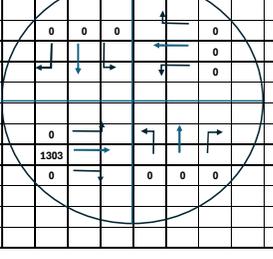
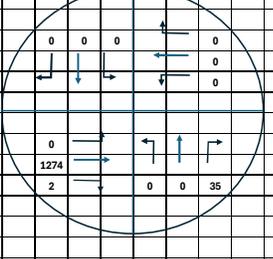
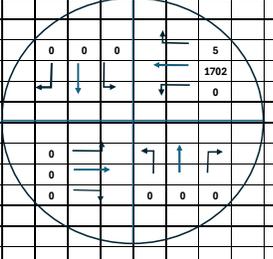
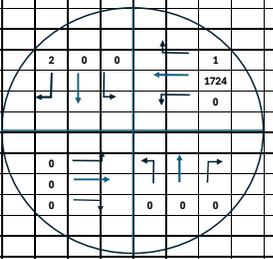
Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Driveway #30	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total		
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U			
Auto Detail Dwy ,Adair St	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	154	0	0	154	
	4:35 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	145	0	0	145	
	4:40 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	125	0	0	125	
	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	146	0	0	146	
	4:50 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	146	0	0	146	
	4:55 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	124	0	0	124	
	5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	138	0	0	139		
	5:05 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	156	0	0	156		
	5:10 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	145	0	0	146		
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	157	0	0	157		
	5:20 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	146	0	0	146		
	5:25 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	157	0	0	157		
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>1739</b>	<b>0</b>	<b>0</b>	<b>1741</b>		
Driveway #29	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total		
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U			
		4:30 PM	1	0	0	0	0	0	0	0	0	0	0	0	1	155	0		0	157
		4:35 PM	2	0	0	0	0	0	0	0	0	0	0	0	3	144	0		0	149
		4:40 PM	3	0	0	0	0	0	0	0	0	0	0	0	1	123	0		0	127
		4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	143	0		0	145
		4:50 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	149	0		0	150
		4:55 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	124	0		0	124
		5:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	2	140	0		0	144
		5:05 PM	1	0	0	0	0	0	0	0	0	0	0	0	1	156	0		0	158
		5:10 PM	1	0	0	0	0	0	0	0	0	0	0	0	2	141	0		0	144
		5:15 PM	4	0	0	0	0	0	0	0	0	0	0	0	0	150	0		0	154
5:20 PM	2	0	0	0	0	0	0	0	0	0	0	0	2	146	0	0	150			
5:25 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	157	0	0	158			
<b>Total</b>		<b>17</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>15</b>	<b>1728</b>	<b>0</b>	<b>0</b>	<b>1760</b>		
Driveway #27	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total		
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U			
		4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	150	0		0	150
		4:35 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	144	0		0	144
		4:40 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	117	0		0	117
		4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	144	0		0	144
		4:50 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	145	0		0	145
		4:55 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	119	0		0	119
		5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	141	0		0	141
		5:05 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	156	0		0	157
		5:10 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	139	0		0	139
		5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	148	0		0	148
5:20 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	150	0	0	150			
5:25 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	148	0	0	148			
<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1701</b>	<b>0</b>	<b>0</b>	<b>1702</b>		
Driveway #28	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total		
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U			
		4:30 PM	3	0	0	0	0	0	0	0	0	0	0	0	1	150	0		0	154
		4:35 PM	1	0	0	0	0	0	0	0	0	0	0	0	5	144	0		0	150
		4:40 PM	4	0	0	0	0	0	0	0	0	0	0	0	1	117	0		0	122
		4:45 PM	2	0	0	0	0	0	0	0	0	0	0	0	1	144	0		0	147
		4:50 PM	2	0	0	0	0	0	0	0	0	0	0	0	2	145	0		0	149
		4:55 PM	2	0	0	0	0	0	0	0	0	0	0	0	1	119	0		0	122
		5:00 PM	4	0	0	0	0	0	0	0	0	0	0	0	0	141	0		0	145
		5:05 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	156	0		0	157
		5:10 PM	5	0	0	0	0	0	0	0	0	0	0	0	1	139	0		0	145
		5:15 PM	2	0	0	0	0	0	0	0	0	0	0	0	1	148	0		0	151
5:20 PM	3	0	0	0	0	0	0	0	0	0	0	0	2	150	0	0	155			
5:25 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	148	0	0	150			
<b>Total</b>		<b>30</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>16</b>	<b>1701</b>	<b>0</b>	<b>0</b>	<b>1747</b>		



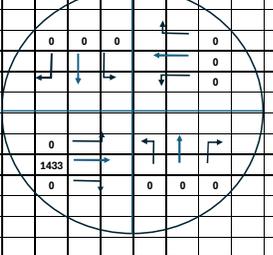
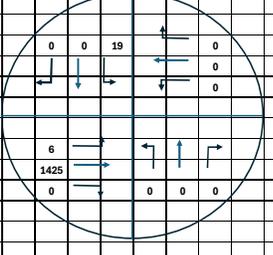
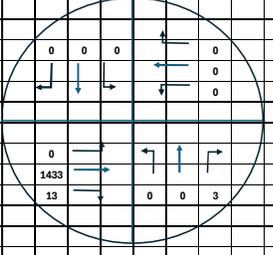
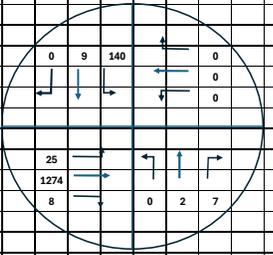
Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Driveway #	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total			
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U				
Jose's Tires Dwy (West) _Adair St	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	155	0	0	155
	4:35 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	147	0	0	147
	4:40 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	125	1	0	126
	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	145	0	0	145
	4:50 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	151	0	0	151
	4:55 PM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	121	0	0	122
	5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	141	0	0	141
	5:05 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	156	0	0	156
	5:10 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	139	0	0	139
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	151	0	0	151
	5:20 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	149	0	0	149
	5:25 PM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	144	0	0	145
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1724</b>	<b>1</b>	<b>0</b>
Jose's Tires Dwy (East) _Adair St	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	154	0	0	154
	4:35 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	146	0	0	146
	4:40 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	122	1	0	123
	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	145	2	0	147
	4:50 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	148	0	0	148
	4:55 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	120	0	0	120
	5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	142	0	0	142
	5:05 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	150	0	0	150
	5:10 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	137	0	0	137
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	148	0	0	148
	5:20 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	148	0	0	148
	5:25 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	142	2	0	144
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1702</b>	<b>5</b>	<b>0</b>
Umpqua Bank Dwy _OR 8	4:30 PM	0	0	1	0	0	0	0	0	0	0	0	0	120	0	0	0	0	0	0	121
	4:35 PM	0	0	1	0	0	0	0	0	0	0	0	0	105	0	0	0	0	0	0	106
	4:40 PM	0	0	3	0	0	0	0	0	0	0	0	0	122	0	0	0	0	0	0	125
	4:45 PM	0	0	2	0	0	0	0	0	0	0	0	0	110	0	0	0	0	0	0	112
	4:50 PM	0	0	4	0	0	0	0	0	0	0	0	0	117	2	0	0	0	0	0	123
	4:55 PM	0	0	3	0	0	0	0	0	0	0	0	0	82	0	0	0	0	0	0	85
	5:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	92	0	0	0	0	0	0	95
	5:05 PM	0	0	5	0	0	0	0	0	0	0	0	0	90	0	0	0	0	0	0	95
	5:10 PM	0	0	3	0	0	0	0	0	0	0	0	0	112	0	0	0	0	0	0	115
	5:15 PM	0	0	3	0	0	0	0	0	0	0	0	0	106	0	0	0	0	0	0	109
	5:20 PM	0	0	3	0	0	0	0	0	0	0	0	0	101	0	0	0	0	0	0	104
	5:25 PM	0	0	4	0	0	0	0	0	0	0	0	0	117	0	0	0	0	0	0	121
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>35</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1274</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
Alery's Automotive Dwy _OR 8	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	119	0	0	0	0	0	0	119
	4:35 PM	0	0	0	0	0	0	0	0	0	0	0	0	103	0	0	0	0	0	0	103
	4:40 PM	0	0	0	0	0	0	0	0	0	0	0	0	125	0	0	0	0	0	0	125
	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	112	0	0	0	0	0	0	112
	4:50 PM	0	0	0	0	0	0	0	0	0	0	0	0	121	0	0	0	0	0	0	121
	4:55 PM	0	0	0	0	0	0	0	0	0	0	0	0	89	0	0	0	0	0	0	89
	5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	95	0	0	0	0	0	0	95
	5:05 PM	0	0	0	0	0	0	0	0	0	0	0	0	92	0	0	0	0	0	0	92
	5:10 PM	0	0	0	0	0	0	0	0	0	0	0	0	108	0	0	0	0	0	0	108
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	113	0	0	0	0	0	0	113
	5:20 PM	0	0	0	0	0	0	0	0	0	0	0	0	109	0	0	0	0	0	0	109
	5:25 PM	0	0	0	0	0	0	0	0	0	0	0	0	117	0	0	0	0	0	0	117
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1303</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>



Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Intersection	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
		Yew St_OR 8	4:30 PM	0	0	0	0	14	3	0	0	2	116	0	0	0	0	
4:35 PM	0		1	0	0	13	0	0	0	3	101	0	0	0	0	0	0	118
4:40 PM	0		0	2	0	17	0	0	0	4	122	1	0	0	0	0	0	146
4:45 PM	0		0	0	0	13	0	0	0	1	107	0	0	0	0	0	0	121
4:50 PM	0		0	1	0	12	0	0	0	4	120	0	0	0	0	0	0	137
4:55 PM	0		0	0	0	7	0	0	0	1	92	1	0	0	0	0	0	101
5:00 PM	0		0	0	0	9	0	0	0	3	88	4	0	0	0	0	0	104
5:05 PM	0		1	2	0	8	2	0	0	2	88	0	0	0	0	0	0	103
5:10 PM	0		0	1	0	10	1	0	0	0	107	0	0	0	0	0	0	119
5:15 PM	0		0	0	0	9	0	0	0	0	114	0	0	0	0	0	0	123
5:20 PM	0		0	1	0	11	2	0	0	2	107	1	0	0	0	0	0	124
5:25 PM	0		0	0	0	17	1	0	0	3	112	1	0	0	0	0	0	134
<b>Total</b>			<b>0</b>	<b>2</b>	<b>7</b>	<b>0</b>	<b>140</b>	<b>9</b>	<b>0</b>	<b>0</b>	<b>25</b>	<b>1274</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
Eagle Crest Dwy_OR 8	4:30 PM	0	0	0	0	0	0	0	0	0	127	1	0	0	0	0	0	128
	4:35 PM	0	0	0	0	0	0	0	0	0	117	1	0	0	0	0	0	118
	4:40 PM	0	0	0	0	0	0	0	0	0	142	1	0	0	0	0	0	143
	4:45 PM	0	0	0	0	0	0	0	0	0	120	1	0	0	0	0	0	121
	4:50 PM	0	0	0	0	0	0	0	0	0	134	1	0	0	0	0	0	135
	4:55 PM	0	0	0	0	0	0	0	0	0	102	0	0	0	0	0	0	102
	5:00 PM	0	0	0	0	0	0	0	0	0	99	2	0	0	0	0	0	101
	5:05 PM	0	0	1	0	0	0	0	0	0	98	1	0	0	0	0	0	100
	5:10 PM	0	0	0	0	0	0	0	0	0	118	1	0	0	0	0	0	119
	5:15 PM	0	0	1	0	0	0	0	0	0	126	1	0	0	0	0	0	128
	5:20 PM	0	0	1	0	0	0	0	0	0	122	3	0	0	0	0	0	126
	5:25 PM	0	0	0	0	0	0	0	0	0	128	0	0	0	0	0	0	128
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1433</b>	<b>13</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
7-Eleven Dwy_OR 8	4:30 PM	0	0	0	0	1	0	0	0	1	127	0	0	0	0	0	0	129
	4:35 PM	0	0	0	0	1	0	0	0	0	114	0	0	0	0	0	0	115
	4:40 PM	0	0	0	0	1	0	0	0	0	142	0	0	0	0	0	0	143
	4:45 PM	0	0	0	0	3	0	0	0	0	120	0	0	0	0	0	0	123
	4:50 PM	0	0	0	0	0	0	0	0	1	135	0	0	0	0	0	0	136
	4:55 PM	0	0	0	0	2	0	0	0	0	102	0	0	0	0	0	0	104
	5:00 PM	0	0	0	0	3	0	0	0	0	99	0	0	0	0	0	0	102
	5:05 PM	0	0	0	0	2	0	0	0	1	97	0	0	0	0	0	0	100
	5:10 PM	0	0	0	0	0	0	0	0	2	117	0	0	0	0	0	0	119
	5:15 PM	0	0	0	0	1	0	0	0	0	123	0	0	0	0	0	0	124
	5:20 PM	0	0	0	0	2	0	0	0	0	122	0	0	0	0	0	0	124
	5:25 PM	0	0	0	0	3	0	0	0	1	127	0	0	0	0	0	0	131
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>19</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>6</b>	<b>1425</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
Kim's Tae Kwon Do Dwy_OR 8	4:30 PM	0	0	0	0	0	0	0	0	0	129	0	0	0	0	0	0	129
	4:35 PM	0	0	0	0	0	0	0	0	0	116	0	0	0	0	0	0	116
	4:40 PM	0	0	0	0	0	0	0	0	0	146	0	0	0	0	0	0	146
	4:45 PM	0	0	0	0	0	0	0	0	0	123	0	0	0	0	0	0	123
	4:50 PM	0	0	0	0	0	0	0	0	0	134	0	0	0	0	0	0	134
	4:55 PM	0	0	0	0	0	0	0	0	0	98	0	0	0	0	0	0	98
	5:00 PM	0	0	0	0	0	0	0	0	0	102	0	0	0	0	0	0	102
	5:05 PM	0	0	0	0	0	0	0	0	0	96	0	0	0	0	0	0	96
	5:10 PM	0	0	0	0	0	0	0	0	0	114	0	0	0	0	0	0	114
	5:15 PM	0	0	0	0	0	0	0	0	0	123	0	0	0	0	0	0	123
	5:20 PM	0	0	0	0	0	0	0	0	0	123	0	0	0	0	0	0	123
	5:25 PM	0	0	0	0	0	0	0	0	0	129	0	0	0	0	0	0	129
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1433</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>



Driveway and Intersection Counts  
 PM Peak Hour: 4:30PM - 5:30PM  
 (November 6, 2024)

Driveway #12	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
Heidi's Dwy_OR 8	4:30 PM	0	0	0	0	0	0	0	0	0	130	0	0	0	0	0	0	130
	4:35 PM	0	0	0	0	0	0	0	0	0	113	0	0	0	0	0	0	113
	4:40 PM	0	0	0	0	0	0	0	0	0	140	0	0	0	0	0	0	140
	4:45 PM	0	0	0	0	0	0	0	0	0	121	0	0	0	0	0	0	121
	4:50 PM	0	0	0	0	0	0	0	0	0	128	0	0	0	0	0	0	128
	4:55 PM	0	0	0	0	0	0	0	0	0	96	0	0	0	0	0	0	96
	5:00 PM	0	0	0	0	0	0	0	0	0	103	0	0	0	0	0	0	103
	5:05 PM	0	0	0	0	0	0	0	0	0	100	0	0	0	0	0	0	100
	5:10 PM	0	0	0	0	0	0	0	0	0	111	0	0	0	0	0	0	111
	5:15 PM	0	0	0	0	0	0	0	0	0	125	0	0	0	0	0	0	125
	5:20 PM	0	0	0	0	0	0	0	0	0	122	0	0	0	0	0	0	122
	5:25 PM	0	0	0	0	0	0	0	0	0	126	0	0	0	0	0	0	126
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1415</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
Driveway #13	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
Subway Dwy_OR 8	4:30 PM	0	0	0	0	0	0	0	0	2	127	0	0	0	0	0	0	129
	4:35 PM	0	0	0	0	2	0	0	0	1	114	0	0	0	0	0	0	117
	4:40 PM	0	0	0	0	2	0	0	0	3	139	0	0	0	0	0	0	144
	4:45 PM	0	0	0	0	3	0	0	0	1	123	0	0	0	0	0	0	127
	4:50 PM	0	0	0	0	1	0	0	0	1	136	0	0	0	0	0	0	138
	4:55 PM	0	0	0	0	2	0	0	0	1	106	0	0	0	0	0	0	109
	5:00 PM	0	0	0	0	1	0	0	0	3	100	0	0	0	0	0	0	104
	5:05 PM	0	0	0	0	0	0	0	0	1	98	0	0	0	0	0	0	99
	5:10 PM	0	0	0	0	0	0	0	0	0	115	0	0	0	0	0	0	115
	5:15 PM	0	0	0	0	1	0	0	0	2	121	0	0	0	0	0	0	124
	5:20 PM	0	0	0	0	1	0	0	0	1	122	0	0	0	0	0	0	124
	5:25 PM	0	0	0	0	1	0	0	0	1	123	0	0	0	0	0	0	125
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>14</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>17</b>	<b>1424</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
Driveway #14	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
Taco Bell Dwy_OR 8	4:30 PM	0	0	0	0	0	2	0	0	1	125	0	0	0	0	0	0	128
	4:35 PM	0	0	0	0	0	0	0	0	2	114	0	0	0	0	0	0	116
	4:40 PM	0	0	0	0	1	0	0	0	3	138	0	0	0	0	0	0	142
	4:45 PM	0	0	0	0	0	0	0	0	2	122	0	0	0	0	0	0	124
	4:50 PM	0	0	0	0	1	0	0	0	3	130	0	0	0	0	0	0	134
	4:55 PM	0	0	0	0	1	0	0	0	1	103	0	0	0	0	0	0	105
	5:00 PM	0	0	0	0	0	0	0	0	1	101	0	0	0	0	0	0	102
	5:05 PM	0	0	0	0	1	0	0	0	2	97	0	0	0	0	0	0	100
	5:10 PM	0	0	0	0	2	0	0	0	0	114	0	0	0	0	0	0	116
	5:15 PM	0	0	0	0	1	0	0	0	1	121	0	0	0	0	0	0	123
	5:20 PM	0	0	0	0	4	0	0	0	0	126	0	0	0	0	0	0	130
	5:25 PM	0	0	0	0	0	0	0	0	2	119	0	0	0	0	0	0	121
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>13</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>18</b>	<b>1410</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
Intersection	5 Min Count Period Beginning At	Northbound				Southbound				Eastbound				Westbound				Total
		Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
S 1st Ave_OR 8	4:30 PM	0	0	8	0	0	0	0	0	0	124	3	0	0	0	0	0	135
	4:35 PM	0	0	0	0	0	0	0	0	0	108	6	0	0	0	0	0	114
	4:40 PM	0	0	4	0	0	0	0	0	0	136	3	0	0	0	0	0	143
	4:45 PM	0	0	0	0	0	0	0	0	0	117	5	0	0	0	0	0	122
	4:50 PM	0	0	0	0	0	0	0	0	0	128	3	0	0	0	0	0	131
	4:55 PM	0	0	3	0	0	0	0	0	0	99	5	0	0	0	0	0	107
	5:00 PM	0	0	0	0	0	0	0	0	0	100	1	0	0	0	0	0	101
	5:05 PM	0	0	4	0	0	0	0	0	0	96	2	0	0	0	0	0	102
	5:10 PM	0	0	1	0	0	0	0	0	0	111	5	0	0	0	0	0	117
	5:15 PM	0	0	1	0	0	0	0	0	0	120	2	0	0	0	0	0	123
	5:20 PM	0	0	0	0	0	0	0	0	0	127	3	0	0	0	0	0	130
	5:25 PM	0	0	1	0	0	0	0	0	0	116	3	0	0	0	0	0	120
	<b>Total</b>		<b>0</b>	<b>0</b>	<b>22</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1382</b>	<b>41</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

