

CITY COUNCIL MONTHLY MEETING CALENDAR

March-16							
Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	
		1	Municipal Court	2	3	4	5
					EDC Noon	S&CC 1st Friday	
		CCI 5:30pm		National League or Cities (NLC) Congressional Conference, Washington, DC			
6	7	8	9	10	11	12	
Planning Comm 7pm	Red Cross Blood Drive 1pm - 6pm - Comm Aud Library 6:30pm	MPAC 5pm		PAC 5pm Ford Leadership	WC Mayors Ford Leadership	Chamber Auction 5:30pm Ford Leadership	
National League or Cities (NLC) Congressional Conference, Washington, DC							
13	14	15	16	17	18	19	
	CITY COUNCIL 5:30 PM - WORK SESSION (Council Goals) 6:00 PM - WORK SESSION (Retirement Plan) 7:00 PM - REGULAR MEETING COMMUNITY AUDITORIUM	Fernhill Wetlands 5:30pm	Municipal Court P&R (CAC 6:30pm) CFC 5:15pm CAO 5pm CWAC 5:30pm	Food Film 7:30pm			
20	21	22	23	24	25	26	
Chamber Luncheon FGS&CC Bd Mtg 6:30pm Planning Comm 7pm	HLB 7:15pm	PSAC 7:30am MPAC 5pm	WEA Breakfast Sustainability 6pm				
27	28	29	30	31			
	CITY COUNCIL 6:00 PM - WORK SESSION (Council Team Agmt) 7:00 PM - REGULAR MEETING COMMUNITY AUDITORIUM	Kidd out		CEP Deadline			
Apr-16							
Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	
					Steve Huffman's Party 2pm - 5pm Comm Aud S&CC 1st Friday	1	2
Sister Cities Plant Sale Student Fundraiser through April 28th							
3	4	5	6	7	8	9	
Planning Comm 7pm	CCI 5:30pm	Water Providers EC 5:30pm	EDC Noon	WC Mayors			
10	11	12	13	14	15	16	
	CITY COUNCIL 7:00 PM - REGULAR MEETING COMMUNITY AUDITORIUM	Red Cross Blood Drive 1pm - 6pm - Comm Aud Library 6:30pm	CEPC SPECIAL MEETING CEP PRESENTATIONS 7pm - Community Auditorium	MPAC 5pm	PAC 5pm	If I Were Mayor Deadline	
17	18	19	20	21	22	23	
Chamber Luncheon FGS&CC Bd Mtg 6:30pm Planning Comm 7pm	JOINT WORK SESSION FG CITY COUNCIL CORNELIUS COUNCIL & FG SCHOOL BOARD TBA ?PM - COMMUNITY AUDITORIUM	Fernhill Wetlands 5:30pm	Municipal Court P&R 7am CFC 5:15pm CAO 5pm CWAC 5:30pm	Budget Comm Info Meeting 7pm - Community Auditorium	Food Film 7:30pm	Ford Leadership Ford Leadership	
24	25	26	27	28	29	30	
	CITY COUNCIL 7:00 PM - REGULAR MEETING COMMUNITY AUDITORIUM	HLB 7:15pm	PSAC 7:30am WC Managers' Mtg 1:30pm Comm Aud WCCLS Bd Mtg 1:30pm Comm Aud MPAC 5pm	WEA Breakfast Sustainability 6pm	Kidd out thru May 8		
May-16							
Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	
1	2	3	4	5	6	7	
Planning Comm 7pm	CCI 5:30pm		Municipal Court	Budget Comm 1st Meeting pm - Community Auditorium	EDC Noon	S&CC 1st Friday	
						ORCC Run Hagg Lake	
8	9	10	11	12	13	14	
	CITY COUNCIL TBA ?PM - CEPC SPECIAL MEETING 7:00 PM - REGULAR MEETING COMMUNITY AUDITORIUM	Red Cross Blood Drive 1pm - 6pm - Comm Aud Library 6:30pm	MPAC 5pm	Budget Comm 2nd Meeting 5pm - Community Auditorium	PAC 5pm	LOC Mtg WC Mayors	
Kidd returns							
15	16	17	18	19	20	21	
Chamber Luncheon FGS&CC Bd Mtg 6:30pm Planning Comm 7pm	Fernhill Wetlands 5:30pm		Municipal Court P&R 7am CFC 5:15pm CAO 5pm CWAC 5:30pm	Food Film 7:30pm			
22	23	24	25	26	27	28	
	CITY COUNCIL 7:00 PM - REGULAR MEETING COMMUNITY AUDITORIUM	HLB 7:15pm	PSAC 7:30am MPAC 5pm	WEA Breakfast Sustainability 6pm			
29	30	31					
	CITY HALL CLOSED HOLIDAY Memorial Day Flag Ceremony 12:30pm						

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CITY COUNCIL MEETING AGENDA

MONDAY, MARCH 28, 2016

**6:00 PM – Work Session (Council Team Agreement)
7:00 PM – Regular Meeting**

**Community Auditorium
1915 Main Street
Forest Grove, OR 97116**

Forest Grove City Council Meetings are televised live by Tualatin Valley Community Television (TVCTV) Government Access Programming, Ch 30. To obtain the programming schedule, please contact TVCTV at 503.629.8534 or visit <http://www.tvctv.org/government-programming/government-meetings/forest-grove>.

PETER B. TRUAX, MAYOR

Thomas L. Johnston, Council President
Richard G. Kidd III
Victoria J. Lowe

Ronald C. Thompson
Elena Uhing
Malynda H. Wenzl

All meetings of the City Council are open to the public and all persons are permitted to attend any meeting except as otherwise provided by ORS 192. The public may address the Council as follows:

➔ Public Hearings – Public hearings are held on each matter required by state law or City policy. Anyone wishing to testify should sign in for any Public Hearing prior to the meeting. The presiding officer will review the complete hearing instructions prior to testimony. The presiding officer will call the individual or group by the name given on the sign in form. When addressing the Council, please use the witness table (center front of the room). Each person should speak clearly into the microphone and must state his or her name and give an address for the record. All testimony is electronically recorded. In the interest of time, Public Hearing testimony is limited to three minutes unless the presiding officer grants an extension. Written or oral testimony is heard prior to any Council action.

➔ Citizen Communications – Anyone wishing to address the Council on an issue not on the agenda should sign in for Citizen Communications prior to the meeting. The presiding officer will call the individual or group by the name given on the sign in form. When addressing the Council, please use the witness table (center front of the room). Each person should speak clearly into the microphone and must state his or her name and give an address for the record. All testimony is electronically recorded. In the interest of time, Citizen Communications is limited to two minutes unless the presiding officer grants an extension.

The public may not address items on the agenda unless the item is a public hearing. Routinely, members of the public speak during Citizen Communications and Public Hearings. If you have questions about the agenda or have an issue that you would like to address to the Council, please contact the City Recorder, aruggles@forestgrove-or.gov, 503-992-3235.

City Council meetings are handicap accessible. Assistive Listening Devices (ALD) or qualified sign language interpreters are available for persons with impaired hearing or speech. For any special accommodations, please contact the City Recorder, aruggles@forestgrove-or.gov, 503-992-3235, at least 48 hours prior to the meeting.

Mayor Peter Truax
Jesse VanderZanden, City Manager

6:00

WORK SESSION: CITY COUNCIL TEAM AGREEMENT

The City Council will convene in the Community Auditorium – Conference Room to conduct the above work session(s). The public is invited to attend and observe the work session(s); however, no public comment will be taken. The Council will take no formal action during the work session(s).

Tom Gamble, Parks and Recreation
Director
Mayor Peter Truax
Dan Riordan, Senior Planner

7:00

1. **REGULAR MEETING:** Roll Call and Pledge of Allegiance

1. A. **AWARD PRESENTATIONS:**

- *Steve Huffman, Parks and Recreation Crew Supervisor, 25 Years of Service and Retirement*
- *Steve Huffman, Parks and Recreation Crew Supervisor, Presentation by Community Forestry Commission*

1. B. **PROCLAMATION:**

- *National Mayor’s Challenge for Water Conservation, presenting to Mitch Taylor on behalf of the Sustainability Commission*

2. **CITIZEN COMMUNICATIONS:** Anyone wishing to speak to Council on an item not on the agenda may be heard at this time. *Please sign-in before the meeting on the Citizen Communications form posted in the foyer.* In the interest of time, please limit comments to two minutes. Thank you.

3. **CONSENT AGENDA:** See Page 5

4. **ADDITIONS/DELETIONS:**

5. **PRESENTATIONS:**

Rob Foster, Public Works Director

7:20

5. A.
 - *State of the District, Mark Jockers, Government & Public Affairs, Clean Water Services*

Paul Downey, Administrative
Services Director

7:35

5. B.
 - *Quarterly Financial Report for Period Ending December 31, 2015*

Derek Robbins, Project Engineer
Rob Foster, Public Works Director
Jesse VanderZanden, City Manager

7:50

6. **CONTINUE PUBLIC HEARING FROM MARCH 14, 2015: SECOND READING OF ORDINANCE NO. 2016-02 AMENDING FOREST GROVE TRANSPORTATION SYSTEM PLAN (TSP) FOR INCLUSION OF COUNCIL CREEK REGIONAL TRAIL PREFERRED ALIGNMENT THROUGH THE CITY OF FOREST GROVE; FILE NO. 311-15-00033-PLNG**

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- | | | |
|---|------|--|
| Dan Riordan, Senior Planner
James Reitz, Senior Planner
Jon Holan Community Development
Director
Jesse VanderZanden, City Manager | 7:55 | 7. <u>CONTINUE PUBLIC HEARING FROM MARCH 14, 2016:
SECOND READING OF ORDINANCE NO. 2016-03
AMENDING FOREST GROVE DEVELOPMENT CODE
ARTICLE 3, ARTICLE 4, ARTICLE 7 AND ARTICLE 12
AND DESIGN GUIDELINE HANDBOOK TO IMPLEMENT
POLICIES OF THE FOREST GROVE COMPREHENSIVE
PLAN; FILE NO. 311-15-00020-PLNG</u> |
| Dan Riordan, Senior Planner
James Reitz, Senior Planner
Jon Holan Community Development
Director
Jesse VanderZanden, City Manager | 8:00 | 8. <u>CONTINUE PUBLIC HEARING FROM MARCH 14, 2016:
SECOND READING OF ORDINANCE NO. 2016-04
AMENDING FOREST GROVE COMPREHENSIVE PLAN
MAP FOR PROPERTIES DESIGNATED TOWN CENTER
SUPPORT AND COMMUNITY COMMERCIAL IN THE
TOWN CENTER EXPANSION AREA ON THE
COMPREHENSIVE PLAN DATED JANUARY 27, 2014;
FILE NO. 311-15-00020-PLNG</u> |
| Dan Riordan, Senior Planner
James Reitz, Senior Planner
Jon Holan Community Development
Director
Jesse VanderZanden, City Manager | 8:05 | 9. <u>CONTINUE PUBLIC HEARING FROM MARCH 14, 2016:
SECOND READING OF ORDINANCE NO. 2016-05
AMENDING OFFICIAL FOREST GROVE ZONING MAP
FOR PROPERTIES DESIGNATED NEIGHBORHOOD
MIXED USE AND TOWN CENTER EXPANSION ON THE
COMPREHENSIVE PLAN MAP DATED JANUARY 27,
2014; FILE NO. 311-15-00020-PLNG</u> |
| Dan Riordan, Senior Planner
James Reitz, Senior Planner
Jon Holan Community Development
Director
Jesse VanderZanden, City Manager | 8:10 | 10. <u>CONTINUE PUBLIC HEARING FROM MARCH 14, 2016:
SECOND READING OF ORDINANCE NO. 2016-06
AMENDING FOREST GROVE DEVELOPMENT CODE
ARTICLE 3 TO ESTABLISH THE BUSINESS
INDUSTRIAL PARK ZONE; FILE NO. 311-15-00027-
PLNG</u> |
| Jon Holan Community Development
Director
J. F. Schutz, Police Chief
Jesse VanderZanden, City Manager | 8:15 | 11. <u>CONTINUE PUBLIC HEARING FROM MARCH 14, 2016:
SECOND READING OF ORDINANCE NO. 2016-07
AMENDING FOREST GROVE DEVELOPMENT CODE
ARTICLE 3, ARTICLE 7 AND ARTICLE 8 TO ADDRESS
MARIJUANA FACILITIES; FILE NO. 311-15-00028-PLNG</u> |

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- | | | |
|--|--------------------|--|
| Jon Holan Community Development
Director
J. F. Schutz, Police Chief
Jesse VanderZanden, City Manager | 8:25 | 12. <u>CONTINUE PUBLIC HEARING FROM MARCH 14, 2016:
SECOND READING OF ORDINANCE NO. 2016-08
AMENDING FOREST GROVE CITY CODE CHAPTER 7
BY RENAMING CODE SECTIONS 7.850 THROUGH 7.865
FROM "MEDICAL MARIJUANA DISPENSARIES" TO
"MARIJUANA ACTIVITIES"; AMENDING CODE
SECTIONS 7.850 THROUGH 7.860 TO ADDRESS
MARIJUANA FACILITIES; AND REPEALING PORTIONS
OF ORDINANCE NO. 2015-03; FILE NO. 311-15-00028-
PLNG</u> |
| Dan Riordan, Senior Planner
Jon Holan Community Development
Director
Jesse VanderZanden, City Manager | 8:30 | 13. <u>PUBLIC HEARING AND FIRST READING OF
ORDINANCE NO. 2016-09 AMENDING THE FOREST
GROVE COMPREHENSIVE PLAN MAP TO DESIGNATE
THE SOUTH URBAN GROWTH BOUNDARY ADDITION
AS CAMPUS EMPLOYMENT (600 ELM STREET,
WASHINGTON COUNTY TAX LOTS 1S30000100 AND
1S306D000700); FILE NO. 311-15-00032-PLNG</u> |
| Peter Truax, Mayor
Jesse VanderZanden, City Manager | 8:45 | 14. <u>RESOLUTION NO. 2016-21 ADOPTING CITY COUNCIL
GOALS FOR FISCAL YEAR 2016-17</u> |
| Peter Truax, Mayor
Jesse VanderZanden, City Manager | 8:50 | 15. <u>RESOLUTION NO. 2016-22 ADOPTING CITY COUNCIL
OBJECTIVES FOR FISCAL YEAR 2016-17</u> |
| Peter Truax, Mayor
Jesse VanderZanden, City Manager | 8:55 | 16. <u>RESOLUTION NO. 2016-23 ADOPTING CITY COUNCIL
TEAM AGREEMENT</u> |
| City Councilors | 9:00 | 17. <u>CITY COUNCIL COMMUNICATIONS:</u> |
| Jesse VanderZanden, City Manager | 9:15 | 18. <u>CITY MANAGER'S REPORT:</u> |
| Peter Truax, Mayor | 9:20 | 19. <u>MAYOR'S REPORT:</u> |
| | <u>9:30</u> | 20. <u>ADJOURNMENT:</u> |

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3. **CONSENT AGENDA:** Items under the Consent Agenda are considered routine and will be adopted with a single motion, without separate discussion. Council members who wish to remove an item from the Consent Agenda may do so prior to the motion to approve the item(s). Any item(s) removed from the Consent Agenda will be discussed and acted upon following the approval of the remaining Consent Agenda item(s).
- A. Approve City Council Work Session (Council Goal-Setting and Team Agreement) Meeting Minutes of March 14, 2016.
 - B. Approve City Council Regular Meeting Minutes of March 14, 2016.
 - C. Accept the Historic Landmarks Board Meeting Minutes of February 22, 2016.
 - D. Accept Library Commission Meeting Minutes of January 12 and February 9, 2016.
 - E. Accept Planning Commission Meeting Minutes of February 15, 2016.
 - F. Accept Resignation on Public Arts Commission (Helvi Smith, Term Expiring December 31, 2017).
 - G. **ENDORSE NEW LIQUOR LICENSE APPLICATION (BREWERY PUBLIC HOUSE) FOR RIDGEWALKER BREWING COMPANY, 1921 21ST AVENUE (APPLICANT: JASON CIRLINCIONE).**
 - H. **ENDORSE LIQUOR LICENSE RENEWAL APPLICATIONS FOR YEAR 2016:**
 - 1. Thai House 2 (Limited On-Premises Sales)
 - 2. Urban Decanter (Full On-Premises Sales)

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<i>CITY RECORDER USE ONLY:</i>	
AGENDA ITEM #:	14, 15 & 16
FINAL ACTION:	

CITY COUNCIL STAFF REPORT

WORK SESSION

TO: City Council

FROM: Jesse VanderZanden, City Manager

MEETING DATE: March 28, 2016

PROJECT TEAM: Jesse VanderZanden, City Manager, and Anna Ruggles, CMC, City Recorder

SUBJECT TITLE: Approve Council Goals and Objectives for FY 2016-17 and Council Team Agreement

ACTION REQUESTED: Ordinance Order Resolution Motion Informational

X all that apply

BACKGROUND:

The purpose of this report is to request that Council approve their Council Key Goals and Council Objectives for FY 2016-17 (Attachment A) and adopt their Council Team Agreement (Attachment B). Council met in Work Sessions on February 20 and March 14 and will continue their discussion at the work session scheduled on Monday, March 28, 2016.

Goals and Objectives for FY 2016-17: Pursuant to City Council Rules of Procedure, Section 16, the City Council shall adopt its Key Council Goals and Council Objectives no later than second regular meeting in March of each year. The Council proposed 12 new Objectives and carried over 13 Objectives from the previous year that were identified as ongoing for a total of 25 Objectives for Fiscal Year 2016-17 (Attachment A). Once Council adopts its Goals and Objectives, the City Manager will assign the Goals and Objectives to departments and departments will incorporate any significant Council Goals and Objectives into their work plans and make presentations to Council at a later date.

Team Agreement: A draft copy of the Council Team Agreement (Attachment B) contains the proposed amendments that Council collectively concurred to include at previous work sessions. Council may:

- Approve as submitted;
- Suggest edits and approve as amended; or
- Not approve and provide further direction.

STAFF RECOMMENDATION:

Staff recommends Council approve the attached resolutions adopting the Council Key Goals, Council Objectives for FY2016-17 (Attachment A), and Council Team Agreement (Attachment B).

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RESOLUTION NO. 2016-21**RESOLUTION OF THE CITY OF FOREST GROVE CITY COUNCIL
ADOPTING CITY COUNCIL GOALS FOR FISCAL YEAR 2016-17**

WHEREAS, pursuant to City Council Rules of Procedure, Section 16, the City Council must set its Council Goals annually; and

WHEREAS, the Forest Grove City Council held an Annual Town Meeting on January 30, 2016; and

WHEREAS, the Forest Grove City Council and Management Team met in Goal-Setting Work Session on February 20, March 14 and March 28, 2016, to set goals for Fiscal Year 2016-17; and

WHEREAS, the City Council has determined three goals for Fiscal Year 2016-17.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FOREST GROVE AS FOLLOWS:

Section 1. That the City of Forest Grove City Council hereby adopts the City Council Goals for Fiscal Year 2016-17 as follows (Exhibit A):

- GOAL 1 Promote Safe, Livable, and Sustainable Neighborhoods
and a Prosperous Dynamic, Green City**
- GOAL 2 Promote a Prudent Financial Plan to Maintain Effective
Service Levels of a Full-Service City**
- GOAL 3 Promote the Interests and Needs of Forest Grove
in Local, State, and National Affairs**

Section 2. This resolution is effective immediately upon its enactment by the City Council.

PRESENTED AND PASSED this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor



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CITY COUNCIL GOALS 2016-17

**Promote Safe, Livable and
Sustainable Neighborhoods and
a Prosperous Dynamic, Green City**

**Promote a Prudent Financial Plan to
Maintain Effective Service Levels
of a Full-Service City**

**Promote the Interests and Needs of
Forest Grove in Local, State, and
National Affairs**

RESOLUTION NO. 2016-22**RESOLUTION OF THE CITY OF FOREST GROVE CITY COUNCIL
ADOPTING CITY COUNCIL GOAL OBJECTIVES FOR FISCAL YEAR 2016-17**

WHEREAS, the City Council adopted three key Council Goals for Fiscal Year 2016-17 pursuant to Resolution No. 2016-21; and

WHEREAS, the City Council and Management Team met in Council Goal Setting Work Sessions on February 20, March 14 and March 28, 2016, to set goal objectives for Fiscal Year 2016-17; and

WHEREAS, the City Council has identified the City Council Goal Objectives for Fiscal Year 2016-17 as attached in Exhibit A.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FOREST GROVE AS FOLLOWS:

Section 1. That the City of Forest Grove City Council hereby adopts the City Council Goal Objectives for Fiscal Year 2016-17 as attached in Exhibit A.

Section 2. This resolution is effective immediately upon its enactment by the City Council.

PRESENTED AND PASSED this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor



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FOREST GROVE CITY COUNCIL

GOALS AND OBJECTIVES

City Council has proposed a set of goals and priority objectives for Fiscal Year 2016-17. Once Council adopts the Goals and Objectives, the City Manager shall assign to departments and the objectives become part of the departments' work plans. For Fiscal Year 2016-17, the Council is proposing to carry over 13 objectives and identified 12 new proposed objectives (# shown in **Bold**) for a total of 25 objectives.

GOAL 1: PROMOTE SAFE, LIVABLE AND SUSTAINABLE NEIGHBORHOODS AND A PROSPEROUS DYNAMIC, GREEN CITY

	Actions:	Budget:	Time Frame:		Depts:	COUNCIL DIRECTION <i>Proposed</i>	STATUS <i>Proposed Next Steps</i>
			Needs Direction	Council Action			
1.1 Acquisition Process for Land Parcels in Watershed Drainage Areas	◆	\$	■		PW; Admin	Reserve a portion of timber sales for acquisition of land parcels in watershed drainage areas	<i>Develop land acquisition process. Determine percentage of reserves as part of budget process FY2016-17.</i>
1.2 Energy Reduction Programs	◆	\$	■		L&P; PW	1) Residential rebate programs for high efficiency toilets and solar; 2) Continue LED replacement project; and 3) Conduct a wind analysis in watershed	<i>Continue energy conservation efforts and LED replacement project. Conduct analysis's for residential rebate programs for high efficiency toilets and solar. Conduct a wind analysis in watershed.</i>
1.3 Industrial Area Planning	◆	\$	■		CD; ECD; L&E; L&P	Complete feasibility study for industrial area site planning	<i>Continue work to certify readiness of industrial sites and complete area planning</i>
1.4 Multiplex Ballfields		\$	■		P&R; L&E	Determine economic feasibility for multiplex ballfields	<i>Conduct economic feasibility analysis and/or incorporate concept analysis as part of Parks Recreation and Open Space Master Plan</i>

Resolution No. 2016-22, Exhibit A

GOAL 1 (continued)		Actions:		Budget:	Time Frame:		Depts:	COUNCIL DIRECTION Proposed	STATUS Next Steps
		Needs Direction	Council Action	Fiscal Impact	1-3 Years	4+ Years	Assigned		
1.5	Neighborhood Watch Program			\$	■		PD	Draft a map showing established neighborhoods and Annual Report	Prepare a map showing established neighborhoods. Draft 1-2 page annual executive summary report.
1.6	Parks Recreation and Open Space Master Plan		◆	\$	■		P&R	Continue efforts and draft brochure listing parks/open spaces by location	Continue efforts. Prepare brochure listing parks/open spaces by location. Submit final master plan recommendation to Council.
1.7	Planned Residential Developments		◆		■		CD	Draft simple flow chart for PRD process	Draft a simple flow chart showing the steps of the process in sequential order for PRD.
1.8	Plaza Downtown		◆	\$		■	CD; L&E	Identify concept location	Incorporate concept analysis into URA downtown storefront revitalization program.
1.9	Rogers Park Upgrades		◆	\$	■		P&R	Complete renovation efforts in FY16-17	Issue an RFP for renovations including, but not limited to, new restroom facilities and playground equipment.
1.10	Staff Succession Planning				■		Admin; L&E	Draft succession planning guidelines for staff	Draft 1-2 page executive summary report outlining procedural guidelines for staff succession planning efforts.
1.11	Urban Renewal Agency Downtown Storefront Revitalization Program		◆	\$	■		CD; ECD; Eng	Draft downtown storefront revitalization program	Draft downtown storefront revitalization program. Hold URA work session.

GOAL 2: PROMOTE A PRUDENT FINANCIAL PLAN TO MAINTAIN EFFECTIVE SERVICE LEVELS OF A FULL-SERVICE CITY

		Actions:		Budget:	Time Frame:		Dept	COUNCIL DIRECTION <i>Proposed</i>	STATUS <i>Next Steps</i>
		Needs Direction	Council Action	Fiscal Impact	1-3 Years	4+ Years	Assigned		
2.12	Capital Improvement Projects		◆	\$	■		PW; L&E; Admin	Develop a list and financial plan/policy for CIP Projects	<i>Draft 1-2 page executive summary report listing long-term CIP projects. Draft a financial plan/policy.</i>
2.13	City Full-Service Definition		◆		■		L&E; Admin; CD	Annual Town Meeting (ATM) topic and format	<i>Schedule joint work session with Committee for Citizen Involvement regarding ATM format and topic.</i>
2.14	Community Services Local Option Levy Renewal		◆	\$	■		Admin; L&E; FD; PD; Lib; P&R	Preparation for local option levy renewal	<i>Draft 1-2 page executive summary report outlining process and potential election dates for submitting local option levy to voters and schedule Council work session discussion.</i>
2.15	Fire Authority IGA		◆	\$	■		FD; Admin; L&E	Draft Fire Authority IGA. Compare costs to TVF&R.	<i>Draft Fire Authority IGA. Compare costs to TVF&R. Continue work session discussions with Council.</i>
2.16	Forest Grove Senior and Community Center		◆	\$	■		Admin; L&E	Review Senior Center financial request as part of budget process FY2016-17	<i>Review Senior Center financial request as part of budget process FY2016-17.</i>
2.17	Police Department Facility		◆	\$	■		Admin; L&E; PD	Continue request for proposal (RFP) for planning and design	<i>Release RFP. Continue work session discussions with Council.</i>

GOAL 3: PROMOTE THE INTERESTS AND NEEDS OF FOREST GROVE IN LOCAL, STATE, AND NATIONAL AFFAIRS

		Actions:		Budget:	Time Frame:		Dept	<i>COUNCIL DIRECTION Proposed</i>	<i>STATUS Next Steps</i>
		Needs Direction	Council Action	Fiscal Impact	1-3 Years	4+ Years	Assigned		
3.18	Affordable Housing				■		CD	1) Participate in local and regional efforts; 2) draft white paper; and 3) review code for tiny houses	<i>Draft white paper for Council review that outlines city's efforts.</i>
3.19	City Charter Review	◆	◆	\$	■		Admin; L&E	Conduct Charter review	<i>Hold work session for the purpose of seeking direction from Council.</i>
3.20	City Success Stories				■		Admin; L&E	Boast City success stories	<i>Review and revamp City website.</i>
3.21	Council Academy	◆	◆	\$	■		L&E	Discuss viability of a Council Academy	<i>Hold work session for the purpose of seeking content and concept from Council.</i>
3.22	Fernhill Wetlands Research & Education Center		◆	\$	■		Admin; L&E	City anticipated funding participation	<i>Initiate joint meeting with Clean Water Services to discuss feasibility and next steps.</i>
3.23	Legislative Priorities; Local, State, Regional & National			\$	■		L&E	Continue supporting legislative-related efforts	<i>Continue supporting legislative-related efforts.</i>
3.24	Tourism			\$	■		ECD; L&E	Enhance collaboration efforts with FG/Cornelius Chamber	<i>Establish a private-public work group for tourism collaboration efforts with FG/Cornelius Chamber.</i>
3.25	Transportation		◆	\$	■		CD; Eng	1) List of strategically important transportation projects; and 2) Economic feasibility of transferring county roads to city jurisdiction	<i>Draft list of strategically important transportation projects. Conduct economic feasibility analysis for transferring county roads to city jurisdiction.</i>

RESOLUTION NO. 2016-23**RESOLUTION OF THE CITY OF FOREST GROVE
ADOPTING COUNCIL TEAM AGREEMENT**

WHEREAS, the Forest Grove City Council is the duly elected governing body for the City of Forest Grove; and

WHEREAS, the City Charter, Section 10, specifies that the City Council shall determine its own set of rules to govern its meetings and proceedings beyond those specified in the Charter; and

WHEREAS, the Council Rules of Procedures, Section 5, specifies the rules of decorum and order; and

WHEREAS, the City Council met in Council Work Sessions on February 20, March 14 and March 28, 2016, and collectively reviewed and discussed their Council Team Agreement; and

WHEREAS, the City Council desires to adopt their Council Team Agreement attached as Exhibit A.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FOREST GROVE AS FOLLOWS:

Section 1. That the City of Forest Grove City Council hereby adopts their City Council Team Agreement attached as Exhibit A.

Section 2. This resolution is effective immediately upon its enactment by the City Council.

PRESENTED AND PASSED this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

CITY COUNCIL TEAM AGREEMENT

Pursuant to Resolution No. 2016-23, the Agreements for Conducting Council Meetings and Business

1. Attendance at Council meetings is first priority; if unable to attend, please contact the City Recorder.
2. Be on time to meetings and read the packet prior to the meeting - be prepared to work.
3. Mayor will take the lead in keeping the meeting and discussion focused.
4. Distribute information in advance of Council discussion.
5. Mayor will recognize Councilors when indicating they wish to speak.
6. Put a time limit on audience testimony and ask them not to repeat previous speakers.
7. Use formal procedure (point of order, call for question, etc.) to focus the meeting. Formal procedure may be used when necessary for effective discussion in lieu of Council's usual, more informal, process. Individuals should use procedure appropriately and courteously.
8. Council meetings are televised live; this requires Council to act professionally by:
 - Treating the public and each other with courtesy;
 - Speaking in turn and on the issue;
 - No interrupting;
 - No engaging in side conversations; and
 - No use of personal communication electronic devices while conducting business at the dais.
9. Refrain from personal attacks, including to presenters, staff, and Council.
10. Agree to be diplomatic about disagreement; leave disagreement at the dais and do not try to polarize other Councilors.
11. Call the City Manager or designee with questions and requests prior to the meeting.
12. Information available to one council member will be available to all, in a timely manner.
13. Every effort shall be made to adjourn meetings by 9:30 p.m. Council recess shall be called if meeting extends past 9:30 p.m.
14. Councilors may request on their own accord one-on-one meetings with the Mayor.

Individual Council Member Conduct Agreements

Council Members agree to:

- Be straightforward about goals and issues.
- Cultivate exchange of views with other councilors.
- Avoid saying or doing anything that would discredit or harm the City.

Commitments as a Council

Council strives to:

- Continue to improve citizen involvement, awareness and participation.
- Improve follow-up and resolution of citizen concerns or complaints.
- Act as an advocate for the City.

Reviewed and Adopted by City Council on March 28, 2016

Signed:

- Concur
- Non-concur

Peter B. Truax, Mayor Date: _____

- Concur
- Non-concur

Thomas L. Johnston, Council President Date: _____

- Concur
- Non-concur

Richard G. Kidd III, Councilor Date: _____

- Concur
- Non-concur

Victoria J. Lowe, Councilor Date: _____

- Concur
- Non-concur

Ronald C. Thompson, Councilor Date: _____

- Concur
- Non-concur

Elena Uhing, Councilor Date: _____

- Concur
- Non-concur

Malynda H. Wenzl, Councilor Date: _____

CERTIFICATE OF RETIREMENT

Awarded to

Steven P. Huffman

Parks Supervisor

**In Recognition of 25 Dedicated Years of Service to the
City of Forest Grove and Citizens of Forest Grove**

The Forest Grove City Council sincerely thanks you for your exceptional employment and years of service and is so honored to have had you as an employee of the City of Forest Grove since 1991.



*Peter B. Truax, Mayor
Forest Grove City Council
March 28, 2016*

PROCLAMATION

NATIONAL MAYOR'S CHALLENGE FOR WATER CONSERVATION Month of April 2016

WHEREAS, the City of Forest Grove ("City") and the State of Oregon continue to explore ways to manage residential consumption of water and power, and to inspire its residents to explore ways to manage residential consumption of water and power; and

WHEREAS, cities can engage in efforts to inspire their own communities, as well as their neighboring cities, to become better environmental stewards; and

WHEREAS, the fourth annual National Mayor's Challenge for Water Conservation presented by the Wyland Foundation and Toyota, with support from the U.S. EPA's Office of Water, National League of Cities, U.S. Forest Service, The Toro Company, Wondergrove Kids, Bytelaunch, and WaterSmart Software is a healthy, non-profit competition for cleaner communities and a water use and pollution reduction competition between our cities; and

WHEREAS, with the encouragement of their Mayors, residents may register their participation online in their city's Challenge by making simple pledges to decrease their water use and to reduce pollution for the period of one year, thereby, assisting their cities to apply State and Federal water conservation strategies and to target mandated reductions.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF FOREST GROVE DOES HEREBY PROCLAIM THE MONTH OF APRIL 2016, AS:

WATER CONSERVATION PLEDGE CHALLENGE MONTH

In Forest Grove, Oregon, and call upon all citizens and local businesses to take the "Wyland National Mayor's Challenge for Water Conservation" during the Month of April 2016, and support the City of Forest Grove's Challenge by making a series of online pledges at www.mywaterpledge.com to conserve water, energy and other natural resources for future generations.



IN WITNESS WHEREOF, I have hereunto set my hand and caused the seal of the City of Forest Grove, Oregon, to be affixed this 28th day of March, 2016.

A handwritten signature in cursive script, appearing to read "Peter B. Truax".

Peter B. Truax, Mayor, City of Forest Grove

3A



A place where families and businesses thrive.

**City Council Work Session Meeting Minutes
Goal-Setting and Team Agreement**

**Monday, March 14, 2016
5:30 p.m., Conference Room**

Minutes are unofficial until approved by Council.

1. CALLED TO ORDER AND ROLL CALL:

Mayor Peter Truax called the Work Session to order at 5:34 p.m.

ROLL CALL: COUNCIL PRESENT: Thomas Johnston, Council President; Richard Kidd; Victoria Lowe; Ronald Thompson; Malynda Wenzl; and Mayor Peter Truax.

COUNCIL ABSENT: Elena Uhing, excused.

STAFF PRESENT: Jesse VanderZanden, City Manager, and Anna Ruggles, City Recorder.

2. WORK SESSION: CITY COUNCIL GOAL-SETTING AND COUNCIL TEAM AGREEMENT

Ruggles and VanderZanden facilitated the above-noted work session, noting the purpose of the work session was so Council could review their proposed goals and objectives for Fiscal Year 2016-17, which were identified at the Council Retreat, held on February 20, 2016. Ruggles and VanderZanden reported the Council made no changes to their three existing key goals and Council identified 12 new proposed objectives and carried over 13 objectives from the previous year that were identified as ongoing for a total of 25 proposed objectives for Fiscal Year 2016-17. In conclusion of the above-noted staff report, Ruggles referenced Exhibit A of the staff report, noting the newly revised spreadsheet contains columns showing Council Direction/Actions Needed; Fiscal Impact, if any; projected Time Frames; Departments Assigned; Council Direction as proposed by Council; and Next Steps and/or Status of each proposed objective.

Council Discussion:

Mayor Truax opened the floor and roundtable discussion ensued, to which Council collectively concurred to make no changes to their three key goals and no changes to the 12 new proposed objectives for Fiscal Year 2016-17. In conclusion of the above-noted discussion, Ruggles and VanderZanden advised proposed resolutions adopting the three Council key goals and Council Objectives for Fiscal Year 2016-17 will be presented for Council consideration at the next regular meeting of Monday, March 28, 2016.

Council Team Agreement:

Ruggles and VanderZanden referenced Council's drafted Team Agreement, noting the Team Agreement was reviewed at the Council Retreat, held on February 20, 2016, and

Council made minor edits as outlined in Attachment B of the staff report. Ruggles and VanderZanden reported staff researched other team agreements as directed at the retreat, noting staff found that most team agreements were very similar to Forest Grove's and/or did not contain improvements that would be useful as part of Council's decision-making process.

Council Discussion:

Mayor Truax opened the floor and roundtable discussion ensued, to which Council collectively concurred to a minor edit/deletion as following: #8. *"No use of personal electric communication devices while conducting business at the dais."* ~~except to send a text message if an urgency arises.~~ However, Lowe warned of potential meeting violations if electronic communication was occurring while at the dais, to which Council collectively concurred that they would leave the dais if the need arises, but that there was no need to include this statement in the agreement. Lowe also stated that she is not signing the Team Agreement as is, unless #10 is wiped out, which reads: *"Agree to be diplomatic about disagreement; leave disagreement at the dais and do not try to polarize other Councilors."* In addition, Lowe noted that additional time was needed to review the Council Team Agreement, pointing out Uhing has not provided her feedback. Kidd stated through his own research, he has found that every business has a "Code of Conduct" instead of a team agreement, pointing out he has some ideas that he would like to present as well. In conclusion of the above-noted Council discussion, Mayor Truax advised he would accept comments up until the next Council meeting, to which Ruggles and VanderZanden advised staff would schedule a follow-up Work Session for review of the Team Agreement for the next meeting of Monday, March 28, 2016.

Council took no formal action nor made any formal decisions during the above-noted work session.

3. ADJOURNMENT:

Mayor Truax adjourned the regular meeting at 6:20 p.m.

Respectfully submitted,

Anna D. Ruggles, CMC, City Recorder

A place where families and businesses thrive.

City Council Regular Meeting Minutes

Monday, March 14, 2016
7:00 p.m., Community Auditorium

Minutes are unofficial until approved by Council.

1. CALLED TO ORDER AND ROLL CALL:

Mayor Peter Truax called the regular City Council meeting to order at 7:08 p.m. and led the Pledge of Allegiance.

ROLL CALL: COUNCIL PRESENT: Thomas Johnston, Council President; Richard Kidd; Victoria Lowe; Ronald Thompson; Malinda Wenzl; and Mayor Peter Truax.
COUNCIL ABSENT: Elena Uhing, excused.

STAFF PRESENT: Jesse VanderZanden, City Manager; Paul Downey, Administrative Services Director; Rob Foster, Public Works Director; Tom Gamble, Parks and Recreation Director; Jon Holan, Community Development Director; Dan Riordan, Senior Planner; Derek Robbins, Project Engineer; George Cress, Light and Power Director (in the audience); Michael Kinkade, Fire Chief (in the audience) and Anna Ruggles, City Recorder.

1. A. PROCLAMATION:

National Community Development Week

Mayor Truax publicly proclaimed March 28 – April 2, 2016, as “National Community Development Week in Forest Grove”.

2. CITIZEN COMMUNICATIONS:

Dale Feik, Forest Grove, distributed a written article titled “Our Children’s Trust, Meet the Youth Plaintiffs” and encouraged Council to read the article, noting the article contains a list of 21 youths who are securing the legal right to a healthy atmosphere and stable climate for the benefit of all present and future generations.

Jim Vance, Forest Grove, addressed Council pertaining to a letter that he received from the City advising him that he was in violation of Code Section 10.7.020 as it pertains to accessory structures, such as shipping containers, which Vance noted are prohibited by code in residential zones beyond 60 days. Vance pointed out his storage unit is not visible from the street, it is not a public hazard or nuisance and, in his opinion, his storage unit does not adversely affect livability or his three neighbors. Vance stressed he feels the City is selectively enforcing the Code, to which Mayor Truax advised code enforcement is often citizen complaint driven, so it is likely that a citizen concern was conveyed to the City.

3. CONSENT AGENDA:

Items under the Consent Agenda are considered routine and are adopted with a single motion, without separate discussion. Council members who wish to remove an item from the Consent Agenda may do so prior to the motion to approve the item(s). Any item(s) removed from the Consent Agenda will be discussed and acted upon following the approval of the remaining Consent Agenda item(s).

- A. Approve City Council Work Session (Fire Services Study, Next Steps) Meeting Minutes of February 8, 2016.
- B. Approve City Council Goal-Setting Retreat Meeting Minutes of February 20, 2016.
- C. Approve City Council Regular Meeting Minutes of February 22, 2016.
- D. Accept Community Forestry Commission Meeting Minutes of January 20, 2016.
- E. Accept Historic Landmarks Board Meeting Minutes of January 26, 2016.
- F. Accept Planning Commission Meeting Minutes of January 19, 2016.
- G. Accept Public Arts Commission Meeting Minutes of January 14, 2016.
- H. Accept Public Safety Advisory Commission Meeting Minutes of January 27, 2016.
- I. Community Development Department Monthly Building Activity Informational Report for February 2016.
- J. **ENDORSE NEW LIQUOR LICENSE APPLICATION (OFF-PREMISES SALES) FOR SMOKE 4 LESS, 3010 PACIFIC AVENUE (APPLICANT: WAE RAFEH).**
ITEM REMOVED
- K. **ENDORSE LIQUOR LICENSE RENEWAL APPLICATIONS FOR YEAR 2016:**
 - 1. McMenamins Grand Lodge (Full On-Premises Sales) *ITEM REMOVED*

MOTION: Councilor Kidd moved, seconded by Councilor Thompson, to approve the Consent Agenda as amended. **ABSENT:** Councilor Uhing. **MOTION CARRIED 6-0 by voice vote.**

Lowe removed Consent Agenda Items 3. J. and 3. K., pointing out four businesses recently sold to minor decoys (Rainbow Lanes Bowling Alley, Half Moon Sports Bar, Forest Grove Tobacco and Tyson's Pub & Grub), to which Ruggles advised when these types of situations occur, the Oregon Liquor Control Commission, along with Forest Grove Police, issue citations to the servers for Furnishing Alcohol to a Minor. Although, the City does not have final authority, Ruggles noted the City does send a letter of warning to the businesses indicating the City takes violations very seriously when considering annual renewal of their existing liquor license.

MOTION: Councilor Lowe moved, seconded by Councilor Thompson, to approve Consent Agenda Items 3. J. and 3. K. as presented. **ABSENT:** Councilor Uhing. **MOTION CARRIED 6-0 by voice vote.**

4. **ADDITIONS/DELETIONS:** None.

5. **PRESENTATIONS:**

5. A. **Audit Report Period Ending June 30, 2015**

Downey introduced Brad Bingenheimer, City Auditor, who presented a PowerPoint presentation outlining the audit process and Financial Audit Report for period ending June 30, 2015, noting he is pleased to report no significant management advisory suggestions were made and no difficulties were encountered while performing and completing the City's audit. Bingenheimer highlighted the City's financial activities and financial position, noting the General Fund reported a fund balance of \$6,156,806, a decrease of \$32,910 from the prior fiscal year. Bingenheimer highlighted the City's capital assets and long-term debt, noting the City issued no additional debt in Fiscal Year 2015. In addition, Bingenheimer highlighted the City's economic factors, noting the City recognizes that costs of providing General Fund services at the current level will continue to increase at a faster rate than the revenue used to fund those services. The City has been able to increase General Fund Balance due to increased property tax revenue and departments spending less than appropriated mostly due to unanticipated vacancies in public safety. Bingenheimer advised the City would need to be diligent to maintain the fund balance while trying to address the increased need for services from growth over the next five years and retirement costs will continue to be a challenge as the City addresses potential actuarial changes for mortality and the assumed rate of return in the City's Defined Benefit Plan. Bingenheimer reported new single-family home construction continued at a high pace in FY 2014-15 with 102 building permits issued, noting the City is reliant on residential growth to increase its property tax revenue as new industrial and commercial property development has not shown any increase. Bingenheimer added electric and water rates are expected to increase for the next three years as the City continues to address updating aging infrastructure for both those utilities and additional capacity to address future electric and water needs; however, due to changes in some long-term capital projects, the City was able to reduce the amount of projected increases over what was originally forecasted, noting electric rates were increased by 4.5 percent instead of six percent and water rates were increased by three percent instead of eight percent. In conclusion of the above-noted presentation, Bingenheimer addressed various Council inquiries pertaining to the auditor's report, internal control and financial analysis, noting he commends staff for their excellent assistance and outstanding support during the City's audit process.

5. B. **Metro Quarterly Exchange Update, Kathryn Harrington, Metro Councilor District 4**
Kathryn Harrington, Metro Councilor District 4, presented a PowerPoint presentation highlighting District 4 Quarterly Exchange Report. Harrington reported on Metro's

Equity Strategy; Regional Jobs Snapshot; Equitable Housing; Moving Metro's Region; Southwest Corridor; Powell-Division Transit and Development Project; Flexible Transportation Funds; Parks and Natural Areas Updates; Natural Areas System Plan; Killin Wetlands; Solid Waste Roadmap; Convention Center Hotel; Oregon Zoo; various upcoming community events; and distributed various handouts. In response to Councilor Lowe's concern pertaining to Harrington's presentation on equitable housing and Harrington's testimony heard during the hearing for Tokola's mixed-use development project, Harrington explained her testimony was specific to the market rate housing application and not as the whole city, noting every community needs a mixture of housing.

5. C. Parks, Recreation and Open Space Master Plan Update and Community Center Feasibility Study

Gamble presented a PowerPoint presentation overview of the Parks, Recreation and Open Space Master Plan Update, noting the purpose of this evening's presentation was to provide a brief update to Council on the progress of the study and next steps. Gamble reported the purpose of the master plan update was to identify community needs for parks, recreation facilities, open space, trails, programs and events in Forest Grove; investigate opportunities and needs for community center development; recommend capital projects, programs and services to enhance the park and recreation system; and develop coordinated funding and short and long-term implementation strategies. In addition, Gamble reported a four-phased planning effort was conducted, which included community engagements; focus groups (recreation/sports providers and Hispanic/Latino community group); community recreation questionnaire (total of 488 responses); needs assessment; community center feasibility study; park and recreation survey; and next steps. In conclusion of the above-noted presentation, Gamble addressed various Council inquiries pertaining to the City's partnership with the school district; cities with community centers; and energy audit conducted on the Aquatic Center.

6. PUBLIC HEARING AND FIRST READING OF ORDINANCE NO. 2016-02 AMENDING FOREST GROVE TRANSPORTATION SYSTEM PLAN (TSP) FOR INCLUSION OF COUNCIL CREEK REGIONAL TRAIL PREFERRED ALIGNMENT THROUGH THE CITY OF FOREST GROVE; FILE NO. 311-15-00033-PLNG

Staff Report:

Robbins, Foster, Riordan and Holan presented the above-proposed ordinance for first reading, noting the proposed ordinance is requesting to adopt the proposed text amendments to the Transportation System Plan (TSP) to incorporate the preferred alignment of the Council Creek Regional Trail (located at the Portland/Western Railroad corridor, Oak Street and Highway 47) as recommended by the Planning Commission. Staff presented a PowerPoint presentation overview of the proposed TSP amendments for Council Creek Regional Trail alignment as outlined in Exhibit A

of the staff report:

- Table 5-1 deleted Council Creek Trail Feasibility Study Project as this study has been completed and insert Council Creek Regional Trail Improvements Project, Segment 3, construct Council Creek Regional Trail through Forest Grove consistent with the Council Creek Regional Trail Master Plan (August 2015), and insert Planning Level Cost Estimate of \$5,200, which amends total amount shown from \$5,155 to \$10,355;
- Figure 5-4 Council Creek Regional Trail Preferred Alignment through Forest Grove, Segment 3 (including Oak Street option); and
- Table 6-2, add Council Creek Regional Trail Improvements Project, Segment 3, construct Council Creek Regional Trail through Forest Grove consistent with the Council Creek Regional Trail Master Plan (August 2015), and insert Planning Level Cost Estimate of \$5,200, which amends total amount shown from \$1,769 to \$6,979.

In addition, staff referenced Attachment 1, Planning Commission Meeting Minutes, Staff Report and Findings, noting the multi-agency study was completed in August 2015, and is a 16-mile multi-use trail from Hillsboro to Banks, from the end of the Westside Max in Hillsboro through Washington County and cities of Cornelius, Forest Grove and Banks, connecting to Banks-Vernonia State Trail, with added short trail south to Tualatin River. Staff added the Forest Grove trail, Segment 3 of the CCRT, is approximately 1.5 miles and primarily follows the existing Portland/Western Railroad corridor through central Forest Grove (near Senior Center), noting there are two possible routes going north, with one route at Quince Street, following the north side of Highway 47, and one route going north on Oak Street to Porter Road, which is less defined and will require more preliminary design and review with the public for specific segment alignment. In conclusion of the above-noted staff report, staff recommended Council adopt the above-noted ordinance amending the TSP to incorporate the preferred alignment of the Council Creek Regional Trail, as outlined in Exhibit A, File No. 311-15-00033-PLNG.

Before proceeding with the Public Hearing and Council discussion, Mayor Truax asked for a motion to adopt Ordinance No. 2016-02 for first reading.

Mayor Truax read Ordinance No. 2016-02 by title for first reading.

MOTION: Councilor Kidd moved, seconded by Councilor Wenzl, to adopt Ordinance No. 2016-02 Amending Forest Grove Transportation System Plan (TSP) for Inclusion of Council Creek Regional Trail Preferred Alignment through the City of Forest Grove; File No. 311-15-00033-PLNG.

Public Hearing Opened:

Mayor Truax opened the Public Hearing and explained hearing procedures.

Written Testimony Received:

No written testimony was received prior to the deadline of March 14, 2016, 7:00 p.m.

Proponents:

No one testified and no written comments were received.

Opponents:

No one testified and no written comments were received.

Others:

No one testified and no written comments were received.

Council Discussion:

In response to Council President Johnston's inquiry pertaining to funding, Foster advised the TSP identifies funding options and funds from the Major Streets Transportation Improvement Program (MSTIP) were used to conduct the multi-agency study.

Hearing no further discussion from the Council, Mayor Truax recessed the Public Hearing until the next meeting of Monday, March 28, 2016.

Public Hearing Recessed:

Mayor Truax recessed the Public Hearing until the next Council meeting of Monday, March 28, 2016.

- 7. PUBLIC HEARING AND FIRST READING OF ORDINANCE NO. 2016-03 AMENDING FOREST GROVE DEVELOPMENT CODE ARTICLE 3, ARTICLE 4, ARTICLE 7 AND ARTICLE 12 AND DESIGN GUIDELINE HANDBOOK TO IMPLEMENT POLICIES OF THE FOREST GROVE COMPREHENSIVE PLAN; FILE NO. 311-15-00020-PLNG**

Staff Report:

Riordan and Holan presented the above-proposed ordinance for first reading, noting the staff report will be heard concurrently with Agenda Item 8, first reading of Ordinance No. 2016-04 amending the Comprehensive Plan, and Agenda Item 9, first reading of Ordinance No. 2016-05 amending the Zoning Map. Riordan reported the above-noted ordinances are implementing the Comprehensive Plan policies related to mixed-use areas, efficient use of land in the Town Center and encourage housing within the Community Commercial District. Riordan referenced the above-noted ordinance, noting the ordinance is proposing to amend Development Code Article 3,

Article 4, Article 7 and Article 12, and Design Guideline Handbook as recommended by the Planning Commission, Decision No. 2016-01. In addition, Riordan presented a PowerPoint presentation overview of the proposed amendments as outlined in Exhibit A of the staff report:

1. Increase residential density in Town Center from 20 units to 40 units per net acre and set maximum density at 100 units per net acre based on project design/amenities;
2. Increase residential density in Community Commercial from 20 units to 30 units per net acre to encourage housing near transit and shopping;
3. Rezone Town Center Expansion Area consistent with Comprehensive Plan (from Town Center Support to Town Center Transition and Community Commercial to Town Center Transition; Town Center core along Main Street will remain);
4. Establish a new Neighborhood Mixed Use Zone (NMU) District to promote complete neighborhoods; consistent with Comprehensive Plan Map and
5. Amend Zoning Map to apply the new NMU in the following locations:
 - a. Generally east of Sunset Drive, south of Highway 47 (approximately 25 gross acres);
 - b. North of Gales Creek Road and west of Thatcher Rd (approximately 6.5 gross acres);
 - c. North of David Hill Road, west of Thatcher Road and east of creek (approximately 8.5 gross acres).
6. Establish NMU development review and approval process through a Planned Development process; and
7. Design Guideline Handbook updates for consistency with Development Code.

In addition, Riordan indicated a robust public involvement process was used to support the City's planning efforts, which included stakeholder interviews, two public community open houses and four Planning Commission work sessions and one joint work session with the Planning Commission and Council. In conclusion of the above-noted staff report, Holan and Riordan recommended Council adopt the above-noted ordinance amending the Development Code and Design Guideline Handbook, noting the amendments will provide an opportunity to increase residential density in the Town Center and near commercial areas; thereby, expanding housing choice and providing an opportunity to promote "complete neighborhoods" with a variety of housing located near shopping and services, as outlined in Exhibit A, File No. 311-15-00020-PLNG.

Before proceeding with the Public Hearing and Council discussion, Mayor Truax asked for a motion to adopt Ordinance No. 2016-03 for first reading.

Mayor Truax read Ordinance No. 2016-03 by title for first reading.

MOTION: Councilor Kidd moved, seconded by Council President Johnston, to adopt Ordinance No. 2016-03 Amending Forest Grove Development Code Article 3, Article 4, Article 7 and Article 12 and Design Guideline Handbook to Implement Policies of the Forest Grove Comprehensive Plan; File No. 311-15-00020-PLNG.

Public Hearing Opened:

Mayor Truax opened the Public Hearing and explained hearing procedures.

Written Testimony Received:

No written testimony was received prior to the deadline of March 14, 2016, 7:00 p.m.

Proponents:

No one testified and no written comments were received.

Opponents:

Nancy Russell, Forest Grove, signed in to testify but was not present when her name was called, to which Holan indicated he spoke with Russell prior to the start of the meeting and was able to address her concern.

No one testified and no written comments were received.

Others:

No one testified and no written comments were received.

Council Discussion:

In response to Wenzl's inquiry pertaining to net density definition/calculation in the Town Center, Riordan noted the Planning Commission had considerable discussion to apply a revised definition that would increase density in the Town Center; however, the Planning Commission's motion failed so no revision occurred.

In response to Lowe's inquiry pertaining to rapid transit corridor, Holan explained the Comprehensive Plan updated in 2014 expanded the Town Center zone from Cedar Street to Elm Street and south of 19th Avenue to 21st Avenue, which is now identified as the Town Center Expansion Area.

Hearing no further discussion from the Council, Mayor Truax recessed the Public Hearing until the next meeting of Monday, March 28, 2016.

Public Hearing Recessed:

Mayor Truax recessed the Public Hearing until the next Council meeting of Monday, March 28, 2016.

8. **PUBLIC HEARING AND FIRST READING OF ORDINANCE NO. 2016-04 AMENDING FOREST GROVE COMPREHENSIVE PLAN MAP FOR PROPERTIES DESIGNATED TOWN CENTER SUPPORT AND COMMUNITY COMMERCIAL IN THE TOWN CENTER EXPANSION AREA ON THE COMPREHENSIVE PLAN, DATED JANUARY 27, 2014; FILE NO. 311-15-00020-PLNG**

Staff Report:

Riordan and Holan presented the above-proposed ordinance for first reading, noting the staff report was heard concurrently under Agenda Item 7. The above-proposed ordinance is amending the Comprehensive Plan Map (2014) in the Town Center Expansion Area from Town Center Support to Town Center Transition and Community Commercial to Town Center Transition as recommended by the Planning Commission, Decision No. 2016-01. In conclusion of the above-noted staff report, Riordan and Holan recommended Council adopt the above-noted ordinance amending the Comprehensive Plan Map to apply the proposed Town Center Transition zone for consistency with the proposed amendments under Agenda Item 7 and Agenda Item 9, as outlined in Exhibit A, File No. 311-15-00020-PLNG.

Before proceeding with the Public Hearing and Council discussion, Mayor Truax asked for a motion to adopt Ordinance No. 2016-04 for first reading.

Mayor Truax read Ordinance No. 2016-04 by title for first reading.

MOTION: Councilor Wenzl moved, seconded by Councilor Kidd, to adopt Ordinance No. 2016-04 Amending Forest Grove Comprehensive Plan Map for Properties Designated Town Center Support and Community Commercial in the Town Center Expansion Area on the Comprehensive Plan, dated January 27, 2014; File No. 311-15-00020-PLNG.

Public Hearing Opened:

Mayor Truax opened the Public Hearing and explained hearing procedures.

Written Testimony Received:

No written testimony was received prior to the deadline of March 14, 2016, 7:00 p.m.

Proponents:

No one testified and no written comments were received.

Opponents:

No one testified and no written comments were received.

Others:

No one testified and no written comments were received.

Council Discussion:

Hearing no discussion from the Council, Mayor Truax recessed the Public Hearing until the next meeting of Monday, March 28, 2016.

Public Hearing Recessed:

Mayor Truax recessed the Public Hearing until the next Council meeting of Monday, March 28, 2016.

9. PUBLIC HEARING AND FIRST READING OF ORDINANCE NO. 2016-05 AMENDING OFFICIAL FOREST GROVE ZONING MAP FOR PROPERTIES DESIGNATED NEIGHBORHOOD MIXED USE AND TOWN CENTER EXPANSION ON THE COMPREHENSIVE PLAN MAP, DATED JANUARY 27, 2014; FILE NO. 311-15-00020-PLNG

Staff Report:

Riordan and Holan presented the above-proposed ordinance for first reading, noting the staff report was heard concurrently under Agenda Item 7. The above-proposed ordinance is amending the Zoning Map for properties designated Neighborhood Mixed Use and Town Center Expansion on the Comprehensive Plan Map (2014) as recommended by the Planning Commission, Decision No. 2016-01. In conclusion of the above-noted staff report, Riordan and Holan recommended Council adopt the above-noted ordinance amending the Zoning Map to apply the proposed Neighborhood Mixed Use and Town Center Transition zones consistent with the amendments under Agenda Item 7 and Agenda Item 8, as outlined in Exhibit A, File No. 311-15-00020-PLNG.

Before proceeding with the Public Hearing and Council discussion, Mayor Truax asked for a motion to adopt Ordinance No. 2016-05 for first reading.

Mayor Truax read Ordinance No. 2016-05 by title for first reading.

MOTION: Councilor Kidd moved, seconded by Councilor Thompson, to adopt Ordinance No. 2016-05 Amending Official Forest Grove Zoning Map for Properties Designated Neighborhood Mixed Use and Town Center Expansion on the Comprehensive Plan Map, dated January 27, 2014; File No. 311-15-00020-PLNG.

Public Hearing Opened:

Mayor Truax opened the Public Hearing and explained hearing procedures.

Written Testimony Received:

No written testimony was received prior to the deadline of March 14, 2016, 7:00 p.m.

Proponents:

No one testified and no written comments were received.

Opponents:

No one testified and no written comments were received.

Others:

No one testified and no written comments were received.

Council Discussion:

Hearing no discussion from the Council, Mayor Truax recessed the Public Hearing until the next meeting of Monday, March 28, 2016.

Public Hearing Recessed:

Mayor Truax recessed the Public Hearing until the next Council meeting of Monday, March 28, 2016.

10. **PUBLIC HEARING AND FIRST READING OF ORDINANCE NO. 2016-06
AMENDING FOREST GROVE DEVELOPMENT CODE ARTICLE 3 TO ESTABLISH
THE BUSINESS INDUSTRIAL PARK ZONE; FILE NO. 311-15-00027-PLNG**

Staff Report:

Riordan and Holan presented the above-proposed ordinance for first reading, noting the proposed ordinance is requesting to amend Development Code Article 3 to establish a new zoning district and applicable development standards for the proposed Business Industrial Park (BIP) Zone areas, which is to correspond to the Campus Employment in the Comprehensive Plan Map (2014) designation applicable within the Urban Growth Boundary as recommended by the Planning Commission, Decision No. 2016-02. Riordan presented a PowerPoint presentation overview of the new zoning district, noting the Campus Employment designation is intended for development of industrial and office parks with a high level of amenities, including landscaping and open space, noting the Campus Employment designation and the proposed BIP zone responds to a need identified in the City's Economic Opportunities Analysis for long-term manufacturing and corporate office employment land. In conclusion of the above-noted staff report, Riordan and Holan recommended Council adopt the above-noted ordinance amending the Development Code to establish the proposed BIP zoning district and applicable development standards, noting the proposed zoning map amendments include the Neighborhood Mixed Use Area 1 (David Hill); Neighborhood Mixed Use Area 2 (Commercial Planned Development Area); Neighborhood Mixed Use Area 3 (Davidson Site); Town Center Consolidation (Town Center Transition); and Town Center Expansion Area (Town Center Transition), as outlined in Exhibit A, File

No. 311-15-00027-PLNG.

Before proceeding with the Public Hearing and Council discussion, Mayor Truax asked for a motion to adopt Ordinance No. 2016-06 for first reading.

Mayor Truax read Ordinance No. 2016-06 by title for first reading.

MOTION: Council President Johnston moved, seconded by Councilor Wenzl, to adopt Ordinance No. 2016-06 Amending Forest Grove Development Code Article 3 to Establish the Business Industrial Park Zone; File No. 311-15-00027-PLNG.

Public Hearing Opened:

Mayor Truax opened the Public Hearing and explained hearing procedures.

Written Testimony Received:

No written testimony was received prior to the deadline of March 14, 2016, 7:00 p.m.

Proponents:

No one testified and no written comments were received.

Opponents:

No one testified and no written comments were received.

Others:

No one testified and no written comments were received.

Council Discussion:

In response to Lowe's inquiry pertaining to the new BIP designation, Riordan explained the new zone is intended to implement the Campus Employment designation, which was established in the Comprehensive Plan update in 2014, noting the designation will allow for high tech companies, call centers, research and development firms and flexible space for small emerging companies and could also include supportive retail and business services.

Hearing no further discussion from the Council, Mayor Truax recessed the Public Hearing until the next meeting of Monday, March 28, 2016.

Public Hearing Recessed:

Mayor Truax recessed the Public Hearing until the next Council meeting of Monday, March 28, 2016.

- 11. PUBLIC HEARING AND FIRST READING OF ORDINANCE NO. 2016-07 AMENDING FOREST GROVE DEVELOPMENT CODE ARTICLE 3, ARTICLE 7 AND**

ARTICLE 8 TO ADDRESS MARIJUANA FACILITIES; FILE NO. 311-15-00028-PLNG

Staff Report:

Holan presented the above-proposed ordinance for first reading, noting the proposed ordinance is amending Development Code Article 3, Article 7 and Article 8, to address marijuana facilities as recommended by the Planning Commission, Decision No. 2016-03. Holan referenced Attachment 1, Planning Commission Findings and Decision No. 2016-03; Attachment 2, Marijuana Zoning Analysis; Attachment 3, League of Oregon Cities Regulation of Marijuana in Oregon; and Attachment 4, House Bill 3400 and presented a PowerPoint presentation overview of the proposed Development Code amendments as outlined in Exhibit A of the staff report:

1. Prohibit marijuana retail sales in Residential Districts;
2. Prohibit marijuana grow sites within Residential Districts;
3. Prohibit marijuana retail outlets in the Neighborhood Commercial District and prohibit taverns, lounges and other commercial uses intended for the on-site consumption of marijuana related products;
4. Allow medical marijuana dispensaries and marijuana retail activities within the Town Center Districts and prohibit taverns, lounges or other commercial uses intended for the on-site consumption of marijuana related products;
5. Prohibit marijuana related activities as a Home Occupation;
6. Apply same design standards and operational requirements for medical marijuana dispensaries to marijuana retailers;
7. Reduce hours of operation for medical marijuana dispensaries and marijuana retailers open to the public between the hours of 8:00 a.m. to ~~8:00 p.m.~~ ~~10:00 p.m.~~ **Council consideration: As proposed, leave code as is, and/or modify code, i.e., closing at 8:00 p.m. Sunday to Thursday and closing at 10:00 p.m. on Friday and Saturday.**
8. Add new subsection 10.8.1110, Other Marijuana Facilities, code requirements for marijuana related warehouses, processing and grow facilities; provide for a method for odor control for indoor/outdoor grow operations; and allow use of removable greenhouses and open field grow operations. **Council consideration: As proposed, allows use of portable greenhouses, either permanent or temporary, which may be of concern under such circumstances.**

In addition, Holan indicated the framework for future discussions with Council may include other potential development requirements, such as buffers, setbacks in residential districts, signage, building design and additional lighting, noting HB 3400 requires adopting an ordinance referred to the voters, if imposing a tax or fee of up to three percent on marijuana retail sales. In conclusion of the above-noted staff report, Holan recommended Council adopt the above-noted ordinance amending the Development Code to address marijuana facilities, as outlined in Exhibit A, File No.

311-15-00028-PLNG.

Before proceeding with the Public Hearing and Council discussion, Mayor Truax asked for a motion to adopt Ordinance No. 2016-07 for first reading.

Mayor Truax read Ordinance No. 2016-07 by title for first reading.

MOTION: Councilor Wenzl moved, seconded by Councilor Lowe, to adopt Ordinance No. 2016-07 Amending Forest Grove Development Code Article 3, Article 7 and Article 8 to Address Marijuana Facilities; File No. 311-15-00028-PLNG.

Public Hearing Opened:

Mayor Truax opened the Public Hearing and explained hearing procedures.

Written Testimony Received:

No written testimony was received prior to the deadline of March 14, 2016, 7:00 p.m.

Proponents:

Stewart Wilson, Hillsboro, testified in support of the recommendations as it relates to outdoor grow operations, pointing out the Oregon Liquor Control Commission requires 8' fences to block views of outdoor grow operations.

Adrian Perte, business located at 3821 Pacific Avenue, Forest Grove, testified in support and reported his business hours of operation are Monday through Thursday 10:00 a.m. to 8:00 p.m.; Friday through Saturday 10:00 a.m. to 10:00 p.m.; and Sunday 10:00 a.m. to 8:00 p.m., pointing out that 8:00 a.m. is the best time for medical users.

No one else testified and no written comments were received.

Opponents:

John Schwan, Senior Director, TTM Technologies, Poplar Lane, Forest Grove, submitted into the record a copy of TTM Technologies' written testimony, noting he is testifying on behalf of TTM and their attorneys who were unable to attend this hearing. Schwan stated TTM is testifying in opposition due to the recent marijuana grow operation adjoining their property, noting TTM currently employs around 650 full-time employees and TTM has a very strict drug policy that prohibits the possession or use of federally illegal drugs, such as marijuana, on all their manufacturing operations and in places in which they do business. Schwan referenced a list of additional opposition reasons, as outlined in his written testimony, noting the Forest Grove operations are under the control of the Department of Defense and Department of Homeland Security, and any threats or potential threats that "could" have an effect on Forest Grove

operations will require both customer and federal government notifications. Schwan noted any federal government notifications would trigger inquiries related to business threats and whether their site's security is or has the potential to be compromised. Schwan added, as a result, their customers and TTM will consider or be forced to move products into one of their 12 other United States based facilities, noting the loss in revenue will force financial hardship to the Forest Grove operations with an increased likelihood of a full facility shutdown. In addition, Schwan pointed out TTM is also very concerned about employee safety and the environmental implications that the grow operation will have on wildlife and preserves in the local area, noting TTM owns a substantial amount of property around the Forest Grove site.

Council Response to Testimony Heard:

In response to a Council inquiry pertaining to the adjoining property owners' responsibilities, Schwan indicated TTM would be held responsible for any threats or potential threats to their business operations and their site security being compromised, noting having drugs within close proximity to their federally controlled operations would be detrimental and would force the Forest Grove operations to implement further "significant" security controls that would have a high financial hardship to the Forest Grove operations.

In response to a Council inquiry pertaining to if TTM testified at the state level or Planning Commission hearing, Schwan indicated TTM did not testify at either hearing, noting he is one week into the hearing process, to which Mayor Truax advised Schwan that the hearing would be continued until Monday, March 28, 2016, if TTM would like to provide any additional testimony.

No one else testified and no written comments were received.

Others:

No one testified and no written comments were received.

Council Discussion:

In response to Lowe's inquiry pertaining to consumption locations, Holan advised public consumption is prohibited under State law and cannabis falls under the regulation of Oregon's Clean Air Act in 2016.

In response to Kidd's inquiry pertaining to hours of operation, Holan explained the Planning Commission's recommendation is to reduce hours of operation at 8:00 p.m. instead of 10:00 p.m. to coincide with other business hours in the community, noting the police department concurs due to safety and patrol.

Thompson voiced concern that marijuana state laws do not address hash oil, pointing out two recent fires caused by using butane.

Hearing no further discussion from the Council, Mayor Truax recessed the Public Hearing until the next meeting of Monday, March 28, 2016.

Public Hearing Recessed:

Mayor Truax recessed the Public Hearing until the next Council meeting of Monday, March 28, 2016.

Council collectively concurred to extend the meeting past 9:30 p.m. adjournment, to which, Mayor Truax called for a recess at 9:37 p.m. and reconvened the meeting at 9:41 p.m.

12. **PUBLIC HEARING AND FIRST READING OF ORDINANCE NO. 2016-08 AMENDING FOREST GROVE CITY CODE CHAPTER 7 BY RENAMING CODE SECTIONS 7.850 THROUGH 7.865 FROM “MEDICAL MARIJUANA DISPENSARIES” TO “MARIJUANA ACTIVITIES”; AMENDING CODE SECTIONS 7.850 THROUGH 7.860 TO ADDRESS MARIJUANA FACILITIES; AND REPEALING PORTIONS OF ORDINANCE NO. 2015-03; FILE NO. 311-15-00028-PLNG**

Staff Report:

Holan presented the above-proposed ordinance for first reading, noting the proposed ordinance is requesting to amend City Code Sections 7.850 through 7.865 to address marijuana facilities as recommended by the Planning Commission, Decision No. 2016-03. Holan presented a PowerPoint presentation overview of the proposed amendments as outlined in Exhibit A of the staff report:

1. Code Section 7.855, deleted ~~Medical Marijuana Dispensary~~ and added Marijuana Related Activities: An activity involved with the growing, processing, wholesaling or selling of marijuana, cannabinoid product, cannabinol concentrate or cannabinoid extract regulated by Oregon Health Authority or Oregon Liquor Control Commission);
2. Code Section 7.860(A), amended, marijuana related activity must comply with all applicable requirements of State laws (ORS 475B (2015));
3. Code Section 7.860(B), amended, marijuana related activity must obtain a business license pursuant to City Code, Chapter 7;
4. Extend opaque bag requirement to include dispensaries and retail facilities; and
5. Prohibiting on-site consumption of marijuana related products at commercial establishments; and
6. Hours of operation for marijuana activities, City Code currently allows between the hours of 8:00 a.m. to 10:00 p.m. ***Council consideration: Leave code as is and/or modify closing at 8:00 p.m. for consistency purpose with the Development Code under Agenda Item 11.***

In conclusion of the above-noted staff report, Holan recommended Council adopt the above-noted ordinance amending City Code Sections 7.850 through 7.865 to address marijuana facilities, as outlined in Exhibit A, File No. 311-15-00028-PLNG.

Before proceeding with the Public Hearing and Council discussion, Mayor Truax asked for a motion to adopt Ordinance No. 2016-08 for first reading.

Mayor Truax read Ordinance No. 2016-08 by title for first reading.

MOTION: Councilor Wenzl moved, seconded by Councilor Lowe, to adopt Ordinance No. 2016-08 Amending Forest Grove City Code Chapter 7 by Renaming Code Sections 7.850 through 7.865 from “Medical Marijuana Dispensaries” to “Marijuana Activities”; Amending Code Sections 7.850 through 7.860 to Address Marijuana Facilities; and Repealing Portions of Ordinance No. 2015-03; File No. 311-15-00028-PLNG.

Public Hearing Opened:

Mayor Truax opened the Public Hearing and explained hearing procedures.

Written Testimony Received:

No written testimony was received prior to the deadline of March 14, 2016, 7:00 p.m.

Proponents:

No one testified and no written comments were received.

Opponents:

No one testified and no written comments were received.

Others:

No one testified and no written comments were received.

Council Discussion:

Hearing no discussion from the Council, Mayor Truax recessed the Public Hearing until the next meeting of Monday, March 28, 2016.

Public Hearing Recessed:

Mayor Truax recessed the Public Hearing until the next Council meeting of Monday, March 28, 2016.

13. **RESOLUTION NO. 2016-20 AUTHORIZING THE EXPENDITURE OF MACC PCN GRANT FUNDS IN THE INFORMATION SYSTEMS FUND FOR FIREWALL REPLACEMENT FOR THE CITY'S COMPUTER NETWORK SYSTEM**

Staff Report:

Downey presented the above-noted resolution requesting authorization of expenditure of MACC PCN grant funds, noting the City was awarded \$49,210 to replace the City's aging firewall server with a new firewall that has more features than the current system. Downey advised receiving the grant means the City will not have to find the additional funds that would have been necessary to purchase an advanced firewall, noting the City will accumulate replacement funds for eventual replacement of the firewall similar to all other information technology assets. In conclusion of the above-noted staff report, Downey advised staff is recommending Council adopt the proposed resolution so grant funds can be spent, noting a supplemental budget hearing is not required pursuant to budget law for expenditure of grant funds.

Before proceeding with Council discussion, Mayor Truax asked for a motion to adopt Resolution No. 2016-20.

Mayor Truax read Resolution No. 2016-20 by title.

MOTION: Councilor Wenzl moved, seconded by Councilor Thompson, to adopt Resolution No. 2016-20 Authorizing the Expenditure of MACC PCN Grant Funds in the Information Systems Fund for Firewall Replacement for the City's Computer Network System.

Council Discussion:

Hearing no discussion from the Council, Mayor Truax asked for a roll call vote on the above-noted motion.

ROLL CALL VOTE: AYES: Councilors Johnston, Kidd, Lowe, Thompson, Wenzl, and Mayor Truax. NOES: None. ABSENT: Councilor Uhing. MOTION CARRIED 6-0.

14. CITY COUNCIL COMMUNICATIONS:

Due to time, Mayor Truax postponed Council Communications to the next Council meeting of March 28, 2016.

15. CITY MANAGER'S REPORT:

Due to time, Mayor Truax postponed City Manager's Report to the next Council meeting of March 28, 2016, noting the City Manager's Report was distributed electronically to Council and was added to the online Council Packet.

16. MAYOR'S REPORT:

Due to time, Mayor Truax postponed Mayor's Report to the next Council meeting of March 28, 2016.

17. **ADJOURNMENT:**

Mayor Truax adjourned the regular meeting at 10:00 p.m.

Respectfully submitted,

Anna D. Ruggles, CMC, City Recorder

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Members Present: Jennifer Brent, George Cushing, MJ Guidetti-Clapshaw, Holly Tsur, Larissa Whalen Garfias (01 vacancy)
Member Excused: Kaylene Toews
Staff Present: James Reitz
Council Liaison: Richard Kidd
Citizens Present: 01

1. **Call to Order:** Tsur opened the meeting at 7:13 p.m.

2. **Citizen Communication:** None.

3. **Action Items / Discussion:**

A. Renovation Grant Request – W. R. Taylor House at 2212 “A” Street (Washington County Tax Lot 1N3 31CC-5201). Applicant: Jeremiah Toews. File Number: 311-16-000003-PLNG. Applicant Jeremiah Toews presented his request for a grant to help with the expense to re-roof the house. He reported that he had two estimates and had chosen one. He described the project, which will include removing two layers of roofing and replacing much of the sheathing to bring the roof back to like-new condition. Board members asked a few questions and were satisfied with the responses. **Tsur/Cushing to award a grant of \$500. Motion carried unanimously.**

B. Strategic Plan – Focus Group Meeting Follow-Up: The Board previously received a meeting synopsis from the consultants of Historic Preservation NW but generally had not had a chance to really review it before the meeting. Tsur felt that we need a more thorough understanding of the Main Street Program. Guidetti-Clapshaw concurred, saying that she felt we needed more information about the differences between the Main Street Program and a historic downtown district.

Discussion ensued about how the Main Street Program is multi-tiered with several levels of commitment and opportunity. Reitz noted that it would not be a Board or City-initiated effort, but that it would have to be started by a private non-governmental organization like the City Club or the Chamber of Commerce. It was noted that Linda Lewis and the City Club would be a natural to initiate this but they would first have to determine if the downtown business community would be supportive.

Reitz stated that the time frame to develop and implement a Main Street Program could be several years out; it is not something that would be in place next year. Kidd added that while it is primarily a national program, the State is also involved. He further noted that the program could be funded through a combination of City, State and National sources; locally, some funding might be available through the urban renewal district.

A question was raised as to whether a Downtown Historic District would be affected by the recently enacted design guidelines and standards. Reitz replied in the negative, noting that the new code addresses only residential construction and was specific to the three existing historic districts. To make any changes or to include other districts in the future would require City Council approval.

Reitz requested that the Board continue to review the synopsis and forward any additions or corrections directly to the consultants, with copies to the rest of us.

C. CEP Grant Application: Tsur stated that she would prepare a draft of the grant application; she'll then forward it to everyone for review and comment. We'll have an opportunity to discuss it at the March 22 meeting, before the application deadline.

Kidd reported that the CEP Grant Committee had revised the requirements for this cycle: the maximum amount to be requested will be capped at \$10,000 and no more than two applications can be submitted by each applicant. He said that Metro stipulates that CEP funds cannot be used to offset government budget line items and that they are striving to stop mini-grants in the future.

The Board discussed the reality that again we may not have all the current funds committed before the end of the fiscal year in June. Tsur suggested that we consider increasing the maximum grant amounts for the next cycle. She noted that Albany has a grant program that distributes leftover funding at the end of the budget cycle to recipients. Reitz was tasked to determine how Albany handles this and to research any legal requirements, and to bring this information back to the next meeting.

4. Old Business/New Business:

- Approval of HLB Meeting Minutes. **The meeting minutes of January 26, 2016 were approved as submitted.**
- Council Liaison Report: Kidd updated the Board on various items of interest, including the Council's approval of the Tokala project. He also suggested that the Board explore the possibility of partnering with the Sustainability, Arts, and Parks and Recreation commissions as we might have some common goals. Cushing volunteered to reach out to the chair persons of each commission and initiate those discussions. Kidd further noted that there are the remains of a number of original homesteads in the Forest Grove watershed; he suggested that Board members sign up for the next walking tour. He stressed that the watershed is not open to the public at any other time.
- Reitz reported that Mary Jo Morelli had contacted him about offering a Forest Grove history class over three or four weeks in May, to coincide with National Historic Preservation Month. The class would be one night a week for a couple of hours and would be sponsored by the Board. There would be an entrance fee to provide for expenses: Morelli's time plus any materials and refreshments. The Board endorsed the idea and requested that Reitz continue with making the arrangements.
- Concern was expressed about the difficulty in locating information on the City website. The possibility of a Facebook page with hyperlinks to specific parts of the HLB page of the City's website was suggested. Kidd said he would look into legalities of the Board using a Facebook page.
- Tsur asked everyone to start thinking about potential recipients of the Eric Steward Award. Garfias asked for some background; Kidd provided a short description of Stewart's involvement in the documentation of Forest Grove history and Tsur described the location and content of the library's historic room, named in Stewart's honor.
- Reitz requested that members turn in their notebooks for updating; Cushing, Guidetti-Clapshaw and Tsur did so. The updated notebooks will be distributed at the next meeting.

5. Adjournment: The February 23, 2016 meeting adjourned at 8:55p.m.

These minutes respectfully submitted by George Cushing, Secretary

APPROVED

Library Commission approved minutes as presented on Feb 9, 2016.

1. **CALLED TO ORDER AND ROLL CALL:**

Pamela Bailey, Chair, called the meeting of the Library Commission to order at 6:30PM on Tuesday January 12, 2016.

Members Present: Pamela Bailey, Chair; Doug Martin; Jon Youngberg, Secretary; Kathleen Poulsen, Vice-Chair; Mitchell Faris, Student; Nickie Augustine;

Members Absent: None

Staff: Colleen Winters, Library Director

Council Liaison: Malynda Wenzl

Others: None

2. **ADDITIONS/DELETIONS:** None.

3. **APPROVE LIBRARY COMMISSION MEETING MINUTES OF (Dec 8, 2015):**

MOTION: Doug moved, seconded by Nickie, to approve the December 8, 2015 minutes as presented. **MOTION CARRIED** by all.

4. **CITIZEN COMMUNICATIONS:** None.

5. **INFORMATIONAL ITEMS:**

5a. **FOUNDATION REPORT:** Colleen Winters reported about the Library Foundation of Forest Grove:

a) The Foundation board will next meet Jan 25.

b) Colleen and library staff will present to the board a final list of all that needs to be done in the Rogers Room, and in the Children's area and Adult Fiction areas of the old part of the library. This will include: new end panels and children's furniture, a sound system for the Rogers Room, and replacing (or perhaps painting) the adult fiction area's unattractive heavy chairs and tables. Also, more electric outlets are needed, because library patrons bring electric devices to the library, etc.

c) Will revisit ideas for the next Foundation project, that of a creating a "Sense of Arrival" to the old half of the library, especially the Children's Area. This project has been considered for at least 8 years. Could buy expensive furniture and signage,

but would rather have local artists design and create whatever is decided upon. Malynda suggested looking into applying for a CEP grant for this project, to help pay for it.

d) See the Library Foundation of Forest Grove's web site (www.fglf.org).

5b. FRIENDS REPORT: Colleen Winters reported about the Friends of the Forest Grove Library:

a) The Friends Board will meet tomorrow. There are two resignations from the board (Kim Reynolds, and Carol Woodford).

b) Dave Pauli and Susan Munger are stepping down as organizers of the Cultural Series, and will need to be replaced.

c) Online book sales continue to do well, on Amazon.com. More volunteers are being recruited for this effort.

d) Will consider creating a "permanent" in-library used book sale, possibly to replace the big semi-annual used book sales. Volunteers would attend to the shelves (near the Adult Fiction area) daily. Library patrons have long asked for this. There is an example permanent used book sale at the Hillsboro Main library.

e) Tues Jan 5, 2016 Cultural Series talk was by the Friends of Historic Forest Grove. There were 50 to 60 attendees for the presentation and performance about some early and important settlers in the Forest Grove area.

f) The Friends web site is at: fglibraryfriends.org.

5c. COUNCIL LIAISON REPORT :

a) Considering Tokola Project at old Times-Litho site. Affordable housing, retail spaces, and amenities for the area discussed. Parking concerns for area.

b) Rules and regulations for marijuana sales continue to be discussed.

c) The B & C's dinner is coming up very soon at the Senior Center. Hope we will all attend. Dinner at 6pm. Plan is to have shorter presentations after the dinner.

5d. LIBRARY DIRECTOR'S REPORT: Colleen Winters reported these items:

a) There are still two vacancies on the Library Commission, although there is one possible applicant. The City Council is currently interviewing applicants for other B & Cs, with some applicants considering serving on the Library Commission.

b) New furniture in Teen area (funds from WCCLS) and in the Children's area (funds from Library Foundation) has arrived, or will very soon. Portions of the Children's area will be painted in the next week (funding from City).

c) January is BEAR month at the library, and a series of events are planned to celebrate it. This will be the 37th BEAR month, and the last with retiring Ann Dondero.

- d) Retirement events for Ann Dondero will be: Mon Jan 25th presentation at City Council meeting with reception following at the library; Jan 27th refreshments for the public following the Teddy Bear parade and before the Shadow Puppet Theater event; Jan 28th reception from 3-5 for city departments, WCCLS staff, WCCLS member libraries, former city employees, and former library employees.
- e) Two new on-call employees have joined the library staff this week (Jim Jatkevicius and Jennie Chamberlin). Both will be helping to fill in for Sergio Lopez who was injured in a motorcycle accident and may be out for some time.
- f) Star Wars Celebration to be held Sat Jan 16 from 2 to 3:30 PM, with games, trivia, and refreshments. Expecting lots of kids. Well-planned event, with help from some volunteers
- g) Facility update: Painting, window leaks, new light fixture over AV collection, new blinds for staff workroom and lunchroom windows, additional lighting in parking lot, and HVAC (heating) issues (not occurring just at the library).
- h) January is usually the biggest circulation month for the library.
- i) The City of Forest Grove is transitioning to a new city logo (3 green oak leaves in a circle). We may see this on documents starting soon.

6. DISCUSSION OF ITEMS:

a) **Election of Officers for 2016.** It was proposed that Pamela Bailey remain as Chair of the Library Commission for 2016, Kathleen Poulsen remain as Vice-Chair, and Jon Youngberg remain as Secretary. **MOTION:** Doug moved, seconded by Kathleen to approve this slate of officers for the Library Commission for 2016. **MOTION CARRIED** by all.

b) **Boards and Commissions dinner** and our Library Commission's presentation (Thurs Jan 21). The Library Commission decided that our presentation should be "short and sweet", follow Pam's redone PowerPoint presentation, and focus on our major 2015 accomplishment: our Strategic Planning work.

c) **Teen Area Proposal.** Colleen said that new furniture for the teen area (colorful, smaller, and low, and that discouraged adults from the area, but might attract younger children) has arrived and was placed last Friday. So far, comments from teens are positive, but that they continue to desire to not have adults and younger children in the area. Colleen said that next month the Library Commission should revisit ways to only have teens in the Teen Area.

d) **"Future of the Forest Grove City Library" Strategic Planning process continues.** The library staff has taken all the input from the two big strategic planning meetings held in 2015, and has done a great deal with it. Combined with

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budget and 2016 planning work. No info to show at this meeting.

7. ANNOUNCEMENT OF NEXT MEETING:

The next Library Commission meeting will be held on Tues Feb 9, 2016 at 6:30PM in the Rogers Conference Room at the Forest Grove Library.

8. ADJOURNMENT:

Hearing no further business, Chair Bailey adjourned the meeting at 8PM.

Minutes respectfully submitted by:

Jon Youngberg, Library Commission Secretary

APPROVED

LIBRARY COMMISSION
MEETING DATE – 6:30PM TUES FEB 9, 2016
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Library Commission approved minutes as presented on March 15, 2016.

1. **CALLED TO ORDER AND ROLL CALL:**

Pamela Bailey, Chair, called the meeting of the Library Commission to order at 6:30PM on Tuesday February 9, 2016.

Members Present: Pamela Bailey, Chair; Doug Martin; Jane Burch-Pesses; Jon Youngberg, Secretary; Kathleen Poulsen, Vice-Chair; Mitchell Faris, Student; Nickie Augustine;

Members Absent: None

Staff: Colleen Winters, Library Director

Council Liaison: Malynda Wenzl

Others: None

2. **ADDITIONS/DELETIONS:** None.

3. **APPROVE LIBRARY COMMISSION MEETING MINUTES OF (Jan 12, 2016):**

MOTION: Doug moved, seconded by Kathleen, to approve the January 12, 2016 minutes as presented. **MOTION CARRIED** by all.

4. **CITIZEN COMMUNICATIONS:** None.

5. **INFORMATIONAL ITEMS:**

5a. **FOUNDATION REPORT:** Colleen Winters shared comments about the activities of the Library Foundation of Forest Grove:

a) The Foundation board met in the last month.

b) There are no current plans for the annual Maggie's Buns fundraiser for the Foundation in 2016. It is usually held early in the year.

c) The Foundation Board and Colleen have not yet heard back from McMenamins Grand Lodge about plans or a date for the annual Foundation fundraiser evening at the Grand Lodge. This has been a popular event for many years.

d) Concerning the future Foundation project "Sense of Arrival" to the old half of the library, especially the Children's Area. The Forest Grove Public Arts Commission will help with this project. Design(s) are to be chosen. It is desired that local artists and crafts people will be chosen for this project. This project will be more than painting existing walls. There might be arches to walk through, mobiles, free-

standing items, all colorful and attractive.

e) Mike Moore has made a proposal to use money left over from the purchase of the grand piano some years ago to landscape the back area of the library. The original agreement regarding the money for purchase of the grand piano allowed use for landscaping too. This project would create a “rain garden”, a nicer, more sustainable, as well as a more attractive area for the community. Mike Moore has talked to an engineer and Clean Water Services. The City would need to approve this project. The City Parks Department will not be in charge of this area, nor be in charge of any maintenance later, and there would be no impact to the City budget related to this project. Some other organization will need to adopt and care for the rain garden. We will hear more about this later.

f) See the Library Foundation of Forest Grove’s web site (www.fglf.org).

5b. FRIENDS REPORT: Colleen Winters shared comments about the activities of the Friends of the Forest Grove Library:

a) The Friends Board will meet tomorrow.

b) No replacements yet for the organizers of the Cultural Series.

c) Online book sales continue to do well, on Amazon.com. Items are shipped all over the world.

d) The next semi-annual Used Book Sale will be held April 18 thru 23, 2016 in the Rogers Room. Many donated items have been gathered for the sale, and as usual, many volunteers will be needed before, during, and after the sale.

e) The Tues Feb 2, 2016 Cultural Series “talk” was a sing-along titled “Remembering Frank Sinatra”. There were 29 attendees.

f) The Tues Mar 1, 2016 Cultural Series talk will be by Russ Dondero with a title of “The twists and turns of the 2016 campaign ...”

g) The Friends web site is at: fglibraryfriends.org.

5c. COUNCIL LIAISON REPORT : Malynda Wenzl shared comments about the activities of the Forest Grove City Council:

a) City Council approved land use plan for the Tokola Project at the old Times-Litho site. (Money portion to be discussed later) There were supporters who spoke. Other speakers wanted more-affordable housing.

b) The Forest Grove Urban Renewal Agency, which is trying to revitalize downtown and other parts of Forest Grove, will hold informative meetings Feb 11th and 18th from 6 to 8PM in the Community Auditorium. The presentation is available at the City’s web site.

c) Rules and regulations for marijuana sales continue to be discussed.

d) The plastic bag “ban” proposal is progressing. There has been a lot of discussion and consensus-building to get the proposal close to approval.

e) A public review of the upcoming traffic circle project at the busy intersection of

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Hwy 47 and Verboort/Purdin roads will be held Feb 23, 2016 from 5 to 7PM at the Community Auditorium.

f) The Annual Town Meeting was held Sat Jan 30, 2016 with 60 attendees. Focus was on the future direction that Forest Grove should choose, including being a bedroom community (Sherwood was discussed as an example), or having more of a mix of development with more jobs, industry, etc. (Tualatin was the example.) Malynda said she would share the results (and ideas) of the ATM later, after they were discussed at the City Council's retreat.

5d. LIBRARY DIRECTOR'S REPORT: Colleen Winters reported these items:

- a) There is still one vacancy on the Library Commission. There may be two possible applicants. The City Council is currently interviewing applicants and may choose and approve another new Library Commission member soon.
- b) The Star Wars Celebration party was held Sat Jan 16 from 2 to 3:30 PM, with games, trivia, and refreshments. About 150 (mostly) children attended. It was a fun and well-planned event, with help from many volunteers.
- c) Retirement events for Ann Dondero were held: Mon Jan 25th presentation at City Council meeting with reception following at the library; Jan 27th refreshments for the public following the Teddy Bear parade and before the Shadow Puppet Theater event; Jan 28th reception from 3-5 for city departments, WCCLS staff, WCCLS member libraries, former city employees, and former library employees. A letter was received from Senator Merkley. State Representative Susan McLain, and Mayor Truax attended.
- d) The library will be closed Mon Feb 15, 2016 (President's Day) for an "in-service" day. Many topics will be discussed with library staff, including: Departmental and City rules, new parking rules for staff, dress code. Also, work will continue on implementation ideas for the 3-year Strategic Plan, and for other projects. Other small staff meetings will occur on various topics.
- e) Sergio Lopez, injured in a motorcycle accident, will be returning to work later in February.
- f) WCCLS "Pool 1" funds are the annual monies given to the City Council for our library, from the Levy and County General Funds. It is an expected annual amount that grows 2% per year. In 2016, this "expected" amount will be larger than for 2015, because of the Nov 2015 levy passage, etc.
- g) WCCLS "Pool 2" funds exist in years where more money is collected than the expected amount. Some years there is no "Pool 2" amount. For 2016, the Forest Grove library might get an extra \$48,000 from Pool 2 funds. A WCCLS committee, with Forest Grove representation, will decide. These Pool 2 funds can't be used for staffing, or to displace current local funding. They are "extra" one-time funds for each library. Colleen will keep us informed about this.
- h) New furniture has recently arrived and been placed in the Children's area of the library. It seems to be just right, receiving lots of good comments. A success!

6. **DISCUSSION OF ITEMS:**

a) **New Library Commission Member.** Jane Burch-Pesses was introduced as the newest member of the Library Commission. She was approved by the City Council on Mon Feb 8, 2016 and attended her first Library Commission meeting tonight. Welcome, Jane!

b) **The Boards and Commissions dinner** was held Thurs night, Jan 21, 2016 at the Senior Center. Doug Martin gave the Library Commission's presentation prepared by Pamela Bailey. The dinner was very nice and the after-dinner presentations were better and shorter than in the past. Malynda mentioned that there are even more ideas for changing this event in future years.

c) **Teen Area Proposal** (on-going topic). Colleen said that the new furniture for the teen area (colorful, smaller, and low, and that discouraged adults from the area, but might attract younger children) has been in place for a month. So far, comments from teens are positive, but that they continue to desire to not have adults and younger children in the area. Adults still go into this area because there is ordinary furniture as well. The signage isn't quite right. We want to encourage more teens to come to the library in general, and also to this one space that is more specially for them. During after-school hours, access to the area could be limited. Colleen wanted to wait and see how things work out. We do not want to have restrictions or do enforcement, as is done in various ways at a few other libraries.

d) **"Future of the Forest Grove City Library" Strategic Planning** process continues. The library staff has taken all the input from the two big strategic planning meetings held in 2015, and is continuing to do a great deal with it. Combined with budget and 2016 planning work. No info to show at this meeting.

7. **ANNOUNCEMENT OF NEXT MEETING:**

The next Library Commission meeting will be our annual retreat, to be held on Tues Mar 1st, 8th, or 15th, 2016 at 6PM (TBD) at Malynda's home. Start reviewing our 2014 goals and thinking about new goals for 2016!

8. **ADJOURNMENT:**

Hearing no further business, Chair Bailey adjourned the meeting at 8PM.

Minutes respectfully submitted by:

Jon Youngberg, Library Commission Secretary

APPROVED

1. CALL TO ORDER:

Vice Chair Ruder called the meeting to order at 7:00 p.m.

Planning Commission Present: Carolyn Hymes, Sebastian B. Lawler, Lisa Nakajima, Dale Smith, Phil Ruder and Hugo Rojas.

Absent: Tom Beck

Staff Present: Jon Holan, Community Development Director; Dan Riordan, Senior Planner; Derek Robbins, City Engineer; Marcia Phillips, Assistant Recorder.

2. PUBLIC MEETING:

2.1 PUBLIC COMMENT PERIOD FOR NON-AGENDA ITEMS:

Dan Martin, 17900 SW Sioux Court, Tualatin, OR 97062. Mr. Martin said he represented a group that has a property on Hawthorne and 26th Avenue, and wanted to get an update on the proposed rezone for that area. He said the property is currently zone RMH at 20.28 units per net acre. The proposed zone change would increase the density to 30 plus units per net acre. He said the property is for sale, but builders want to wait for the higher density.

Mr. Riordan explained that the Planning Commission considered changes to the Development Code to increase density in the Town Center and along the Commercial Corridor that would increase the density from 20 units per net acre to 30 units per net acre. He said there are some property owners in the City who have property within the Residential Multi-family High Density (RMH) zone that are also interested in potentially higher density in that zone. Riordan said currently the RMH Zone is at 20.28 units per net acre, and there is interest in changing that to 30 or more units per acre, but that was outside the scope of the Code update project. Riordan said he believed the property owners were in the audience to make known this interest and to have the City address this.

Mr. Holan said this matter has been put on the next fiscal year's work program. He explained that there was some brief discussion with the Commission about it, but there was not enough exploration of the implication on this residential zone. Holan said this is certainly a matter worth considering for the same reasons the City looked into it in the Town Center and Community Commercial zones. In response to a question from Mr. Martin, Holan stated that no determination has been made to change the zone/density in the area around Hawthorne and 26th Avenue, and suggested that it would be good for the group to talk to staff about this matter.

Mr. Riordan explained that the next fiscal year begins in July.

In response to another question from Mr. Martin, Holan explained that we are looking at 10 to 20 years before light rail comes to Forest Grove.

Mr. Martin asked if there is a need for housing with Pacific University increasing the amount of student housing available.

Mr. Holan explained that staff has no information on housing needs, but staff knows there is a reasonable rationale for thirty units per net acre.

2.2 PUBLIC HEARING:

A. Recommendation to assign the Campus Employment Comprehensive Plan designation to approximately 38 acres of land located south of the BPA power line transmission easement and south of the Elm Street terminus.

Record # 311-15-00032-PLNG.

Vice Chair Ruder opened the public hearing, read the hearing procedures, and asked for disclosure of any conflicts of interest, ex-parte contacts, bias, or abstentions. There were none, and no challenges from the audience. Ruder called for the staff report.

Mr. Riordan stated that there was a lot of data in the Commission's packet, so he would keep the staff report brief. He said in 2014 the Oregon Legislature approved a House Bill 4078 known as the "Grand Bargain" which modified and then enacted urban and rural reserves for Washington County. He explained that this modified the Portland regional urban growth boundary including two locations adjacent to the Forest Grove planning area. Riordan said one UGB modification resulted in the addition of 235 acres of land north of David Hill Road and west of Highway 47. He said land use in the David Hill area is being considered as part of the Westside Planning Project. Riordan said the other modification to the UGB made by the House Bill was the addition of approximately 38 gross acres into the UGB south of the Taylor Industrial Park. It is the second modification that is the subject of the meeting tonight. He said now that the area is in the UGB, the city is required, under Title 11 of the Metro Urban Growth Regional Functional Plan, to assign a Comprehensive Plan (CP) designation to the property which shows how land may be developed in the future and guides the zoning of property when annexed by the City. He said the property is currently zoned Washington County FD-20 and is outside of the revised 100-year floodplain.

Mr. Riordan said the Commission has several alternatives to consider regarding a CP designation for this new UGB area which in general include applying the City's General Industrial, Light Industrial or the new Campus Employment designation created as part of the CP update in 2014. He said the Forest Grove Economic Opportunity Analysis (EOA) identifies a need for campus employment uses, and the UGB addition area provides an opportunity to fulfill this need.

Mr. Riordan explained that the proposed CP amendment will allow for development of the subject property when annexed and zoned by the City. He said future development will impact the Highway 47/Elm Street intersection. Riordan said the traffic analysis included in the Commission's packet assumes that all traffic flows through the Highway 47/Elm Street intersection. He said the Transportation System Plan includes a future extension of Taylor Way from its terminus to Elm Street, and when completed, some trips could be diverted to Fern Hill Road via Taylor Way from the proposed Campus Employment area. Riordan showed a chart and stated that current Hwy. 47 traffic volume is 11,400 vehicles and is projected to increase by about 58% by 2035.

Mr. Riordan said the proposed amendment is consistent with applicable CP policies, the Metro Regional Framework Plan, the Metro Regional Functional Plan and Regional Transportation Functional Plan and is consistent with the Oregon Statewide Land Use Planning goals. Riordan said staff recommends the Planning Commission recommend City Council approval of the proposed Comprehensive Plan Map amendment to designate the 38 acre urban growth boundary addition area Campus Employment.

**PLANNING COMMISSION MEETING MINUTES
FOREST GROVE COMMUNITY AUDITORIUM**

February 15, 2016–7:00 P.M. **PAGE 3 of 7**

In response to a question from Commissioner Nakajima, Mr. Riordan explained that between putting a signal at Elm/Hwy 47 or at Elm St./Hwy 47, Elm would likely be ODOT's preferred location for a signal.

Mr. Holan explained that intersections must meet warrants to be approved by ODOT for a signal. Both intersections meet preliminary signal warrants.

In response to a question from Commissioner Lawler, Mr. Holan said with development 50 peak hourly trips would be generated, the City would require a traffic analysis, and there would also be an ODOT review.

Commissioner Lawler made the comment that the area may not be developed in its entirety due to difficulties with supplying sanitary sewer.

Mr. Riordan said this may be viewed as a constraint since it is not known if there is sufficient depth to supply the entire 38 acres. He explained that the area nearest Elm St. would be the most easy to develop with the area to the east more difficult.

Mr. Holan explained that Clean Water Services does not support pump stations, but it would be possible to have a private pump station.

PROPONENTS:

Tom Vanderzanden, 15903 NW Logi Trail, Hillsboro, OR 97124. Mr. Vanderzanden explained that he was representing the Hayworths, who own property in the subject area. He said was in favor of the amendment to assign the Campus Employment designation to the area as are the Hayworths. Vanderzanden said this is a truly unique setting. He said this could help change the employment situation on Forest Grove, but more needs to be done to make the site more desirable than other areas in Washington County in order to attract business here. He stated that it takes more than a beautiful setting to attract business.

In response to a question from Vice Chair Ruder, Mr. Holan stated that the City's Economic Development Coordinator, Jeff King, has this property designated as "shovel ready". Holan explained that now we need to find funding opportunities such as using System Development Charge fees to help with the sanitary sewer. He encouraged the property owners to talk with Mr. King.

OPPONENTS:

John Malnirich, Owner of Westside Redi-Mix, 451 Elm St., Forest Grove, OR 97116. Mr. Malnirich explained that he has just started a new concrete business in this area and is in favor of

**PLANNING COMMISSION MEETING MINUTES
FOREST GROVE COMMUNITY AUDITORIUM**

February 15, 2016–7:00 P.M. **PAGE 4 of 7**

nice greenways, but sees a problem with people sitting outside at picnic tables eating their lunch and not liking what he is doing. He said he started his business away from the City (unlike VanDoren's), but would feel like the City is surrounding him if this is approved. Malnirich said traffic flow is a problem. He said the extension of Taylor Way would help ease the flow so Elm Street does not get overloaded, and the City may need to go both to Elm St. and Taylor Way.

In response to a question from Vice Chair Ruder, Mr. Malnirich said regarding the Campus Employment design, it would depend on where the greenways are located and which way the doors are facing. He said the buffer for the powerline is pretty good.

Mary Jo Morelli, 1320 Cedar St., Forest Grove, OR 97116. Ms. Morelli said the area should be designated Campus Employment. She said the zoning design does not sound wrong to her. Morelli explained that historically for the past 150 years this has been agricultural land with the A.T. Smith House nearby, so why now and is this the right time. She said if there is a need why cannot it come from the excess General Industrial land in Forest Grove.

Commissioner Lawler made the comment that the 100 year flood plain is a buffer to the agricultural land. He asked if staff had given any consideration to the proximity of the A.T. Smith House.

Mr. Riordan said due to landscape and buffer requirements, staff feels the Campus Employment design would make less impact than the other designations.

OTHER:

Tom Carlson, 42366 SW Sandstrom Rd., Gaston, OR. Mr. Carlson stated that with traffic the way it is, the City is going to have to utilize Taylor Way. He said he likes the look of the business parks in Hillsboro along Brookwood Way, and can see a beautiful trail with a lunch area. He said he cares for the A.T. Smith House sometimes, and getting out of the area by car is difficult especially with the cement trucks coming out of there.

COMMISSION DISCUSSION:

Commissioner Lawler expressed his appreciation for the number of interested people in the audience and the testimony given. He said even the opponents do not sound opposed to the designation, but have brought up some good points to be considered. Lawler said we need to be conscience of what is surrounding this area.

In response to a question from Commissioner Hymes, Mr. Holan explained that the sanitary sewer would be a constraint no matter which designation was assigned to the area. He then showed the TSP map and pointed out the circled areas that need further traffic study, and said that the study could include the entire Highway 47 corridor. Holan stated that whatever designation is assigned,

traffic will be an issue.

Commissioner Lawler stated that there are only two vacant developable sites for offices now, so there is a need.

Commissioner Nakajima said this was discussed at the November meeting, and it was felt that the Campus Employment was the most appropriate zone designation and she is in favor of this.

Vice Chair Ruder said he has not heard a push for General Industrial or Light Industrial and asked for a motion.

Commissioner Hymes made a motion to make a recommendation to the City Council to assign the Campus Employment Comprehensive Plan designation to approximately 38 acres of land located south of the BPA power line transmission easement and south of the Elm Street terminus.

Record # 311-15-00032-PLNG. Commissioner Smith seconded. Motion passed 6-0.

B. Amend the Forest Grove Transportation System Plan to incorporate the preferred alignment of the Council Creek Regional Trail. Record # 311-15-00033-PLNG.

Vice Chair Ruder opened the public hearing at 7:59 p.m. read the hearing procedures and asked for disclosure of any conflicts of interest, ex-parte contacts, bias, or abstentions. There were none, and no challenges from the audience. Ruder called for the staff report.

Mr. Riordan explained that the Council Creek Regional Trail Master Plan (Master Plan) will recommend a comprehensive strategy for the completion of an uninterrupted 15-mile long regional trail from downtown Hillsboro Oregon through the cities of Cornelius and Forest Grove and then north through unincorporated Washington County to the City of Banks. He said the trail study corridor is divided into seven segments based on the differing attributes along the corridor, which include older neighborhoods, business and industrial areas, riparian stream corridors, and rural farmlands. He said public engagement has been ongoing throughout the entire master plan process. Riordan showed a map of the trail corridor, and a close-up of the trail corridor in Forest Grove showing two possible northern routes.

Mr. Robbins explained that the Master Plan was done over a period of two years with several alignments considered. He said some of the alignments are on street, street adjacent and along the railroad right-of-way with room for mass transit in future. Robbins explained that the next step is to look at the alignment in more detail, for instance which side of the street the trail should follow.

Mr. Riordan explained that since the City now has a Trail Master Plan, it is time to update tables and add maps to the Transportation System Plan (TSP). He said the amendments propose to update the wording and add current cost estimates. Mr. Riordan showed a chart of cost estimates. He said the total cost will be \$4.6 million based on the length of the improvement.

Mr. Riordan explained that the trail in Forest Grove primarily follows the Portland – Western Railroad right-of-way east to west terminating near the Senior Center. He said there are two

**PLANNING COMMISSION MEETING MINUTES
FOREST GROVE COMMUNITY AUDITORIUM**

February 15, 2016–7:00 P.M. **PAGE 6 of 7**

possible routes going north with one route at Quince following the north side of Highway 47 and the other route going north on Oak St. to Porter Rd. Riordan said more study and discussion with ODOT will follow.

Mr. Riordan stated that staff recommends that the Planning Commission recommend City Council accept the Council Creek Regional Trail Master Plan and approve the proposed TSP amendment to include the Council Creek Regional Trail and updated cost estimates.

Commissioner Nakajima referred to the Transit Oriented Plan discussed in previous meetings, and stated that the Oak St. alignment seems more appropriate. She asked if staff has expressed a preference.

Mr. Robbins explained that at this point staff wants to keep things flexible.

Commissioner Nakajima asked if the Transit Oriented Plan is still being kept current.

Mr. Riordan explained that it is in the CP as a place holder.

PUBLIC TESTAMONY:

David Morelli, 1320 Cedar St., Forest Grove, OR. Mr. Morelli agreed that the Oak St. alignment is more appropriate with Quince St. alignment being more problematic. He stated that he is in favor of the trail.

In response to a question from the Commission, Mr. Riordan explained that the Commission can state a preference when making the motion for recommendation.

In response to a question from Commissioner Rojas, Mr. Robbins said there can be more or fewer crossings on Hwy 47 depending on the alignment chosen. He said there will be a lot of work to determine how Martin Rd. will be improved – perhaps with a five leg roundabout. Robbins explained that the City will team with ODOT to design the crossings.

Vice Chair Ruder closed the public hearing at 8:19 p.m.

COMMISSION DISCUSSION:

Commissioner Lawler said he was all in favor of this project and there has been a lot of “buy in” from the public. He said it is a great option for people to come into Forest Grove, Roy and Banks.

Commissioner Rojas agreed.

Commissioner Smith stated that he was in favor of the Oak St. alignment.

Commissioner Lawler made a motion to amend the Forest Grove Transportation System Plan to incorporate the preferred alignment of the Council Creek Regional Trail. Record # 311-15-000033-PLNG with a preference for the Oak St. alignment. Commissioner Nakajima seconded. Motion passed 6-0.

2.3 **ACTION ITEMS:** None.

2.4 **WORK SESSION ITEMS:** None.

3.0 **BUSINESS MEETING:**

3.1 **APPROVAL OF MINUTES:** Commissioner Nakajima made a motion to approve the minutes of the January 5, 2016 meeting. Commissioner Lawler seconded. Motion passed 6-0.

3.2 **REPORTS FROM COMMISSIONERS/SUBCOMMITTEES:** Commissioner Lawler stated that on Tuesday, February 23rd there will be a meeting in the public auditorium to discuss the roundabout project.

3.3 **DIRECTOR'S REPORT:**

Mr. Holan informed the Commissioners that on February 22nd the City Council, as the Urban Renewal Agency, will consider the development agreement with Tokola.

Mr. Holan said staff held an "Urban Renewal 101" open house and will repeat this on Thursday February 18th at 6 p.m. in the public auditorium. He said there were only five or six people at the first open house.

Mr. Holan informed the Commission that staff has received the final report from the Westside Planning consultant, so a work session will need to be scheduled.

3.4 **ANNOUNCEMENT OF NEXT MEETING:** Next meeting will be held on March 21, 2016.

3.5 **ADJOURNMENT:** The meeting was adjourned at 8:29 p.m.

Respectfully submitted by:
Marcia Phillips
Assistant Recorder

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A place where families and businesses thrive.

<i>CITY RECORDER USE ONLY:</i>	
AGENDA ITEM #:	<u>3F</u>
FINAL ACTION:	_____

CITY COUNCIL STAFF REPORT

TO: *City Council*

FROM: *Jesse VanderZanden, City Manager*

MEETING DATE: *March 28, 2016*

PROJECT TEAM: *Anna Ruggles, CMC, City Recorder*

SUBJECT TITLE: *Accept Resignation on Public Arts Commission*

ACTION REQUESTED: Ordinance Order Resolution Motion Informational

X all that apply

BACKGROUND:

Helvi Smith, Public Arts Commission (PAC), Term Expiring December 31, 2017, has informed the staff liaison and PAC of her desire to resign from the PAC as per her attached resignation e-mail.

STAFF RECOMMENDATION:

Staff recommends Council accept the above-noted resignation and deem the seat vacant.

----- Original message-----

From: helvi smith

Date: Sun, Mar 20, 2016 12:13 PM

To: Kathleen Leatham; Beverly Maughan;

Subject: Helvi

I have to resign from the Public Arts Commission.

I have no idea when I will be well enough to do the things I want to do, and I think the Commission deserves to have someone available and someone who can add to the group on a regular basis.

I have enjoyed my time on the board and hope to come back at a later time. But right now, I think you need someone who can help to create a quorum, if need be.

Public art is near and dear to my heart and I am so glad to have gotten to know each of you.

Carry on!

Helvi

Term Exp 12/31/17

CERTIFICATE OF APPRECIATION

Awarded to

Helvi Smith

Public Arts Commission

2012 ~ 2016

**In Recognition of Dedicated Service to the
City of Forest Grove and Citizens of Forest Grove**

*The Forest Grove City Council sincerely thanks you for your dedicated service and
the exceptional leadership you exemplified on the Public Arts Commission.*



FOREST GROVE OREGON



A place where families and businesses thrive.

Peter B. Truax, Mayor
Forest Grove City Council
March 28, 2016

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A place where businesses and families thrive

CITY RECORDER USE ONLY:

AGENDA ITEM #: 3-G.

FINAL ACTION: _____

LIQUOR LICENSE RECOMMENDATION

BUSINESS NAME / INDIVIDUAL: Ridgewalker Brewing Company LLC (Jason Cirilincione)

BUSINESS LOCATION ADDRESS: 1921 21st Ave

LIQUOR LICENSE TYPE: BP - Brewery Public House

CITY BUSINESS LICENSE: BL- pending

TYPE OF LICENSE REQUESTED:

Liquor License Application is for the following:

<p>1. LICENSE TYPE:</p> <p><input type="checkbox"/> F-COM – Full On-Premises Sales, Commercial</p> <p><input type="checkbox"/> F-CAT – Full On-Premises Sales, Caterer</p> <p><input type="checkbox"/> F-FPC/F-CLU – Full On-Premises Private Club</p> <p><input type="checkbox"/> F-PL – Full On-Premises Public Location</p> <p><input type="checkbox"/> TSL – Temporary Sales License</p> <p><input type="checkbox"/> Other _____</p>	<p><input type="checkbox"/> L – Limited On-Premises</p> <p><input type="checkbox"/> O – Off-Premises Sales</p> <p><input checked="" type="checkbox"/> BP – Brewery Public House</p> <p><input type="checkbox"/> SEW – Special Event Winery</p> <p><input type="checkbox"/> SEG – Special Event Grower</p> <p><input type="checkbox"/> SED – Special Event Distillery</p>	<p>2. LICENSE FEE:</p> <p><input checked="" type="checkbox"/> New Application \$100</p> <p><input type="checkbox"/> Temporary \$35</p> <p><input type="checkbox"/> Change \$75</p> <p><input type="checkbox"/> Annual Renewal \$35</p> <p><input type="checkbox"/> Event \$22.60</p> <p><input type="checkbox"/> Other _____</p>
<p><input type="checkbox"/> FULL ON-PREMISES SALES:</p> <p>F-COM – Allows sale and service of distilled spirits, malt beverages, wine and cider for consumption on licensed premises and required to have dining seating. Allows sale of malt beverages, wine and cider in securely covered container (growler) for consumption off licensed premises. Also allows applying for temporary use of annual license for special events off-premises.</p>	<p><input checked="" type="checkbox"/> BREWERY - PUBLIC</p> <p>BP – Allows manufacturing malt beverages and to sell and distribute to patrons and wholesalers. Allows sale of malt beverages, wine and cider in securely covered container (growler) for consumption off licensed premises.</p>	<p><input type="checkbox"/> LIMITED ON-PREMISES SALES:</p> <p>L – Allows sale and service of malt beverages, wine and cider for consumption on licensed premises. Allows sale of malt beverages, wine and cider in securely covered container (growler) for consumption off licensed premises. Also allows applying for temporary use of annual license for special events off-premises.</p> <p><input type="checkbox"/> OFF-PREMISES SALES:</p> <p>O – Allows the sale of malt beverages, wine and cider in factory sealed containers for consumption off licensed premises. Also allows applying for sample tasting on premises.</p>

APPLICABLE CRIMINAL RECORDS CHECK:

NONE

SUPPORTING DOCUMENTATION ATTACHED

RECOMMENDED ACTION:

FORWARD WITH APPROVAL

REJECT APPLICATION (Memorandum Required)

J. F. Schutz
J. F. Schutz, Chief of Police/Designee

3/16/2016
Date



OREGON LIQUOR CONTROL COMMISSION
LIQUOR LICENSE APPLICATION

Application is being made for:

LICENSE TYPES

- Full On-Premises Sales (\$402.60/yr)
 - Commercial Establishment
 - Caterer
 - Passenger Carrier
 - Other Public Location
 - Private Club
- Limited On-Premises Sales (\$202.60/yr)
- Off-Premises Sales (\$100/yr)
 - with Fuel Pumps
- Brewery Public House (\$252.60)
- Winery (\$250/yr)
- Other: _____

ACTIONS

- Change Ownership
- New Outlet
- Greater Privilege
- Additional Privilege
- Other Location Change

90-DAY AUTHORITY

Check here if you are applying for a change of ownership at a business that has a current liquor license, or if you are applying for an Off-Premises Sales license and are requesting a 90-Day Temporary Authority

APPLYING AS:

- Limited Partnership
- Corporation
- Limited Liability Company
- Individuals

CITY AND COUNTY USE ONLY

Date application received: _____

The City Council or County Commission:

(name of city or county)

recommends that this license be:

- Granted
- Denied

By: _____
(signature) (date)

Name: _____

Title: _____

OLCC USE ONLY

Application Rec'd by: KD

Date: 3/7/16

90-day authority: Yes No

1. Entity or Individuals applying for the license: [See SECTION 1 of the Guide]

① Ridgewalker Brewing Company LLC ③ _____

② _____ ④ _____

2. Trade Name (dba): Ridgewalker Brewing

3. Business Location: 1921 21st Ave Forest Grove, Washington OR 97116
(number, street, rural route) (city) (county) (state) (ZIP code)

4. Business Mailing Address: 1921 21st Ave Forest Grove OR 97116
(PO box, number, street, rural route) (city) (state) (ZIP code)

5. Business Numbers: 503-621-6320
(phone) (fax)

6. Is the business at this location currently licensed by OLCC? Yes No

7. If yes to whom: N/A Type of License: N/A

8. Former Business Name: N/A

9. Will you have a manager? Yes No Name: Justin Marble
(manager must fill out an Individual History form)

10. What is the local governing body where your business is located? Forest Grove
(name of city or county)

11. Contact person for this application: Christopher M. Ciolicione _____
(name) (phone number(s))

(address) (fax number) (e-mail address)

I understand that if my answers are not true and complete, the OLCC may deny my license application.

Applicant(s) Signature(s) and Date:

① _____ e 3/01/16 ③ _____ Date _____

② _____ Date _____ ④ _____ Date _____



OREGON LIQUOR CONTROL COMMISSION BUSINESS INFORMATION

Please Print or Type

Applicant Name: Ridgewalker Brewing Company LLC Phone: 503-621-6320

Trade Name (dba): Ridgewalker Brewing

Business Location Address: 1921 21st Ave.

City: Forest Grove ZIP Code: 97116

DAYS AND HOURS OF OPERATION

Business Hours:

Sunday 11:00 am to 10:00 pm
 Monday 11:00 am to 10:00 pm
 Tuesday 11:00 am to 10:00 pm
 Wednesday 11:00 am to 10:00 pm
 Thursday 11:00 am to 12:00 am
 Friday 11:00 am to 12:00 am
 Saturday 11:00 am to 12:00 am

Outdoor Area Hours:

Sunday _____ to _____
 Monday _____ to _____
 Tuesday _____ to _____
 Wednesday _____ to _____
 Thursday _____ to _____
 Friday _____ to _____
 Saturday _____ to _____

The outdoor area is used for:

- Food service Hours: _____ to _____
 Alcohol service Hours: _____ to _____
 Enclosed, how _____

The exterior area is adequately viewed and/or supervised by Service Permittees.

(Investigator's Initials)

Seasonal Variations: Yes No If yes, explain: _____

Outdoor Seating as weather allows

ENTERTAINMENT

Check all that apply:

- Live Music Karaoke
 Recorded Music Coin-operated Games
 DJ Music Video Lottery Machines
 Dancing Social Gaming
 Nude Entertainers Pool Tables
 Other: _____

DAYS & HOURS OF LIVE OR DJ MUSIC

Sunday 8:00 pm to 10:00 pm
 Monday _____ to _____
 Tuesday _____ to _____
 Wednesday 8:00 pm to 10:00 pm
 Thursday 8:00 pm to 10:00 pm
 Friday 8:00 pm to 10:00 pm
 Saturday 8:00 pm to 10:00 pm

SEATING COUNT

Restaurant: 100 Outdoor: ~20
 Lounge: _____ Other (explain): _____
 Banquet: _____ Total Seating: 120

OLCC USE ONLY

Investigator Verified Seating: _____ (Y) _____ (N)
 Investigator Initials: _____
 Date: _____

I understand if my answers are not true and complete, the OLCC may deny my license application.

Applicant Signature: _____ Date: 03-01-16

1-800-452-OLCC (6522)

www.oregon.gov/olcc

OREGON LIQUOR CONTROL COMMISSION
LIMITED LIABILITY COMPANY QUESTIONNAIRE



Please Print or Type

LLC Name: Ridgewalker Brewing Company LLC Year Filed: 2016

Trade Name (dba): Ridgewalker Brewing

Business Location Address: 1921 21st Ave

City: Forest Grove ZIP Code: 97116

List Members of LLC:

Percentage of Membership Interest:

List Members of LLC:	Percentage of Membership Interest:
1. <u>Justin Marble</u> <small>(managing member)</small>	<u>25%</u>
2. <u>Storm Brawn</u> <small>(members)</small>	<u>25%</u>
3. <u>Chris Cirlicione</u>	<u>25%</u>
4. <u>Jason Cirlicione</u>	<u>25%</u>
5. _____	_____
6. _____	_____

(Note: If any LLC member is another legal entity, that entity must also complete an LLC, Limited Partnership or Corporation Questionnaire. If the LLC has officers, please list them on a separate sheet of paper with their titles.)

Server Education Designee: Justin Marble DOB: 06/16/1989

I understand that if my answers are not true and complete, the OLCC may deny my license application.

Signature: [Redacted] Member Date: 03/01/16
(title)



A place where businesses and families thrive

CITY RECORDER USE ONLY:

AGENDA ITEM #: A-2

FINAL ACTION: _____

LIQUOR LICENSE RECOMMENDATION

BUSINESS NAME / INDIVIDUAL: Urban Decanter (Becky Kramer)

BUSINESS LOCATION ADDRESS: 2030 Main Street, Suite B

LIQUOR LICENSE TYPE: Full On-Premises Sales, Off-Premises Sales

CITY BUSINESS LICENSE: BL-000106

TYPE OF LICENSE REQUESTED:

Liquor License Application is for the following:

<p>1. LICENSE-TYPE:</p> <p><input checked="" type="checkbox"/> F-COM – Full On-Premises Sales, Commercial</p> <p><input type="checkbox"/> F-CAT – Full On-Premises Sales, Caterer</p> <p><input type="checkbox"/> F-FPC/F-CLU – Full On-Premises Private Club</p> <p><input type="checkbox"/> F-PL – Full On-Premises Public Location</p> <p><input type="checkbox"/> TSL – Temporary Sales License</p> <p><input type="checkbox"/> Other _____</p>	<p><input type="checkbox"/> L – Limited On-Premises</p> <p><input checked="" type="checkbox"/> O – Off-Premises Sales</p> <p><input type="checkbox"/> BP – Brewery Public House</p> <p><input type="checkbox"/> SEW – Special Event Winery</p> <p><input type="checkbox"/> SEG – Special Event Grower</p> <p><input type="checkbox"/> SED – Special Event Distillery</p>	<p>2. LICENSE FEE:</p> <p><input type="checkbox"/> New Application \$100</p> <p><input type="checkbox"/> Temporary \$35</p> <p><input type="checkbox"/> Change \$75</p> <p><input checked="" type="checkbox"/> Annual Renewal \$35</p> <p><input type="checkbox"/> Event \$22.60</p> <p><input type="checkbox"/> Other _____</p>
<p><input checked="" type="checkbox"/> FULL ON-PREMISES SALES:</p> <p>F-COM – Allows sale and service of distilled spirits, malt beverages, wine and cider for consumption on licensed premises and required to have dining seating. Allows sale of malt beverages, wine and cider in securely covered container (growler) for consumption off licensed premises. Also allows applying for temporary use of annual license for special events off-premises.</p>	<p><input type="checkbox"/> BREWERY - PUBLIC</p> <p>BP – Allows manufacturing malt beverages and to sell and distribute to patrons and wholesalers. Allows sale of malt beverages, wine and cider in securely covered container (growler) for consumption off licensed premises.</p>	<p><input type="checkbox"/> LIMITED ON-PREMISES SALES:</p> <p>L – Allows sale and service of malt beverages, wine and cider for consumption on licensed premises. Allows sale of malt beverages, wine and cider in securely covered container (growler) for consumption off licensed premises. Also allows applying for temporary use of annual license for special events off-premises.</p> <p><input checked="" type="checkbox"/> OFF-PREMISES SALES:</p> <p>O – Allows the sale of malt beverages, wine and cider in factory sealed containers for consumption off licensed premises. Also allows applying for sample tasting on premises.</p>

APPLICABLE CRIMINAL RECORDS CHECK:

NONE SUPPORTING DOCUMENTATION ATTACHED

RECOMMENDED ACTION:

FORWARD WITH APPROVAL REJECT APPLICATION (Memorandum Required)

J. F. Schutz
J. F. Schutz, Chief of Police/Designee

 5/9/16
Date



INVESTING IN CLEAN WATER

March 28, 2016

Forest Grove City Council
Diane Taniguchi-Dennis, Deputy General Manager
Mark Jockers, Government & Public Affairs Manager



WHERE WE'VE COME FROM



AUGUST 1959: TUALATIN RIVER AT FARMINGTON



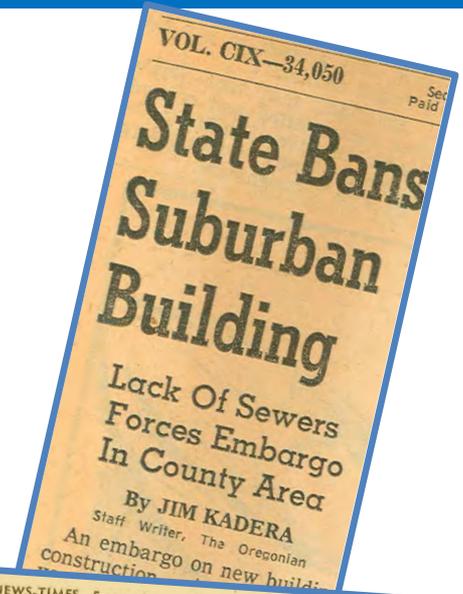
Forest Grove 1969

- Population: 7,160
- Served by FG WWTP
- Largest facility in WaCo
- 2.5 mgd and nearing capacity



1969: STATE TAKES ACTION

- Building moratorium
- Water quality standards
- Conditions for lifting ban
 - Formation of regional authority
 - Secure financing
 - Secure source of water for flow augmentation



THE COMMUNITY MOBILIZES

**YOU HAVE A POLLUTION
CRISIS**



- **CRISIS** The Tualatin River is an open sewer...Solution: The USA is the *only* way!
- **CRISIS** All our streams are polluted... Solution: The USA is the *only* way!
- **CRISIS** Pollution causes disease...Solution: The USA is the *only* way!
- **CRISIS** No new building means unemployment...Solution: The USA is the *only* way!
- **CRISIS** No new building means higher taxes...Solution: The USA is the *only* way!

USA STANDS FOR THE UNIFIED SEWERAGE AGENCY OF WASHINGTON COUNTY

Vote YES • for USA on Feb. 3rd



HELPING the Clean Water for Life Committee inform citizens about the upcoming bond election for the Unified Sewerage Agency are: (from left) Mrs. Roy Neal, Air and Water Resources advisory committee and member of Oregon State League of Women Voters; Mrs. Roland Hundley, president of the Forest Grove League; Mrs. George Mc-

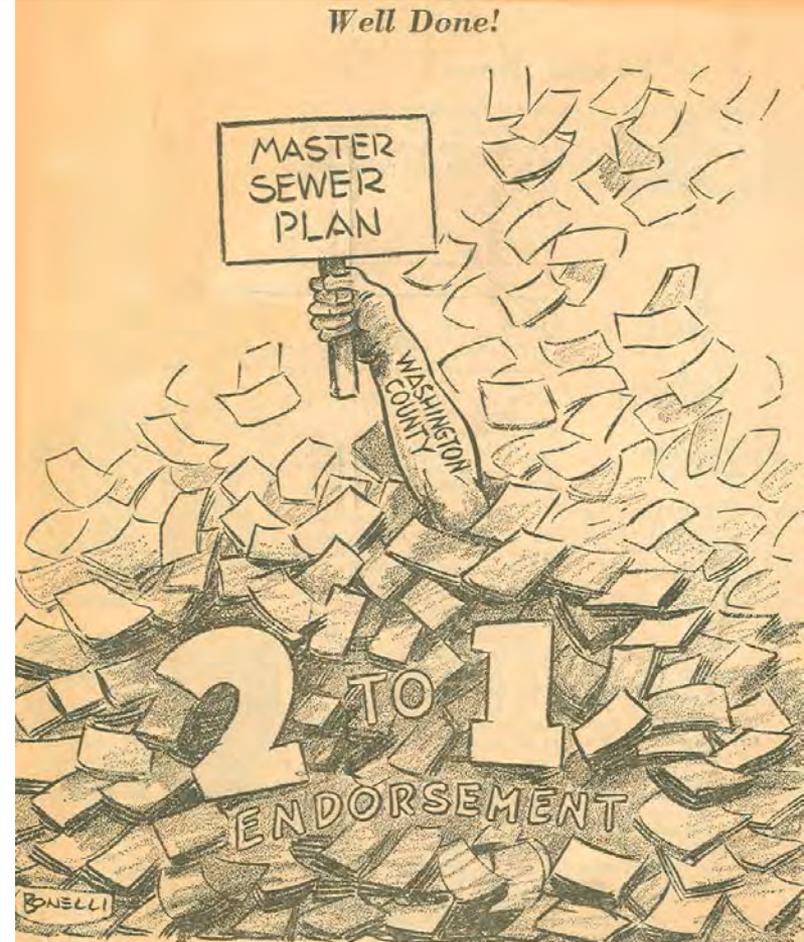
Bride, president of East Washington County League; Mrs. William P. Coburn, water chairman of East Washington County League; Mrs. Lester A. Dow, secretary of Clean Water for Life Committee; Paul Dennis, vice chairman of the committee; M. V. Walker, treasurer.

Clean Water for Life committee opens office



THE PEOPLE SPEAK

- **February 3, 1970:** Voters approve formation of Unified Sewerage Agency
- **February 1970:** Hillsboro agrees to lease water to USA
- **April 21, 1970:** Voters approve \$36 million bond measure to finance construction
- **May 15, 1970:** State lifts ban



1948-1975: A NEW WATER SOURCE TAKE SHAPE

- 54% irrigation—TVID
- 25% water quality and fish habitat—CWS
- 21% drinking water—Hillsboro, Beaverton and Forest Grove



CLEAN WATER SERVICES TODAY



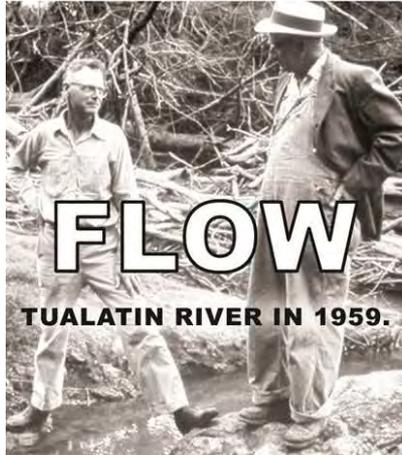
Resource Recovery



Surface Water Management



Water Security & Planning



FLOW
TUALATIN RIVER IN 1959.



MATTERS
TUALATIN RIVER TODAY.

River Flow Management



INVESTING IN THE TUALATIN BASIN

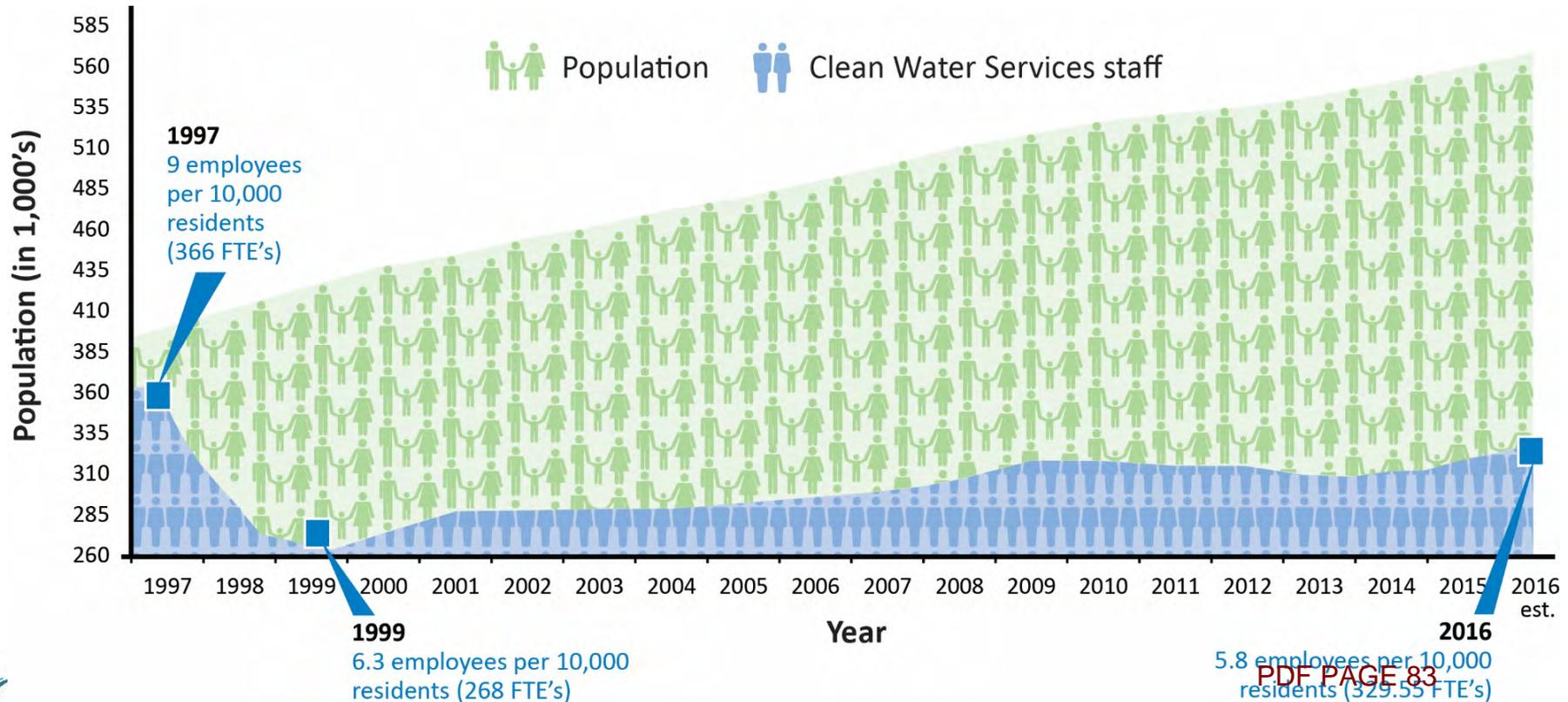
- Infrastructure repair & replacement
- Protecting and restoring the watershed
- Optimizing assets
- Planning for growth
- Resource recovery



INNOVATION & PARTNERSHIPS

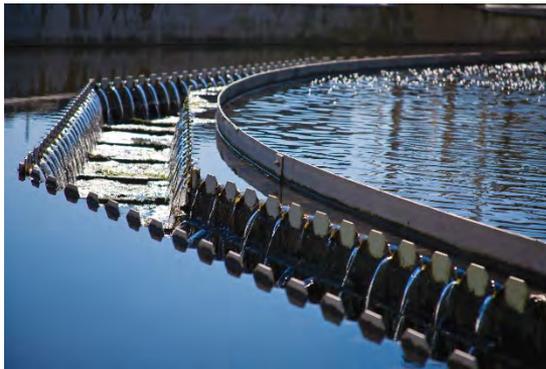


DOING MORE WITH LESS



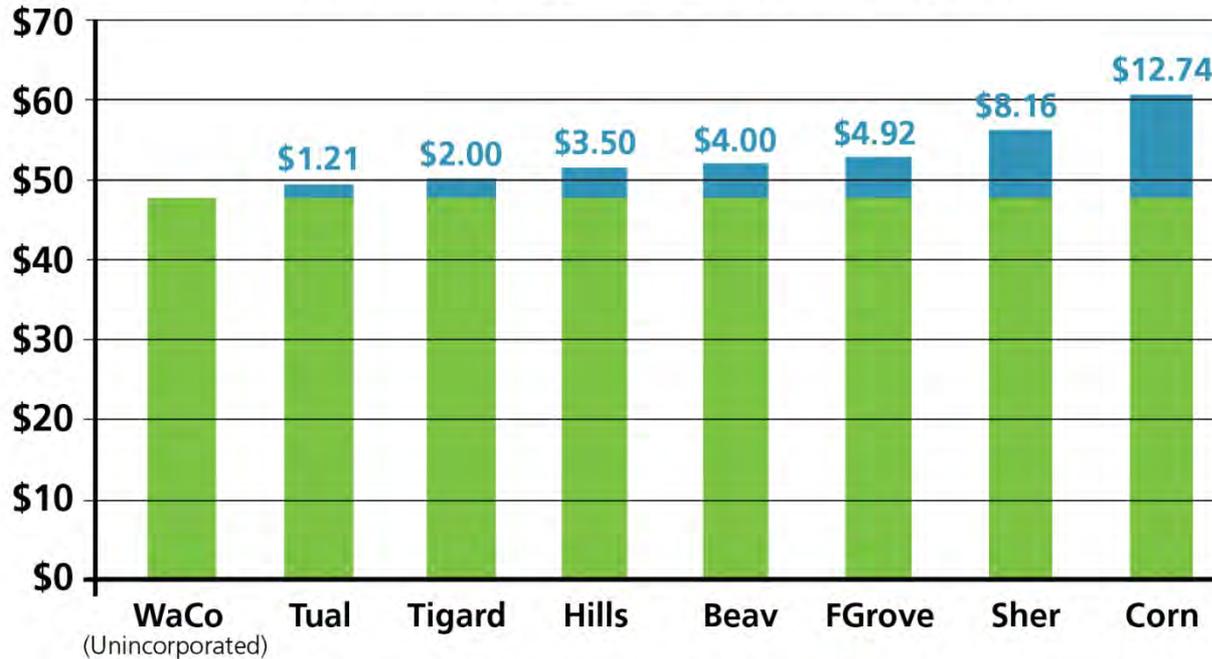
FOREST GROVE & CLEAN WATER SERVICES

Investing in clean water and public health



LOCAL SURCHARGES TO MEET LOCAL NEEDS

2015-16 Sanitary Sewer and SWM Rate and Local Surcharges to Meet Local Needs

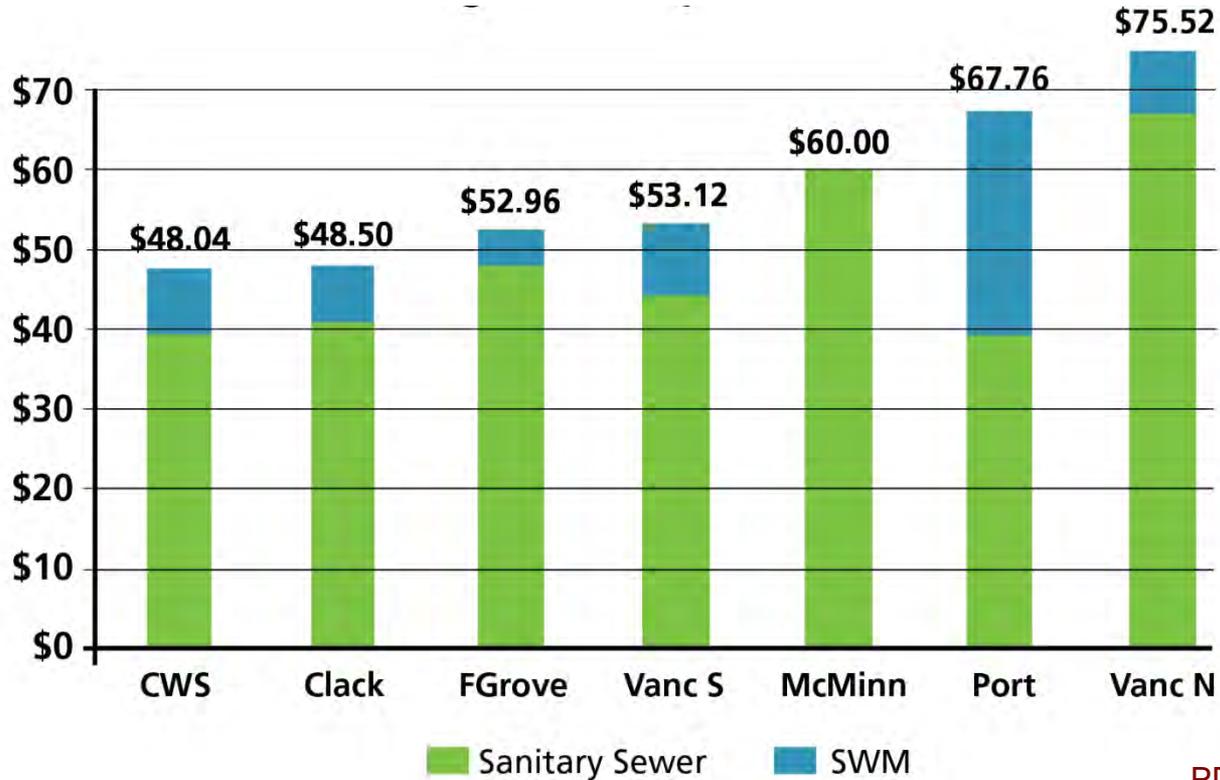


■ CWS Sanitary Sewer / SWM fee (\$48.04)

■ Local Surcharge Fee

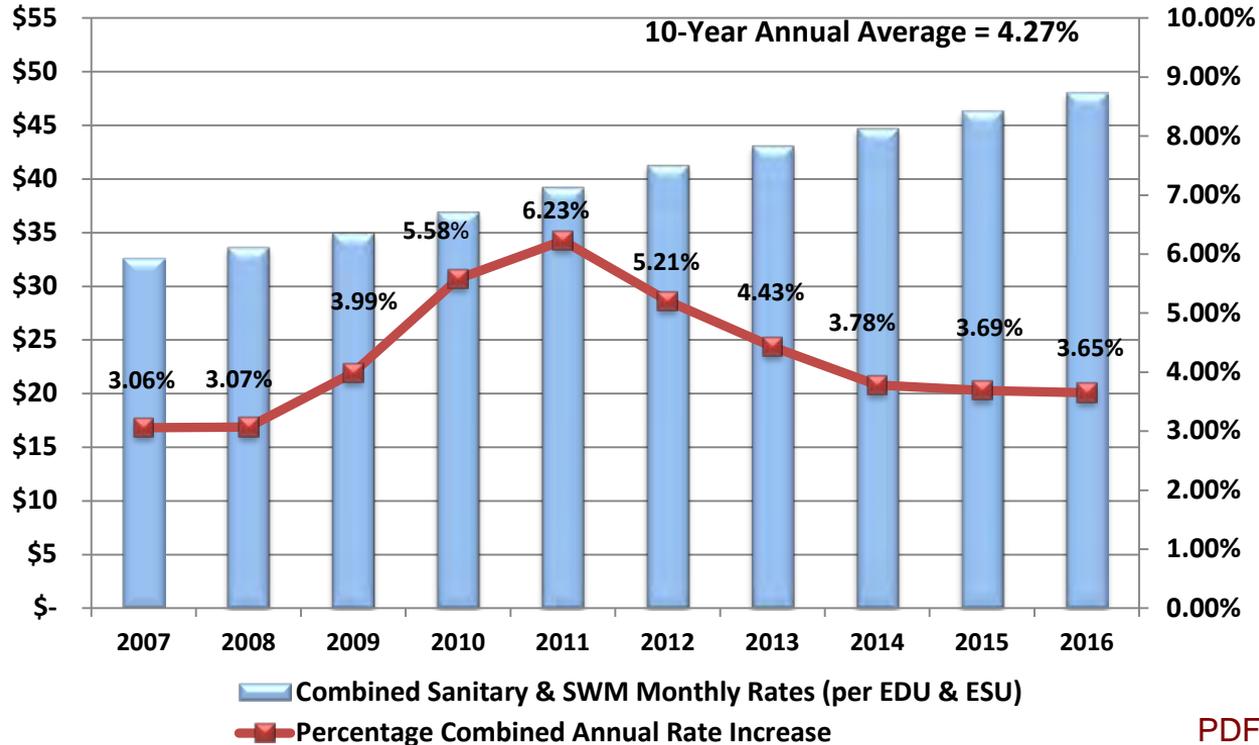


COMPARABLE RATES 2015-16



10-YEAR RATE HISTORY

SANITARY SEWER & SWM AVERAGE MONTHLY RATES (SINGLE FAMILY RESIDENTIAL)



CHALLENGES AHEAD

- Water Supply Planning & Security
- Extreme Weather
- Stormwater
- Planning for Growth
- Aging Infrastructure



CITY OF FOREST GROVE

Financial Report For The Quarter Ending December 31, 2015

This is the quarterly financial report for the quarter ending December 31, 2015. This is the 2nd quarter of the City's fiscal year ending June 30, 2016. If revenues and expenditures were received and spent evenly throughout the year, they would be at 50% received or spent at December 31, 2015.

The quarterly report compares the budget to the year-to-date revenues and expenditures.

The revenues include Beginning Fund Balances for each Fund. For the expenditures, the Contingency and Unappropriated Fund Balances have been removed to show a "true cost" outlook on each fund. When you look at the charts, the total revenues and the total expenditures will not balance. The difference is the Contingency and Unappropriated Fund Balance which are not included with the expenditures.

<u>PAGE</u>	<u>DESCRIPTION</u>
2	General Fund Overview
3	General Fund Revenues
4-6	General Fund Specific Department Breakdowns
7	Building Fund
8	Light and Power Fund
8	Street Fund
9	Sewer Fund
9	Water Fund
10	Surface Water Management Fund
10	Information Systems Fund
11	Equipment Fund
11	Other Funds

General Fund



The General Fund receives the most focus throughout the year and during the budget cycle. The General Fund houses property taxes and the major service areas the public associates with local government – police, fire, parks, aquatics and the library, in addition to administrative and planning functions. Below is a summary of the General Fund. General Fund revenues and expenditures are next broken down into categories and departments, respectively. Line item detail for each revenue source and expense item follows in the detail section of this report.

The General Fund’s financial position is good through the second quarter of FY 2015-16. The Beginning Fund Balance for FY 2015-16 is just over \$6.15 Million which is \$214,000 over what the five-year financial forecast estimated the balance would be for FY 2015-16. Most General Fund revenue for the current fiscal year has been received now that most of the property tax revenue for the current fiscal year have been received.

General Fund Revenue

The following graph displays the General Fund Revenue broken out into separate revenue categories. Budgeted Amounts are shown in BLUE and the Year to Date Actuals are shown in RED.

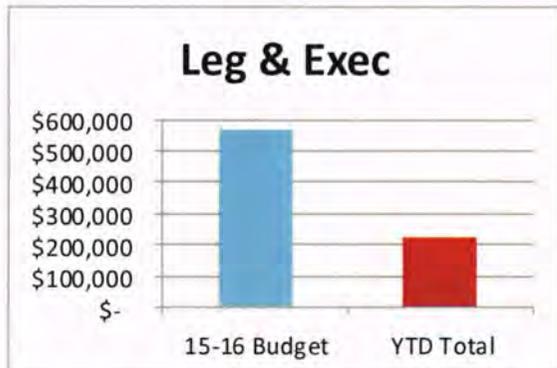
- Property taxes revenues are received mostly in the second quarter of the fiscal year.
- Charges for Services and Transfers/Reimbursement categories are shown as almost achieving their budgeted amounts. This is because the majority of revenues come from other internal funds and departments at the beginning of each fiscal year.
- In the Charges for Services category, the largest revenue item is overhead charges to the enterprise funds.

As noted in the summary, the Unaudited Beginning Fund balance is stated as \$6,156,805 giving the City of Forest Grove a healthy starting point for the fiscal year in 2015-2016 and looking forward into 2016-2017.



General Fund Expenditures

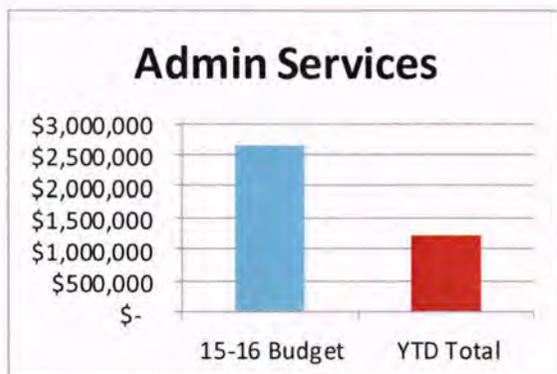
The following charts display each General Fund Department Expenditures. Brief comments are included with each department.



Legislative and Executive

Percentage of Budgeted Expenses Spent: 40%

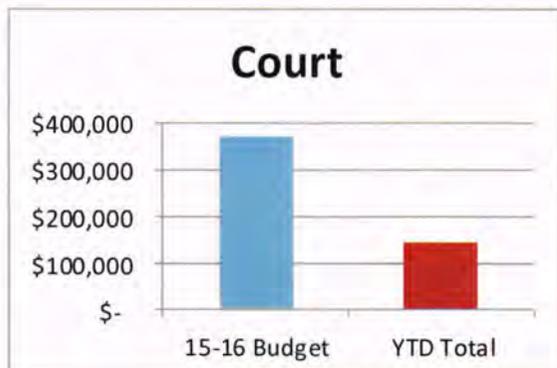
Three specific accounts are overspent (\$9,166). The largest overexpenditure is recruiting expenses for the City Manager which will be offset by not paying the full City Manager salary during the recruitment. No budget transfer is needed or expected.



Administrative Services

Percentage of Budgeted Expenses Spent: 47%

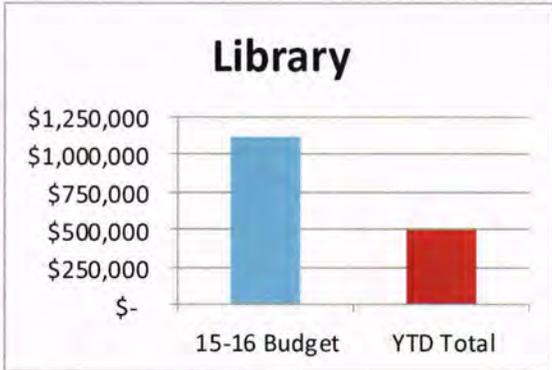
Expenditures are on track and no budget transfer is needed or expected.



Court

Percentage of Budgeted Expenses Spent: 39%

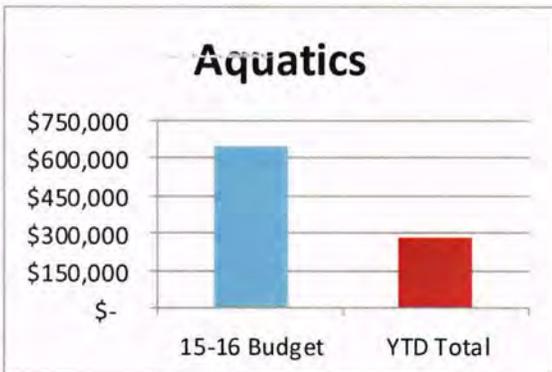
Fine revenue is lower than expected in both the Forest Grove and Cornelius courts due to lower citation volume. Court expenditures are tracking lower due to the lower assessment payments to the State and County due to lower fine revenue.



Library

Percentage of Budgeted Expenses Spent: 45%

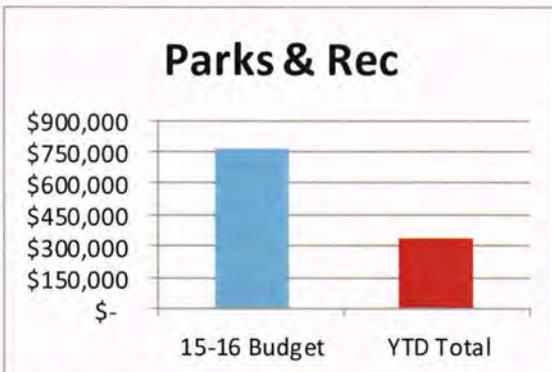
Library expenditures are on target. Temporary employee wages are over spent due to covering for part-time staff vacancies. Part-time wages will be underspent so the two line items will balance each other. No budget transfer is needed or expected.



Aquatics

Percentage of Budgeted Expenses Spent: 44%

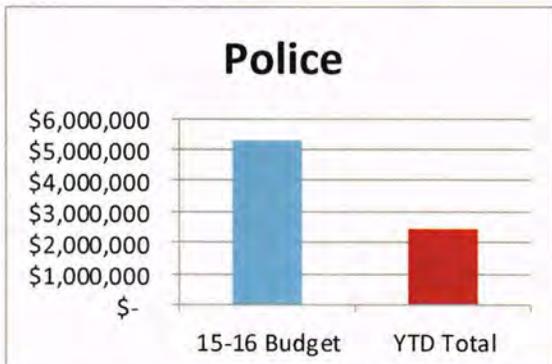
Expenditures are under budget for the year. The Aquatics Center will use savings from other Materials and Services line items to replace some old system controllers which are failing. No budget transfer is needed or expected due to the replacement of the controllers.



Parks and Recreation

Percentage of Budgeted Expenses Spent: 44%

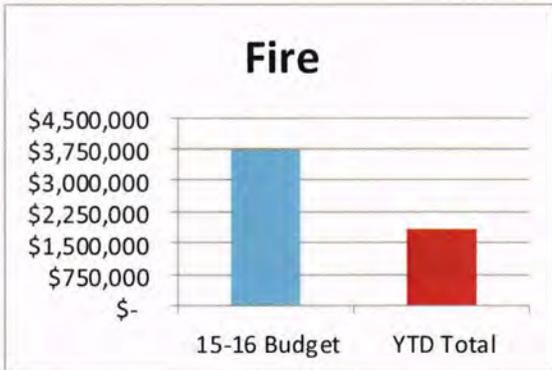
The budget is on track.
No budget transfer is needed or expected.



Police

Percentage of Budgeted Expenses Spent: 46%

Wages are underspent due to vacancies which the department is actively recruiting to fill.



Fire

Percentage of Budgeted Expenses Spent: 49%

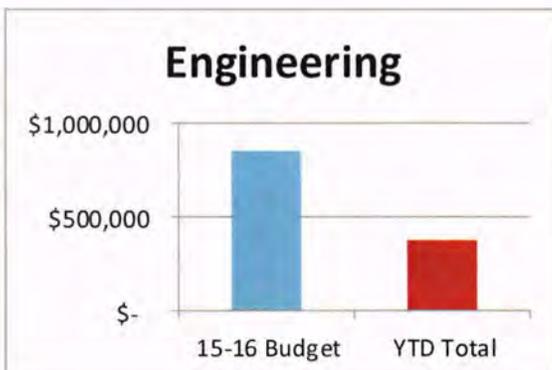
Contracts for Services is overspent by \$16,000 but this line item will be offset by grant revenue to be received.



Planning

Percentage of Budgeted Expenses Spent: 47%

This budget is on track and no budget transfer is needed or expected.



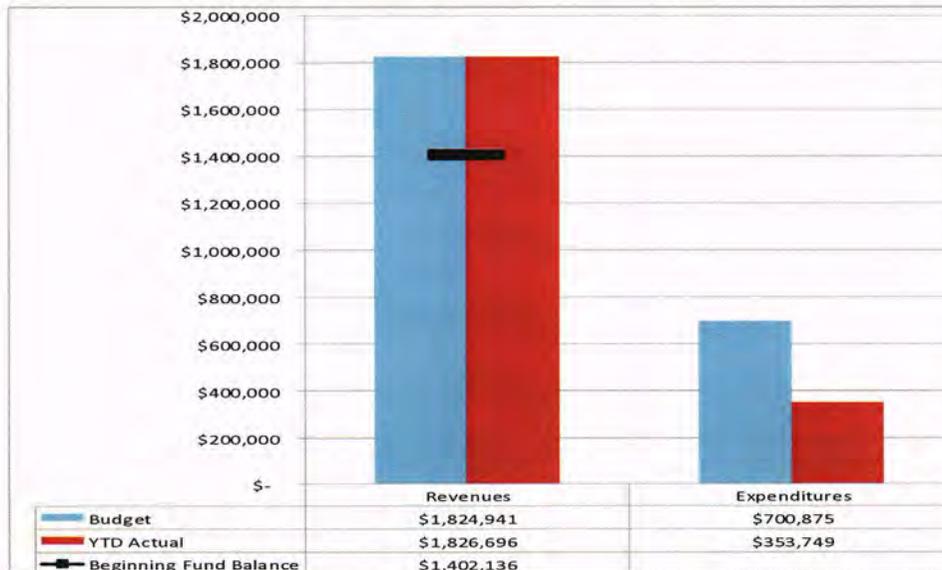
Engineering

Percentage of Budgeted Expenses Spent: 44%

This budget is tracking as expected and no budget transfer is needed or expected.

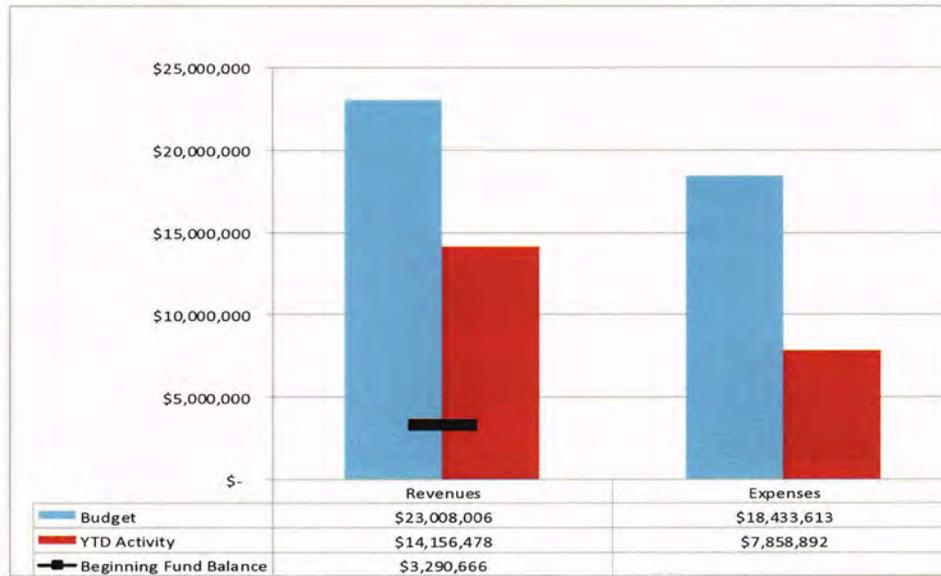
Building Fund

SINGLE FAMILY NEW PERMITS ISSUED							Prior Year Avg.
By Fiscal Year; July 1 through June 30							
	10/11	11/12	12/13	13/14	14/15	15/16	
July	5	6	6	5	16	3	
August	3	6	6	7	7	19	
September	3	11	11	5	17	19	
Q3 Total	11	23	23	17	40	41	23
October	5	8	8	13	5	10	
November	5	5	5	8	8	7	
December	7	8	8	6	1	7	
Q4 Total	17	21	21	27	14	24	20
January	5	12	10	10	3		
February	7	5	23	5	2		
March	6	7	17	2	11		
Q1 Total	18	24	50	17	16	0	25
April	7	8	23	14	14		
May	7	9	22	9	8		
June	4	8	23	15	10		
Q2 Total	18	25	68	38	32	0	36
Grand Total =	64	93	162	99	102	65	



As of December 2015, this fund has received 100% of the Budgeted Revenue. This fund has spent 50% of Budgeted Expenses. Three specific expense accounts are overspent, most notable is "Bank Service Fees" by \$1,066. This is due in part to the new online and state-run building permits system that started this year. There is no need for a budget adjustment due to the fund still being at 50% spent of budgeted expenses.

Light Fund



As of December 2015, this fund has received 62% of the Budgeted Revenue. This fund has spent 43% of Budgeted Expenses. Three accounts are overspent, however since the overall Materials and Services budget is only spent at 43%, it does not require any budget adjustments and none are expected towards the end of the fiscal year at this time.

Street Fund



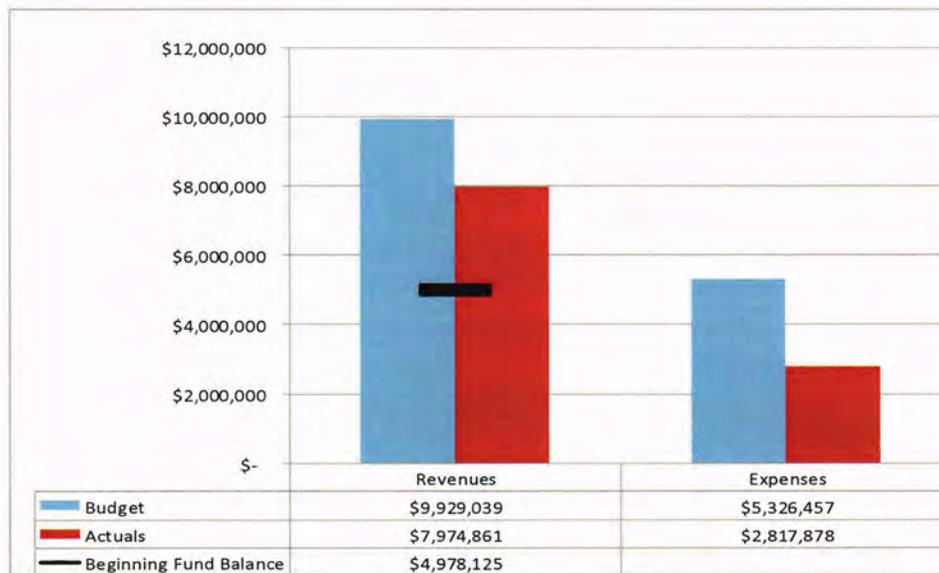
As of December 2015, this fund has received 74% of the Budgeted Revenue. This fund has spent 53% of budgeted expenses. Activity in the Street Fund varies during the year, so staff does not anticipate any budget adjustments be needed toward the end of the fiscal year at this time.

Sewer Fund



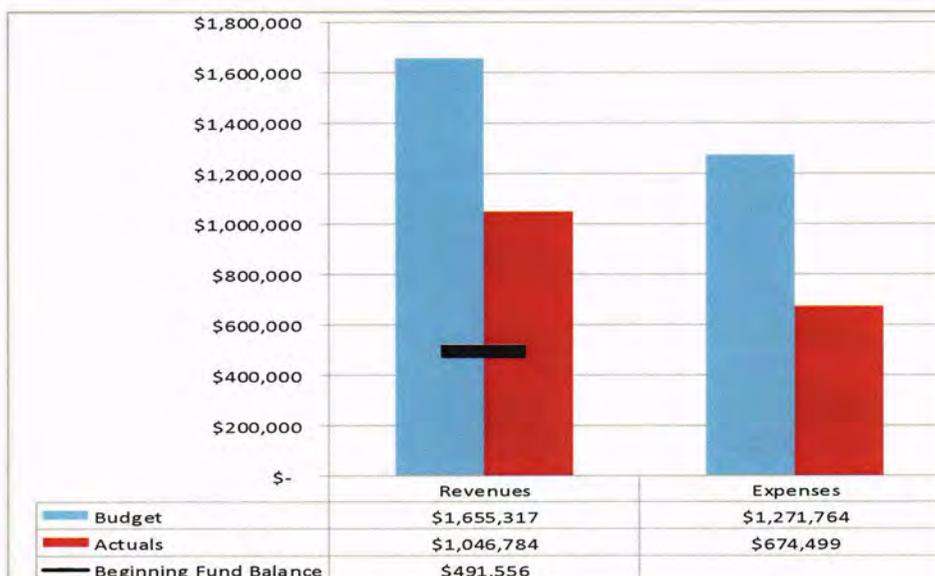
As of December 2015, this fund has received 65% of the Budgeted Revenue. This fund has spent 47% of Budgeted Expenses. No specific expenditure accounts are overspent.

Water Fund



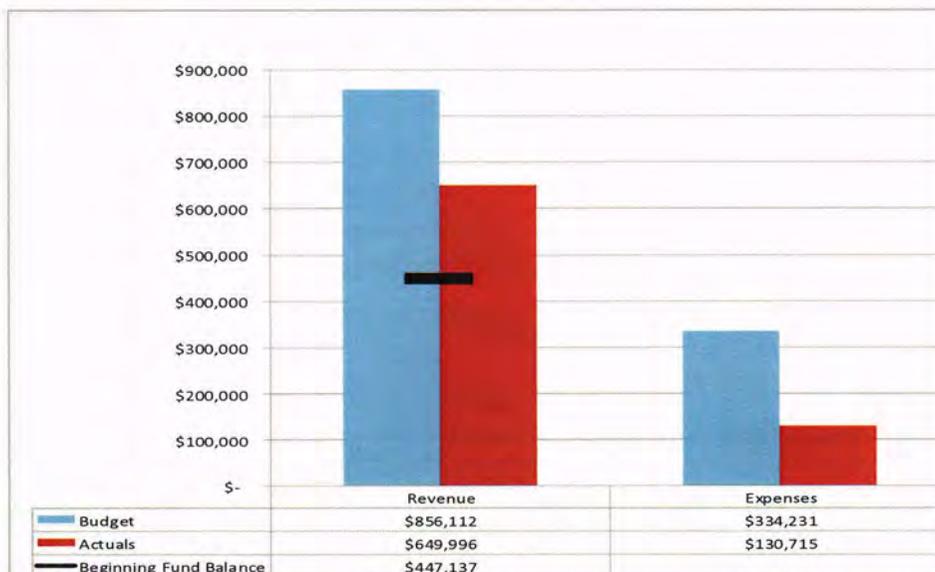
As of December 2015, this fund has received 80% of the Budgeted Revenue (*Note: Water Sales are specifically at 55% of Budgeted Revenue). Timber harvest revenue is coming in slower than anticipated mostly due to the weather. This fund has spent 53% of budgeted expenses. Expenditure line items are tracking as expected and no budget adjustments are expected towards the end of the fiscal year at this time.

Surface Water Management Fund



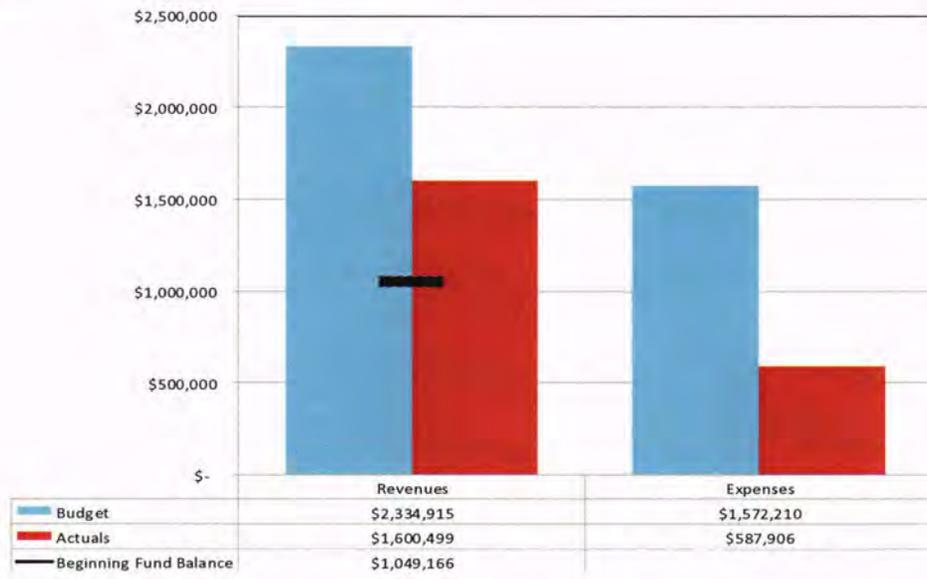
As of December 2015, this fund has received 63% of the Budgeted Revenue and has only spent 53% of Budgeted Expenses. No specific expenditure accounts are overspent. The main revenue item for this fund is the Management Fees received from Utility Bills, which is fairly static. Expenses are well on-track and no specific expense accounts are overspent.

Information Systems Fund



As of December 2015, this fund has received 76% of the Budgeted Revenue and has only spent 39% of Budgeted Expenses. This fund is internal, so revenue should remain fairly static because it comes from other City funds. Overall Materials and Services group is only 38% spent, so there is no need for a budget transfer at this time.

Equipment Fund



As of December 2015, this fund has received 69% of the Budgeted Revenue and has only spent 37% of Budgeted Expenses. No specific expense accounts are overspent and revenue is fairly static since the Equipment Fund is an internal fund within Public Works, which is the fund where all vehicles (other than the Fire and Light & Power Dept. vehicles) are purchased and serviced from.

Other Funds

The revenue in the Funds that collect system development charges is tracking higher than expected because revenue from development was conservatively budgeted.

100 - General Fund

Department #11 - Legislative & Executive

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440025	Copy Service	30	0	(30)	0%
440029	General Fund Spt Svc (522023)	370,888	370,618	(270)	100%
REVENUES TOTAL		370,918	370,618	(300)	100%
EXPENDITURES					
511005	Regular Employee Wages	(294,202)	(110,613)	183,589	38%
511010	Part-Time Employee Wages	(10,680)	(4,081)	6,599	38%
512005	Health & Dental Benefits	(61,056)	(29,228)	31,828	48%
512008	Health Reimb Arrangement	(2,848)	(1,284)	1,564	45%
512010	Retirement	(69,200)	(17,176)	52,024	25%
512015	FICA	(23,324)	(8,524)	14,800	37%
512020	Workers Comp	(248)	(248)	0	100%
512025	Other Benefits	(358)	(2,017)	(1,659)	563%
512030	Other Payroll Taxes	(5,572)	(1,005)	4,567	18%
520110	Operating Supplies	(750)	(778)	(28)	104%
520120	Organization Business Expense	(8,000)	(7,299)	701	91%
520180	Subscriptions & Books	(100)	0	100	0%
520220	Small Equipment	(5,430)	0	5,430	0%
520503	Printing	(3,000)	(1,730)	1,270	58%
520506	Postage	(500)	(91)	409	18%
520509	Telephone	(452)	(208)	244	46%
520521	Public Information	(9,250)	(3,851)	5,399	42%
520524	Publications	(110)	0	110	0%
520530	Memberships	(5,016)	(2,967)	2,049	59%
520533	Recruiting Expenses	(10,000)	(17,479)	(7,479)	175%
520563	Sister City Contribution	(8,900)	(3,470)	5,430	39%
520578	Insurance & Bonds	(1,485)	(1,485)	0	100%
521003	Training/Conferences	(9,825)	(1,967)	7,858	20%
521004	Legislative Conferences & Trainin	(31,700)	(7,966)	23,734	25%
521006	Travel	(200)	0	200	0%
521150	Professional Services	(1,500)	0	1,500	0%
522003	Equipment Maint & Oper Supplies	(500)	0	500	0%
522021	Equipment Fund Charges	(3,218)	(1,610)	1,608	50%
522022	Information Systems Fund Charge	(1,835)	(918)	917	50%
EXPENDITURES TOTAL		(569,259)	(225,995)	343,264	40%
Department Net Totals		(198,341)	144,623		

100 - General Fund

Department #12 - Administration Services

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
430455	Metro Cleanup Grant	833	0	(833)	0%
432280	Healthy Benefits Grant	0	500	500	0%
440022	Lien Searches	14,500	9,453	(5,047)	65%
440025	Copy Service	2,550	16	(2,534)	1%
440028	Passport Execution Fee	8,750	4,575	(4,175)	52%
440029	General Fund Spt Svc (522023)	2,104,657	2,104,657	0	100%
440030	Reserved Parking	1,280	846	(434)	66%
440040	New Account Set-Up Fee	27,500	14,899	(12,601)	54%
440042	Door Hanger Fee	30,000	16,035	(13,965)	53%
440044	Online Phone Pymt Convenience	0	201	201	0%
440046	Failed Payment Arrangement Fee	3,600	2,723	(877)	76%
440048	UT Account Transfer Fees	0	721	721	0%
450056	Business License	37,000	20,616	(16,384)	56%
450057	Other	0	1,500	1,500	0%
471021	Donations	1,500	0	(1,500)	0%
472005	Miscellaneous	0	910	910	0%
480006	Reimbursements	12,500	0	(12,500)	0%
	REVENUES TOTAL	2,244,670	2,177,652	(67,018)	97%
EXPENDITURES					
511005	Regular Employee Wages	(1,053,091)	(462,781)	590,310	44%
511010	Part-Time Employee Wages	(120,845)	(64,394)	56,451	53%
511015	Overtime	(2,000)	(418)	1,582	21%
512005	Health & Dental Benefits	(382,553)	(179,027)	203,526	47%
512008	Health Reimb Arrangement	(13,443)	(6,016)	7,427	45%
512010	Retirement	(223,200)	(96,282)	126,918	43%
512015	FICA	(89,959)	(39,721)	50,238	44%
512020	Workers Comp	(5,611)	(5,611)	0	100%
512025	Other Benefits	(12,851)	(4,301)	8,550	33%
512030	Other Payroll Taxes	(9,031)	(4,061)	4,970	45%
520110	Operating Supplies	(6,600)	(2,009)	4,591	30%
520120	Organization Business Expense	(4,000)	(1,761)	2,239	44%
520150	Utilities	(12,000)	(6,257)	5,743	52%
520180	Subscriptions & Books	(500)	(171)	329	34%
520190	Computer Software	(250)	(153)	97	61%
520200	Computer Software Maintenance	(64,969)	(58,500)	6,469	90%
520220	Small Equipment	(1,600)	(30)	1,570	2%
520503	Printing	(21,100)	(11,070)	10,030	52%
520506	Postage	(72,000)	(46,689)	25,311	65%
520509	Telephone	(6,600)	(2,940)	3,660	45%
520521	Public Information	(2,500)	(147)	2,353	6%
520524	Publications	(1,202)	(225)	977	19%
520530	Memberships	(21,715)	(19,275)	2,440	89%
520533	Recruiting Expenses	(3,000)	(1,083)	1,917	36%
520557	Intergovernmental Services	(4,180)	(3,112)	1,068	74%
520560	Senior Center	(44,000)	(43,416)	584	99%
520578	Insurance & Bonds	(16,365)	(16,365)	0	100%
521003	Training/Conferences	(35,400)	(4,935)	30,465	14%
521006	Travel	(2,000)	(2,051)	(51)	103%
521113	Attorney Services	(57,000)	(13,932)	43,068	24%
521150	Professional Services	(34,900)	(16,884)	18,016	48%
521165	Contracts for Services	(6,000)	(2,810)	3,190	47%

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100 - General Fund

Department #12 - Administration Services

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
521171	Financial Services, Auditing, Invest	(40,250)	(21,965)	18,285	55%
521172	Bank Service Fees	(1,570)	(765)	805	49%
522003	Equipment Maint & Oper Supplies	(37,060)	(11,463)	25,597	31%
522021	Equipment Fund Charges	(8,919)	(4,461)	4,458	50%
522022	Information Systems Fund Charge	(85,324)	(42,662)	42,662	50%
522303	Custodial	(6,000)	(3,978)	2,022	66%
522306	Rents & Leases	(1,475)	0	1,475	0%
522309	Building/Facility Rental	(21,860)	(10,928)	10,932	50%
522312	Facility Maintenance Supplies	(12,600)	(2,026)	10,574	16%
522315	Facility Mnt/Repairs	(88,455)	(20,749)	67,706	23%
	EXPENDITURES TOTAL	(2,633,978)	(1,235,420)	1,398,558	47%
	Department Net Totals	(389,308)	942,233		

100 - General Fund

Department #13 - Municipal Court

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
460115	Parking Fines (PK)	8,900	7,731	(1,169)	87%
460120	Traffic Fines (TR)	203,450	103,180	(100,270)	51%
460121	Cornelius Court Revenue	160,250	72,584	(87,666)	45%
460125	PD Ordinance Fines	1,250	525	(726)	42%
460130	Marijuana Fines/Fees (ML)	860	695	(165)	81%
460135	Minor in Possession Fines/Fees (I)	5,130	1,983	(3,147)	39%
470105	Interest	0	683	683	0%
472005	Miscellaneous	0	61	61	0%
480006	Reimbursements	2,160	2,107	(53)	98%
REVENUES TOTAL		382,000	189,549	(192,451)	50%
EXPENDITURES					
511005	Regular Employee Wages	(107,834)	(49,388)	58,446	46%
511010	Part-Time Employee Wages	(17,952)	(3,960)	13,992	22%
512005	Health & Dental Benefits	(38,475)	(19,355)	19,120	50%
512008	Health Reimb Arrangement	(1,299)	(639)	660	49%
512010	Retirement	(20,684)	(9,359)	11,325	45%
512015	Fica	(9,623)	(3,980)	5,643	41%
512020	Workers Comp	(149)	(149)	0	100%
512025	Other Benefits	(1,329)	(408)	921	31%
512030	Other Payroll Taxes	(969)	(414)	555	43%
520110	Operating Supplies	(1,000)	(204)	796	20%
520120	Organization Business Expense	(200)	(26)	174	13%
520200	Computer Software Maintenance	(7,360)	(5,987)	1,373	81%
520220	Small Equipment	(2,738)	(2,847)	(109)	104%
520503	Printing	(400)	(34)	366	8%
520506	Postage	(1,665)	(762)	903	46%
520509	Telephone	(250)	(70)	180	28%
520521	Public Information	(100)	0	100	0%
520524	Publications	(500)	0	500	0%
520530	Memberships	(195)	0	195	0%
520539	Assessment - County	(26,050)	(9,811)	16,240	38%
520557	Intergovernmental Services	(30,800)	3	30,803	0%
520566	Assessment - State	(81,675)	(32,298)	49,377	40%
520578	Insurance & Bonds	(640)	(640)	0	100%
521003	Training/Conferences	(2,600)	(310)	2,290	12%
521150	Professional Services	(7,184)	(1,583)	5,601	22%
521172	Bank Service Fees	(2,950)	(747)	2,203	25%
522022	Information Systems Fund Charge	(4,971)	(2,486)	2,485	50%
522309	Building/Facility Rental	(784)	(394)	390	50%
EXPENDITURES TOTAL		(370,376)	(145,846)	224,530	39%
Department Net Totals		11,624	43,703		

100 - General Fund

Department #14 - Library

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
420035	W.C.C.L.S.	742,491	595,606	(146,885)	80%
430601	Public Library Support Grant	3,500	0	(3,500)	0%
440019	WCCLS Collection Agency Charg	750	350	(400)	47%
440021	Library Charges	5,500	2,950	(2,550)	54%
440023	Print Fees	3,200	1,816	(1,384)	57%
440025	Copy Service	1,020	743	(277)	73%
440301	Rental Income	1,300	730	(570)	56%
450057	Other	0	110	110	0%
460500	Library Late Fines	24,000	12,093	(11,907)	50%
471020	Unrestricted Donations	100	41	(59)	41%
471022	Restricted Library Memorials	500	100	(400)	20%
	REVENUES TOTAL	782,361	614,539	(167,822)	79%
EXPENDITURES					
511005	Regular Employee Wages	(392,397)	(175,491)	216,906	45%
511010	Part-Time Employee Wages	(262,042)	(108,394)	153,648	41%
511020	Temporary Employee Wages	(5,000)	(8,651)	(3,651)	173%
512005	Health & Dental Benefits	(103,359)	(50,295)	53,064	49%
512008	Health Reimb Arrangement	(6,465)	(2,758)	3,707	43%
512010	Retirement	(108,920)	(46,915)	62,005	43%
512015	FICA	(50,447)	(21,534)	28,913	43%
512020	Workers Comp	(524)	(524)	0	100%
512025	Other Benefits	(6,379)	(1,396)	4,983	22%
512030	Other Payroll Taxes	(5,114)	(2,275)	2,839	44%
520110	Operating Supplies	(5,500)	(945)	4,555	17%
520120	Organization Business Expense	(500)	(420)	80	84%
520140	Library Materials	(88,950)	(45,279)	43,671	51%
520150	Utilities	(2,050)	(969)	1,081	47%
520220	Small Equipment	(6,500)	(2,561)	3,939	39%
520506	Postage	(1,650)	(555)	1,095	34%
520509	Telephone	(1,360)	(653)	707	48%
520521	Public Information	(800)	(328)	472	41%
520530	Memberships	(750)	(460)	290	61%
520557	Intergovernmental Services	(2,520)	(814)	1,706	32%
520578	Insurance & Bonds	(7,818)	(7,818)	0	100%
521003	Training/Conferences	(4,000)	0	4,000	0%
521006	Travel	(750)	0	750	0%
521150	Professional Services	(3,000)	0	3,000	0%
521165	Contracts for Services	(3,500)	(301)	3,200	9%
521172	Bank Service Fees	(1,010)	(399)	611	39%
522003	Equipment Maint & Oper Supplies	(4,550)	(813)	3,737	18%
522021	Equipment Fund Charges	(663)	(333)	330	50%
522022	Information Systems Fund Charge	(23,872)	(11,936)	11,936	50%
522312	Facility Maintenance Supplies	(4,400)	(2,172)	2,228	49%
522315	Facility Mnt/Repairs	(6,700)	(3,513)	3,187	52%
	EXPENDITURES TOTAL	(1,111,490)	(498,501)	612,989	45%
	Department Net Totals	(329,129)	116,038		

100 - General Fund

Department #15 - Aquatics

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440004	Swimming Pool	319,000	155,323	(163,677)	49%
440010	Lockers/Vending Machines	13,000	5,898	(7,102)	45%
REVENUES TOTAL		332,000	161,221	(170,779)	49%
EXPENDITURES					
511005	Regular Employee Wages	(142,155)	(64,895)	77,260	46%
511010	Part-Time Employee Wages	(199,117)	(89,567)	109,550	45%
511015	Overtime	0	(24)	(24)	0%
512005	Health & Dental Benefits	(40,892)	(17,608)	23,284	43%
512008	Health Reimb Arrangement	(2,231)	(955)	1,276	43%
512010	Retirement	(40,283)	(17,635)	22,648	44%
512015	FICA	(26,107)	(11,199)	14,908	43%
512020	Workers Comp	(5,059)	(5,059)	0	100%
512025	Other Benefits	(3,762)	(697)	3,065	19%
512030	Other Payroll Taxes	(2,759)	(1,245)	1,514	45%
520110	Operating Supplies	(30,000)	(12,582)	17,418	42%
520120	Organization Business Expense	(400)	(347)	53	87%
520130	Personnel Uniforms & Equipment	(400)	0	400	0%
520150	Utilities	(75,000)	(22,425)	52,575	30%
520220	Small Equipment	(5,600)	(2,624)	2,976	47%
520503	Printing	(2,800)	(1,620)	1,180	58%
520506	Postage	(550)	(115)	435	21%
520509	Telephone	(600)	(380)	220	63%
520521	Public Information	(1,800)	(1,092)	709	61%
520530	Memberships	(400)	0	400	0%
520557	Intergovernmental Services	(1,350)	(1,428)	(78)	106%
520578	Insurance & Bonds	(6,152)	(6,152)	0	100%
521003	Training/Conferences	(2,000)	(646)	1,354	32%
521150	Professional Services	(1,000)	0	1,000	0%
521168	Misc Medical Services	0	(126)	(126)	0%
521172	Bank Service Fees	(2,200)	(1,727)	473	78%
522003	Equipment Maint & Oper Supplies	(1,500)	(49)	1,451	3%
522021	Equipment Fund Charges	(663)	(333)	330	50%
522022	Information Systems Fund Charge	(5,645)	(2,823)	2,822	50%
522312	Facility Maintenance Supplies	(13,000)	(3,209)	9,791	25%
522315	Facility Mnt/Repairs	(29,900)	(17,897)	12,003	60%
EXPENDITURES TOTAL		(643,325)	(284,461)	358,864	44%
Department Net Totals		(311,325)	(123,240)		

100 - General Fund

Department #16 - Parks & Recreation

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440007	Recreation User Fees	43,000	13,798	(29,202)	32%
440029	General Fund Spt Svc (522023)	128,397	128,397	0	100%
	REVENUES TOTAL	171,397	142,195	(29,202)	83%
EXPENDITURES					
511005	Regular Employee Wages	(331,006)	(150,245)	180,761	45%
511015	Overtime	0	(20)	(20)	0%
511020	Temporary Employee Wages	(41,171)	(16,855)	24,316	41%
512005	Health & Dental Benefits	(96,186)	(37,181)	59,005	39%
512008	Health Reimb Arrangement	(4,051)	(1,946)	2,105	48%
512010	Retirement	(74,501)	(33,931)	40,570	46%
512015	FICA	(28,471)	(12,439)	16,032	44%
512020	Workers Comp	(6,569)	(6,569)	0	100%
512025	Other Benefits	(3,257)	(1,108)	2,149	34%
512030	Other Payroll Taxes	(2,854)	(1,277)	1,577	45%
520110	Operating Supplies	(2,800)	(15)	2,785	1%
520120	Organization Business Expense	(100)	(21)	79	21%
520130	Personnel Uniforms & Equipment	(2,000)	(1,131)	869	57%
520150	Utilities	(9,000)	(3,112)	5,888	35%
520220	Small Equipment	(10,000)	(6,342)	3,658	63%
520506	Postage	(150)	0	150	0%
520509	Telephone	(2,800)	(363)	2,437	13%
520521	Public Information	(200)	0	200	0%
520530	Memberships	(1,800)	(1,434)	366	80%
520557	Intergovernmental Services	(400)	(100)	300	25%
520578	Insurance & Bonds	(4,382)	(4,382)	0	100%
521003	Training/Conferences	(2,500)	(403)	2,097	16%
521150	Professional Services	(12,000)	(7,456)	4,544	62%
521168	Misc Medical Services	0	(183)	(183)	0%
522003	Equipment Maint & Oper Supplies	(3,000)	(251)	2,749	8%
522012	Fuel/Oil	(600)	(447)	153	74%
522021	Equipment Fund Charges	(83,027)	(41,513)	41,514	50%
522022	Information Systems Fund Charge	(1,140)	(570)	570	50%
522306	Rents & Leases	(500)	0	500	0%
522312	Facility Maintenance Supplies	(24,000)	(7,379)	16,621	31%
522315	Facility Mnt/Repairs	(20,000)	(1,118)	18,882	6%
	EXPENDITURES TOTAL	(768,465)	(337,790)	430,675	44%
	Department Net Totals	(597,068)	(195,594)		

100 - General Fund

Department #18 - Non-Departmental

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
411003	Property Taxes	5,399,003	5,115,154	(283,849)	95%
411010	Local Option Levy	2,192,655	2,072,062	(120,593)	95%
411150	Property Tax Prior Years	125,000	93,621	(31,379)	75%
413001	Franchise Tax	570,000	160,433	(409,567)	28%
420020	State Revenue Sharing	249,857	110,141	(139,716)	44%
420055	Metro Construction Excise Tax	1,500	1,071	(429)	71%
422045	Transient Room Tax	85,000	63,112	(21,888)	74%
422055	SD15 Construction Excise Tax	7,200	5,642	(1,558)	78%
470105	Interest	65,000	29,808	(35,192)	46%
470150	Enterprise Zone Repayment	0	200	200	0%
480050	In-Lieu of Tax	1,128,831	1,128,831	0	100%
481005	Transfer from Other Funds	76,063	76,063	0	100%
495005	Fund Bal Avail. for Approp.	5,942,538	6,156,805	214,267	104%
	REVENUES TOTAL	15,842,647	15,012,944	(829,703)	95%
EXPENDITURES					
570127	Transfer to Other Funds	(457,500)	(961)	456,539	0%
580206	Contingency	(750,000)	0	750,000	0%
590304	Unapp Fund Balance	(4,579,697)	0	4,579,697	0%
	EXPENDITURES TOTAL	(5,787,197)	(961)	5,786,236	0%
	Department Net Totals	10,055,450	15,011,983		

100 - General Fund

Department #21 - Police

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
420005	Alcoholic Beverages	356,939	165,564	(191,375)	46%
430329	SPF SIG Grant	4,500	0	(4,500)	0%
430335	DEC/DUII Police Grant	3,600	2,588	(1,012)	72%
440025	Copy Service	3,100	1,028	(2,072)	33%
450050	Liquor Licenses	2,250	827	(1,423)	37%
450051	Police Permits	100	666	566	666%
450057	Other	0	207	207	0%
450225	Impound Fees	100	118	18	118%
450230	Abatement Cost Billed	150	0	(150)	0%
460105	State Court Fines	42,000	17,820	(24,180)	42%
460116	Immobilization Fees	0	118	118	0%
471021	Donations	0	3,000	3,000	0%
472005	Miscellaneous	0	2,691	2,691	0%
480006	Reimbursements	0	9,791	9,791	0%
480010	SRO Reimbursement	66,225	0	(66,225)	0%
480011	Overtime Reimbursement	0	910	910	0%
	REVENUES TOTAL	478,964	205,327	(273,637)	43%
EXPENDITURES					
511005	Regular Employee Wages	(2,778,768)	(1,139,714)	1,639,054	41%
511010	Part-Time Employee Wages	(27,699)	(6,482)	21,217	23%
511015	Overtime	(158,294)	(78,430)	79,864	50%
512005	Health & Dental Benefits	(584,127)	(269,823)	314,304	46%
512008	Health Reimb Arrangement	(24,435)	(25,997)	(1,562)	106%
512010	Retirement	(565,063)	(241,348)	323,715	43%
512015	FICA	(226,804)	(92,313)	134,491	41%
512020	Workers Comp	(49,443)	(49,443)	0	100%
512025	Other Benefits	(36,813)	(8,948)	27,865	24%
512030	Other Payroll Taxes	(22,581)	(9,358)	13,223	41%
520110	Operating Supplies	(53,475)	(28,617)	24,858	54%
520120	Organization Business Expense	(4,255)	(2,033)	2,222	48%
520130	Personnel Uniforms & Equipment	(23,660)	(10,099)	13,561	43%
520150	Utilities	(3,000)	(1,642)	1,358	55%
520190	Computer Software	(4,390)	(4,896)	(506)	112%
520220	Small Equipment	(7,000)	(6,494)	506	93%
520225	Reserve Officer Expenses	(9,000)	(2,416)	6,584	27%
520270	Miscellaneous	(500)	(573)	(73)	115%
520274	Hands and Words Project	(3,273)	0	3,273	0%
520503	Printing	(3,650)	(640)	3,010	18%
520506	Postage	(2,000)	(1,436)	564	72%
520509	Telephone	(17,745)	(8,838)	8,907	50%
520518	Volunteer Reimburseable Expens	(300)	0	300	0%
520521	Public Information	(4,775)	(4,939)	(164)	103%
520524	Publications	(2,025)	(190)	1,835	9%
520530	Memberships	(1,500)	0	1,500	0%
520557	Intergovernmental Services	(23,140)	(22,075)	1,065	95%
520558	WCCCA	(264,378)	(195,295)	69,083	74%
520578	Insurance & Bonds	(64,580)	(64,580)	0	100%
521003	Training/Conferences	(29,675)	(20,928)	8,747	71%
521150	Professional Services	(14,135)	(3,676)	10,459	26%
521168	Misc Medical Services	(3,075)	(5,182)	(2,107)	169%
522003	Equipment Maint & Oper Supplies	(6,310)	(7,116)	(806)	113%

PIDPPAGE106

100 - General Fund

Department #21 - Police

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
522009	Vehicle Maint & Oper Supplies	(1,800)	(246)	1,554	14%
522010	Vehicle Maint External	(700)	(111)	590	16%
522012	Fuel/Oil	(100)	(89)	11	89%
522021	Equipment Fund Charges	(200,727)	(100,365)	100,362	50%
522022	Information Systems Fund Charge	(50,801)	(25,401)	25,400	50%
522306	Rents & Leases	(6,060)	0	6,060	0%
522312	Facility Maintenance Supplies	(1,600)	(1,147)	453	72%
522315	Facility Mnt/Repairs	(3,520)	(2,918)	602	83%
550181	Major Tools & Work Equipment	(25,000)	(11,621)	13,379	46%
	EXPENDITURES TOTAL	(5,310,176)	(2,455,417)	2,854,759	46%
	Department Net Totals	(4,831,212)	(2,250,090)		

100 - General Fund

Department #22 - Fire

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
420015	Cigarette	26,577	16,022	(10,555)	60%
420050	Rural Fire District	559,637	0	(559,637)	0%
430214	Homeland Security Grant	0	30,300	30,300	0%
440025	Copy Service	100	73	(27)	73%
446010	Transport Revenue	2,500	3,727	1,227	149%
472005	Miscellaneous	0	15,169	15,169	0%
480008	Cornelius Fire Chief Reimbursemen	82,863	33,528	(49,335)	40%
480015	Fire Dept Reimbursement	8,000	29,573	21,573	370%
	REVENUES TOTAL	679,677	128,392	(551,285)	19%
EXPENDITURES					
511005	Regular Employee Wages	(1,714,594)	(782,514)	932,080	46%
511010	Part-Time Employee Wages	(20,658)	(9,391)	11,267	45%
511015	Overtime	(111,696)	(68,182)	43,514	61%
511019	Volunteer Firefighter Stipend	(62,880)	(31,276)	31,604	50%
511020	Temporary Employee Wages	(20,000)	(15,321)	4,679	77%
512005	Health & Dental Benefits	(406,282)	(199,521)	206,761	49%
512008	Health Reimb Arrangement	(15,917)	(7,598)	8,319	48%
512010	Retirement	(409,480)	(167,190)	242,290	41%
512015	FICA	(147,564)	(66,017)	81,547	45%
512020	Workers Comp	(46,812)	(46,812)	0	100%
512025	Other Benefits	(21,399)	(6,588)	14,811	31%
512030	Other Payroll Taxes	(15,234)	(6,861)	8,373	45%
512035	Volunteer Fringe Benefits	(3,064)	0	3,064	0%
520110	Operating Supplies	(18,000)	(11,087)	6,913	62%
520120	Organization Business Expense	(4,300)	(2,359)	1,941	55%
520130	Personnel Uniforms & Equipment	(15,000)	(9,105)	5,895	61%
520150	Utilities	(28,881)	(6,561)	22,320	23%
520190	Computer Software	(3,500)	0	3,500	0%
520220	Small Equipment	(10,000)	(5,478)	4,522	55%
520230	Tools- 50/50	(182,900)	(88,856)	94,044	49%
520300	Emergency Operations Center	(3,500)	(719)	2,781	21%
520503	Printing	(500)	0	500	0%
520506	Postage	(700)	(110)	590	16%
520509	Telephone	(5,000)	(3,361)	1,639	67%
520521	Public Information	(1,800)	(175)	1,625	10%
520524	Publications	(1,600)	(1,465)	135	92%
520530	Memberships	(1,184)	(1,140)	44	96%
520557	Intergovernmental Services	(1,400)	(1,082)	318	77%
520558	WCCCA	(164,469)	(122,591)	41,878	75%
520578	Insurance & Bonds	(37,626)	(37,626)	0	100%
521003	Training/Conferences	(27,000)	(10,914)	16,086	40%
521150	Professional Services	(29,040)	(12,938)	16,102	45%
521165	Contracts for Services	0	(16,000)	(16,000)	0%
521168	Misc Medical Services	(29,000)	(1,765)	27,235	6%
522003	Equipment Maint & Oper Supplies	(9,000)	(6,230)	2,770	69%
522009	Vehicle Maint & Oper Supplies	(55,000)	(41,228)	13,772	75%
522012	Fuel/Oil	(32,000)	(11,561)	20,439	36%
522021	Equipment Fund Charges	(3,658)	(1,828)	1,830	50%
522022	Information Systems Fund Charge	(28,575)	(14,288)	14,287	50%
522303	Custodial	(1,500)	0	1,500	0%
522312	Facility Maintenance Supplies	(3,500)	(1,196)	2,304	34%

100 - General Fund

Department #22 - Fire

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
522315	Facility Mnt/Repairs	(12,000)	(6,464)	5,536	54%
	EXPENDITURES TOTAL	(3,706,213)	(1,823,397)	1,882,816	49%
	Department Net Totals	(3,026,536)	(1,695,005)		

100 - General Fund

Department #31 - Planning

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
420070	Payment from FG Urban Renewal	5,000	0	(5,000)	0%
430460	SHPO Grant	13,000	0	(13,000)	0%
430587	Grant Match - Other Agencies	0	8,650	8,650	0%
440020	Code Enforcement Revenue	1,800	1,545	(255)	86%
440025	Copy Service	100	7	(93)	7%
440029	General Fund Spt Svc (522023)	77,921	77,921	0	100%
450054	Metro Business License	12,000	22,587	10,587	188%
450101	Planning Fees	37,500	34,219	(3,281)	91%
	REVENUES TOTAL	147,321	144,929	(2,392)	98%
EXPENDITURES					
511005	Regular Employee Wages	(240,869)	(106,892)	133,977	44%
511010	Part-Time Employee Wages	(4,541)	(2,027)	2,514	45%
512005	Health & Dental Benefits	(40,924)	(17,584)	23,340	43%
512008	Health Reimb Arrangement	(2,129)	(1,064)	1,065	50%
512010	Retirement	(57,042)	(26,171)	30,871	46%
512015	FICA	(18,774)	(8,183)	10,591	44%
512020	Workers Comp	(205)	(205)	0	100%
512025	Other Benefits	(2,396)	(733)	1,663	31%
512030	Other Payroll Taxes	(1,864)	(829)	1,035	44%
520110	Operating Supplies	(1,912)	(458)	1,454	24%
520120	Organization Business Expense	(2,000)	(88)	1,912	4%
520170	Code Enforcement Expenditures	(4,500)	(1,240)	3,261	28%
520190	Computer Software	(3,150)	0	3,150	0%
520200	Computer Software Maintenance	0	(124)	(124)	0%
520220	Small Equipment	(900)	0	900	0%
520503	Printing	(2,100)	(66)	2,034	3%
520506	Postage	(2,400)	(1,736)	664	72%
520509	Telephone	(800)	(277)	523	35%
520521	Public Information	(1,800)	(698)	1,102	39%
520524	Publications	(800)	0	800	0%
520530	Memberships	(2,700)	(1,545)	1,155	57%
520557	Intergovernmental Services	(1,200)	(330)	870	28%
520578	Insurance & Bonds	(1,098)	(1,098)	0	100%
521003	Training/Conferences	(2,300)	(760)	1,540	33%
521113	Attorney Services	(10,000)	(11,193)	(1,193)	112%
521150	Professional Services	(22,000)	(20,867)	1,133	95%
521172	Bank Service Fees	0	(14)	(14)	0%
522003	Equipment Maint & Oper Supplies	(4,140)	(1,573)	2,567	38%
522021	Equipment Fund Charges	(1,591)	(793)	798	50%
522022	Information Systems Fund Charge	(17,525)	(8,763)	8,762	50%
522309	Building/Facility Rental	(1,573)	(787)	786	50%
523006	SHPO Grant Expense	(3,000)	0	3,000	0%
	EXPENDITURES TOTAL	(456,233)	(216,098)	240,135	47%
	Department Net Totals	(308,912)	(71,169)		

100 - General Fund

Department #51 - Engineering

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440025	Copy Service	1,400	504	(896)	36%
440029	General Fund Spt Svc (522023)	728,946	728,946	0	100%
450122	Engineering Inspection Fees	40,000	2,743	(37,257)	7%
	REVENUES TOTAL	770,346	732,193	(38,153)	95%
EXPENDITURES					
511005	Regular Employee Wages	(431,993)	(196,983)	235,010	46%
511010	Part-Time Employee Wages	(45,879)	(19,311)	26,568	42%
512005	Health & Dental Benefits	(96,664)	(43,560)	53,104	45%
512008	Health Reimb Arrangement	(4,290)	(2,014)	2,276	47%
512010	Retirement	(98,103)	(44,184)	53,919	45%
512015	FICA	(36,557)	(16,356)	20,201	45%
512020	Workers Comp	(4,034)	(4,034)	0	100%
512025	Other Benefits	(5,460)	(2,572)	2,888	47%
512030	Other Payroll Taxes	(3,643)	(1,651)	1,992	45%
520110	Operating Supplies	(3,250)	(1,057)	2,193	33%
520120	Organization Business Expense	(250)	(194)	56	77%
520130	Personnel Uniforms & Equipment	(500)	0	500	0%
520190	Computer Software	(750)	0	750	0%
520200	Computer Software Maintenance	(3,000)	(3,596)	(596)	120%
520220	Small Equipment	(3,300)	(138)	3,162	4%
520503	Printing	(2,500)	(175)	2,326	7%
520506	Postage	(550)	(141)	409	26%
520509	Telephone	(2,350)	(416)	1,934	18%
520521	Public Information	(500)	0	500	0%
520524	Publications	(1,300)	(139)	1,162	11%
520530	Memberships	(2,530)	(820)	1,710	32%
520557	Intergovernmental Services	(650)	(204)	446	31%
520578	Insurance & Bonds	(2,775)	(2,775)	0	100%
521003	Training/Conferences	(6,050)	(145)	5,905	2%
521113	Attorney Services	(3,000)	0	3,000	0%
521150	Professional Services	(10,520)	(2,461)	8,059	23%
522003	Equipment Maint & Oper Supplies	(3,440)	(549)	2,891	16%
522021	Equipment Fund Charges	(15,685)	(7,843)	7,842	50%
522022	Information Systems Fund Charge	(46,442)	(23,221)	23,221	50%
522306	Rents & Leases	(9,624)	0	9,624	0%
522312	Facility Maintenance Supplies	0	(376)	(376)	0%
522315	Facility Mnt/Repairs	0	(443)	(443)	0%
	EXPENDITURES TOTAL	(845,589)	(375,356)	470,233	44%
	Department Net Totals	(75,243)	356,838		

100 - General Fund

Department #51 - Engineering

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
	Fund Total Revenues	22,202,301	19,879,560		
	Fund Total Expenditures	(22,202,301)	(7,599,242)		
	Fund Net	0	12,280,318		

100 - General Fund

Department #51 - Engineering

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
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205 - Building Permits Fund

Department #32 - Building Services

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440025	Copy Service	0	236	236	0%
450074	Structural Building Permits	177,675	181,992	4,317	102%
450076	Manufactured Home Permits	329	210	(119)	64%
450078	Plumbing Permits	46,394	42,153	(4,241)	91%
450080	Mechanical Permits	14,320	17,474	3,154	122%
450082	Structural State Surcharge	26,143	21,642	(4,501)	83%
450084	Manuf. Housing State Surcharge	39	22	(17)	55%
450086	Plumbing State Surcharge	6,663	4,987	(1,676)	75%
450088	Mechanical Plans St. Surcharge	1,718	2,076	358	121%
450106	Structural Plan Review Fees	102,004	116,335	14,331	114%
450108	Plumbing Plan Review Fees	572	1,883	1,311	329%
450110	Mechanical Plan Review Fees	2,605	2,859	254	110%
450112	FL&S Plan Review Fees	12,971	9,217	(3,754)	71%
450124	Erosion Control Fees	22,000	20,330	(1,670)	92%
450126	Misc - Reinspect & Invstg Fee	600	37	(563)	6%
450130	Cornelius Permit Revenue	40,000	0	(40,000)	0%
470105	Interest	4,500	3,109	(1,391)	69%
495005	Fund Bal Avail. for Approp.	1,366,408	1,402,136	35,728	103%
	REVENUES TOTAL	1,824,941	1,826,696	1,755	100%
EXPENDITURES					
511005	Regular Employee Wages	(321,284)	(139,238)	182,046	43%
511010	Part-Time Employee Wages	(9,771)	(4,055)	5,716	41%
512005	Health & Dental Benefits	(61,689)	(27,661)	34,028	45%
512008	Health Reimb Arrangement	(3,441)	(1,527)	1,914	44%
512010	Retirement	(62,228)	(27,033)	35,195	43%
512015	FICA	(25,326)	(10,840)	14,486	43%
512020	Workers Comp	(2,056)	(2,056)	0	100%
512025	Other Benefits	(3,363)	(1,027)	2,336	31%
512030	Other Payroll Taxes	(6,489)	(1,101)	5,388	17%
520110	Operating Supplies	(975)	(433)	542	44%
520120	Organization Business Expense	(400)	0	400	0%
520130	Personnel Uniforms & Equipment	(500)	(514)	(14)	103%
520200	Computer Software Maintenance	0	(187)	(187)	0%
520220	Small Equipment	(5,130)	0	5,130	0%
520503	Printing	(1,500)	(197)	1,303	13%
520506	Postage	(420)	(227)	193	54%
520509	Telephone	(1,770)	(1,067)	703	60%
520521	Public Information	(150)	(242)	(92)	161%
520530	Memberships	(950)	(350)	600	37%
520557	Intergovernmental Services	(44,564)	(25,462)	19,102	57%
520578	Insurance & Bonds	(1,394)	(1,394)	0	100%
521003	Training/Conferences	(4,250)	(1,508)	2,742	35%
521113	Attorney Services	(3,000)	0	3,000	0%
521150	Professional Services	(25,500)	(5,280)	20,220	21%
521172	Bank Service Fees	(150)	(1,216)	(1,066)	811%
522003	Equipment Maint & Oper Supplies	(1,100)	0	1,100	0%
522021	Equipment Fund Charges	(10,655)	(5,327)	5,328	50%
522022	Information Systems Fund Charge	(14,027)	(7,014)	7,013	50%
522023	General Fund Admin Services	(55,130)	(55,130)	0	100%
570103	Transfer to General Fund	(33,663)	(33,663)	0	100%

205 - Building Permits Fund

Department #32 - Building Services

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
580206	Contingency	(250,000)	0	250,000	0%
590304	Unapp Fund Balance	(874,066)	0	874,066	0%
	EXPENDITURES TOTAL	(1,824,941)	(353,749)	1,471,192	19%
	Department Net Totals	0	1,472,948		
	Fund Total Revenues	1,824,941	1,826,696		
	Fund Total Expenditures	(1,824,941)	(353,749)		
	Fund Net	0	1,472,948		

210 - Street Fund

Department #52 - Streets & Traffic

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
422015	State Gas Tax	1,296,653	676,050	(620,603)	52%
422025	County Gas Tax	84,000	43,897	(40,103)	52%
440172	Streetlight Fee	146,400	72,585	(73,815)	50%
450057	Other	0	1,442	1,442	0%
450100	Street Improv Fee-N Central Proje	0	12,250	12,250	0%
450114	Street Improv Fee-Pac Cross-SG	0	1,440	1,440	0%
470105	Interest	3,500	980	(2,520)	28%
470131	Sidewalk Program Interest	0	359	359	0%
470132	Sidewalk Program Principal	23,100	5,037	(18,063)	22%
470133	Sidewalk Program Pymt in Full	100,000	0	(100,000)	0%
495005	Fund Bal Avail. for Approp.	845,581	1,043,656	198,075	123%
495006	Restricted Fund Balance	9,125	9,125	0	100%
	REVENUES TOTAL	2,508,359	1,866,819	(641,540)	74%
EXPENDITURES					
511005	Regular Employee Wages	(186,575)	(96,775)	89,800	52%
511015	Overtime	(7,094)	(784)	6,310	11%
512005	Health & Dental Benefits	(59,853)	(28,838)	31,015	48%
512008	Health Reimb Arrangement	(2,175)	(1,096)	1,079	50%
512010	Retirement	(40,206)	(21,438)	18,768	53%
512015	FICA	(14,816)	(7,458)	7,358	50%
512020	Workers Comp	(4,677)	(4,677)	0	100%
512025	Other Benefits	(1,871)	(624)	1,247	33%
512030	Other Payroll Taxes	(1,502)	(666)	836	44%
520110	Operating Supplies	(2,000)	(648)	1,352	32%
520120	Organization Business Expense	(150)	0	150	0%
520130	Personnel Uniforms & Equipment	(3,000)	(1,547)	1,453	52%
520150	Utilities	(70,000)	(40,411)	29,589	58%
520190	Computer Software	(3,000)	(802)	2,198	27%
520220	Small Equipment	(3,500)	(518)	2,982	15%
520240	Construction Supplies	(68,000)	(31,414)	36,586	46%
520280	Street Light Maint. (610-440170)	(88,350)	0	88,350	0%
520503	Printing	(400)	(227)	173	57%
520506	Postage	(200)	(49)	151	25%
520509	Telephone	(1,100)	(238)	862	22%
520521	Public Information	(300)	0	300	0%
520524	Publications	(300)	0	300	0%
520530	Memberships	(100)	0	100	0%
520557	Intergovernmental Services	(22,000)	(9,986)	12,014	45%
520578	Insurance & Bonds	(3,699)	(3,699)	0	100%
521003	Training/Conferences	(2,000)	(879)	1,121	44%
521150	Professional Services	(83,900)	(300)	83,600	0%
521168	Misc Medical Services	(300)	(116)	184	39%
521172	Bank Service Fees	(700)	(447)	254	64%
522003	Equipment Maint & Oper Supplies	(1,700)	(1,911)	(211)	112%
522012	Fuel/Oil	(500)	(255)	245	51%
522021	Equipment Fund Charges	(61,101)	(30,549)	30,552	50%
522022	Information Systems Fund Charge	(3,618)	(1,809)	1,809	50%
522023	General Fund Admin Services	(515,365)	(515,365)	0	100%
522306	Rents & Leases	(10,000)	(4,500)	5,500	45%
522309	Building/Facility Rental	(3,150)	(1,572)	1,578	50%

210 - Street Fund

Department #52 - Streets & Traffic

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
522315	Facility Mnt/Repairs	(500)	0	500	0%
550181	Major Tools & Work Equipment	(10,000)	(4,309)	5,692	43%
550563	Construction Maintenance	(385,000)	(261,191)	123,809	68%
550575	Street Construction	(198,314)	(10,205)	188,109	5%
550580	Sidewalk Program	(100,000)	(82)	99,918	0%
551066	Safe Routes to School	(65,000)	2,073	67,073	-3%
580206	Contingency	(100,000)	0	100,000	0%
590304	Unapp Fund Balance	(382,343)	0	382,343	0%
EXPENDITURES TOTAL		(2,508,359)	(1,083,311)	1,425,048	43%
Department Net Totals		0	783,508		
Fund Total Revenues		2,508,359	1,866,819		
Fund Total Expenditures		(2,508,359)	(1,083,311)		
Fund Net		0	783,508		

212 - Street Tree Fund

Department #31 - Planning

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
450140	Tree Planting Fees	40,000	10,461	(29,539)	26%
470105	Interest	30	10	(20)	33%
472025	Tree Compensation Revenue	0	13	13	0%
495005	Fund Bal Avail. for Approp.	8,550	9,967	1,417	117%
	REVENUES TOTAL	48,580	20,450	(28,130)	42%
EXPENDITURES					
520290	Street Tree Planting	(47,380)	(705)	46,676	1%
570103	Transfer to General Fund	(1,200)	(1,200)	0	100%
	EXPENDITURES TOTAL	(48,580)	(1,905)	46,676	4%
	Department Net Totals	0	18,546		
	Fund Total Revenues	48,580	20,450		
	Fund Total Expenditures	(48,580)	(1,905)		
	Fund Net	0	18,546		

220 - Forfeiture Sharing Fund

Department #21 - Police

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
460150	Forfeiture Sharing Revenue	0	3,341	3,341	0%
	REVENUES TOTAL	0	3,341	3,341	0%
	Department Net Totals	0	3,341		
	Fund Total Revenues	0	3,341		
	Fund Total Expenditures				
	Fund Net	0	3,341		

225 - Fire Equipment Repl. Fund
Department #23 - Fire Equipment Repl Fund
 For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
420010	Rural District Share	177,500	34,592	(142,908)	19%
445015	Sale of Equipment	12,500	12,500	0	100%
470105	Interest	2,000	481	(1,519)	24%
481005	Transfer from Other Funds	167,000	83,500	(83,500)	50%
495005	Fund Bal Avail. for Approp.	472,388	431,175	(41,213)	91%
	REVENUES TOTAL	831,388	562,248	(269,140)	68%
EXPENDITURES					
550166	Building Improvements	(160,000)	(151,277)	8,723	95%
550181	Major Tools & Work Equipment	(65,000)	776	65,776	-1%
551261	Equipment Replacement	(130,000)	(40,348)	89,653	31%
580206	Contingency	(100,000)	0	100,000	0%
590304	Unapp Fund Balance	(376,388)	0	376,388	0%
	EXPENDITURES TOTAL	(831,388)	(190,849)	640,539	23%
	Department Net Totals	0	371,400		
	Fund Total Revenues	831,388	562,248		
	Fund Total Expenditures	(831,388)	(190,849)		
	Fund Net	0	371,400		

226 - Fire SAFER Grant Fund

Department #22 - Fire

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
430214	Homeland Security Grant	94,993	16,371	(78,622)	17%
495005	Fund Bal Avail. for Approp.	0	1,136	1,136	0%
	REVENUES TOTAL	94,993	17,507	(77,486)	18%
EXPENDITURES					
511005	Regular Employee Wages	(54,739)	(27,446)	27,293	50%
512005	Health & Dental Benefits	(13,176)	(10,466)	2,710	79%
512008	Health Reimb Arrangement	(489)	0	489	0%
512010	Retirement	(13,302)	(9,178)	4,124	69%
512015	Fica	(4,188)	(3,086)	1,102	74%
512020	Workers Comp	(1,138)	(1,138)	0	100%
512025	Other Benefits	(396)	(350)	46	88%
512030	Other Payroll Taxes	(734)	(323)	411	44%
520110	Operating Supplies	(6,831)	(99)	6,732	1%
	EXPENDITURES TOTAL	(94,993)	(52,085)	42,908	55%
	Department Net Totals	0	(34,578)		
	Fund Total Revenues	94,993	17,507		
	Fund Total Expenditures	(94,993)	(52,085)		
	Fund Net	0	(34,578)		

240 - Bikeway Development Fund

Department #52 - Streets & Traffic

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
422015	State Gas Tax	13,100	6,829	(6,271)	52%
470105	Interest	60	0	(60)	0%
495005	Fund Bal Avail. for Approp.	20,939	21,124	185	101%
	REVENUES TOTAL	34,099	27,953	(6,146)	82%
EXPENDITURES					
550169	General Capital Outlay	(34,099)	(1,659)	32,440	5%
	EXPENDITURES TOTAL	(34,099)	(1,659)	32,440	5%
	Department Net Totals	0	26,294		
	Fund Total Revenues	34,099	27,953		
	Fund Total Expenditures	(34,099)	(1,659)		
	Fund Net	0	26,294		

265 - Transportation Services Fund
Department #12 - Administration Services
 For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
430725	DHS Transportation Grant	500,000	174,193	(325,807)	35%
	REVENUES TOTAL	500,000	174,193	(325,807)	35%
EXPENDITURES					
523015	Transportation Grant	(500,000)	(136,587)	363,413	27%
	EXPENDITURES TOTAL	(500,000)	(136,587)	363,413	27%
	Department Net Totals	0	37,606		
	Fund Total Revenues	500,000	174,193		
	Fund Total Expenditures	(500,000)	(136,587)		
	Fund Net	0	37,606		

270 - Facility Major Maintenance Fund
Department #12 - Administration Services
 For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
470105	Interest	3,200	1,856	(1,344)	58%
472005	Miscellaneous	0	7,170	7,170	0%
481007	Transfer from General Fund	450,000	0	(450,000)	0%
495005	Fund Bal Avail. for Approp.	808,209	786,447	(21,762)	97%
	REVENUES TOTAL	1,261,409	795,473	(465,936)	63%
EXPENDITURES					
550166	Building Improvements	(511,409)	(8,799)	502,610	2%
550170	Capital Projects - Support Service	0	(56,933)	(56,933)	0%
550173	Capital Projects - Aquatic Center	0	(85,341)	(85,341)	0%
590304	Unapp Fund Balance	(750,000)	0	750,000	0%
	EXPENDITURES TOTAL	(1,261,409)	(151,073)	1,110,336	12%
	Department Net Totals	0	644,400		
	Fund Total Revenues	1,261,409	795,473		
	Fund Total Expenditures	(1,261,409)	(151,073)		
	Fund Net	0	644,400		

275 - Community Enhancement Fund
Department #12 - Administration Services
 For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
420025	Metro Enhancement Fee	104,741	44,593	(60,148)	43%
431001	Repayment of Grants	0	500	500	0%
495005	Fund Bal Avail. for Approp.	13,589	21,014	7,425	155%
	REVENUES TOTAL	118,330	66,107	(52,223)	56%
EXPENDITURES					
523003	Comm Enhancement Grants	(100,000)	(55,308)	44,692	55%
523004	CEP Carryover	(7,434)	(80)	7,354	1%
523005	Funding Not Allocated	(10,396)	0	10,396	0%
570103	Transfer to General Fund	(500)	0	500	0%
	EXPENDITURES TOTAL	(118,330)	(55,388)	62,942	47%
	Department Net Totals	0	10,719		
	Fund Total Revenues	118,330	66,107		
	Fund Total Expenditures	(118,330)	(55,388)		
	Fund Net	0	10,719		

280 - Public Arts Fund

Department #11 - Legislative & Executive

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
470105	Interest	90	21	(69)	23%
471025	Public Arts Comm. Donations	2,000	1,295	(705)	65%
495005	Fund Bal Avail. for Approp.	10,625	8,908	(1,717)	84%
	REVENUES TOTAL	12,715	10,223	(2,492)	80%
EXPENDITURES					
520564	Public Arts Comm. Expenditures	(12,715)	(1,289)	11,426	10%
	EXPENDITURES TOTAL	(12,715)	(1,289)	11,426	10%
	Department Net Totals	0	8,934		
	Fund Total Revenues	12,715	10,223		
	Fund Total Expenditures	(12,715)	(1,289)		
	Fund Net	0	8,934		

310 - Traffic Impact Fund

Department #52 - Streets & Traffic

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
470105	Interest	15,000	7,654	(7,346)	51%
495005	Fund Bal Avail. for Approp.	3,151,107	3,149,983	(1,124)	100%
	REVENUES TOTAL	3,166,107	3,157,637	(8,470)	100%
EXPENDITURES					
550169	General Capital Outlay	(3,166,107)	0	3,166,107	0%
	EXPENDITURES TOTAL	(3,166,107)	0	3,166,107	0%
	Department Net Totals	0	3,157,637		
	Fund Total Revenues	3,166,107	3,157,637		
	Fund Total Expenditures	(3,166,107)	0		
	Fund Net	0	3,157,637		

311 - Transportation Development Tax Fund

Department #52 - Streets & Traffic

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
451070	Transportation Development Tax	675,000	582,112	(92,888)	86%
470105	Interest	16,500	12,118	(4,382)	73%
495005	Fund Bal Avail. for Approp.	4,207,857	4,399,571	191,714	105%
	REVENUES TOTAL	4,899,357	4,993,801	94,444	102%
EXPENDITURES					
550169	General Capital Outlay	(4,899,357)	0	4,899,357	0%
	EXPENDITURES TOTAL	(4,899,357)	0	4,899,357	0%
	Department Net Totals	0	4,993,801		
	Fund Total Revenues	4,899,357	4,993,801		
	Fund Total Expenditures	(4,899,357)	0		
	Fund Net	0	4,993,801		

320 - Park Acq. & Dev Fund

Department #16 - Parks & Recreation

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
430708	State Parks Grant	250,000	0	(250,000)	0%
451009	Parks SDC	270,000	231,000	(39,000)	86%
470105	Interest	9,000	3,649	(5,351)	41%
495005	Fund Bal Avail. for Approp.	1,370,116	1,458,090	87,974	106%
	REVENUES TOTAL	1,899,116	1,692,739	(206,377)	89%
EXPENDITURES					
521150	Professional Services	(250,000)	(32,613)	217,387	13%
550200	Parks Projects - General	(1,329,116)	0	1,329,116	0%
550248	Trails and Greenways	(320,000)	(58,212)	261,788	18%
	EXPENDITURES TOTAL	(1,899,116)	(90,825)	1,808,291	5%
	Department Net Totals	0	1,601,914		
	Fund Total Revenues	1,899,116	1,692,739		
	Fund Total Expenditures	(1,899,116)	(90,825)		
	Fund Net	0	1,601,914		

330 - Capital Projects Fund

Department #12 - Administration Services

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
470105	Interest	8,500	4,468	(4,032)	53%
481005	Transfer from Other Funds	261,803	261,803	0	100%
495005	Fund Bal Avail. for Approp.	1,578,378	1,573,531	(4,847)	100%
	REVENUES TOTAL	1,848,681	1,839,802	(8,879)	100%
EXPENDITURES					
550169	General Capital Outlay	(1,491,481)	0	1,491,481	0%
550178	Main Street Sidewalks	0	(4,476)	(4,476)	0%
550188	Times Litho Site	(357,200)	(390,441)	(33,241)	109%
	EXPENDITURES TOTAL	(1,848,681)	(394,917)	1,453,764	21%
	Department Net Totals	0	1,444,885		
	Fund Total Revenues	1,848,681	1,839,802		
	Fund Total Expenditures	(1,848,681)	(394,917)		
	Fund Net	0	1,444,885		

350 - CIP Excise Tax Fund

Department #12 - Administration Services

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
450200	C.I.P. Fees	330,000	165,227	(164,773)	50%
470105	Interest	475	94	(381)	20%
495005	Fund Bal Avail. for Approp.	131,253	141,671	10,418	108%
	REVENUES TOTAL	461,728	306,993	(154,735)	66%
EXPENDITURES					
550170	Capital Projects - Support Service	(99,198)	(9,256)	89,942	9%
550171	Capital Projects - Public Safety	(67,350)	0	67,350	0%
570127	Transfer to Other Funds	(295,180)	(161,645)	133,536	55%
	EXPENDITURES TOTAL	(461,728)	(170,900)	290,828	37%
	Department Net Totals	0	136,092		
	Fund Total Revenues	461,728	306,993		
	Fund Total Expenditures	(461,728)	(170,900)		
	Fund Net	0	136,092		

410 - General Debt Service Fund

Department #19 -

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
411003	Property Taxes	468,528	441,456	(27,072)	94%
411150	Property Tax Prior Years	9,500	6,643	(2,857)	70%
470105	Interest	500	613	113	123%
495005	Fund Bal Avail. for Approp.	44,672	47,171	2,499	106%
	REVENUES TOTAL	523,200	495,884	(27,316)	95%
EXPENDITURES					
562008	Principal - 2010 Series	(505,000)	0	505,000	0%
562048	Interest - 2010 Series	(18,200)	(9,100)	9,100	50%
	EXPENDITURES TOTAL	(523,200)	(9,100)	514,100	2%
	Department Net Totals	0	486,784		
	Fund Total Revenues	523,200	495,884		
	Fund Total Expenditures	(523,200)	(9,100)		
	Fund Net	0	486,784		

430 - SPWF-Debt Service Fund

Department #19 -

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
451200	Assessment Charges	5,507	10,377	4,870	188%
470130	Interest - Assessments	1,509	2,512	1,003	166%
481007	Transfer from General Fund	7,500	961	(6,539)	13%
495005	Fund Bal Avail. for Approp.	11,118	11,118	0	100%
	REVENUES TOTAL	25,634	24,968	(666)	97%
EXPENDITURES					
562020	Principal - 95 S.P.W.F. Loan	(23,610)	(23,610)	0	100%
562055	Interest - 95 S.P.W.F. Loan	(1,358)	(1,358)	0	100%
590304	Unapp Fund Balance	(666)	0	666	0%
	EXPENDITURES TOTAL	(25,634)	(24,968)	666	97%
	Department Net Totals	0	0		
	Fund Total Revenues	25,634	24,968		
	Fund Total Expenditures	(25,634)	(24,968)		
	Fund Net	0	0		

505 - Library Endowment Fund

Department #14 - Library

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
470105	Interest	225	112	(113)	50%
495005	Fund Bal Avail. for Approp.	486	490	4	101%
495006	Restricted Fund Balance	41,187	41,186	(1)	100%
	REVENUES TOTAL	41,898	41,788	(110)	100%
EXPENDITURES					
520220	Small Equipment	(711)	0	711	0%
590304	Unapp Fund Balance	(41,187)	0	41,187	0%
	EXPENDITURES TOTAL	(41,898)	0	41,898	0%
	Department Net Totals	0	41,788		
	Fund Total Revenues	41,898	41,788		
	Fund Total Expenditures	(41,898)	0		
	Fund Net	0	41,788		

622 - Sewer SDC Fund

Department #54 - Sewer

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
430706	CDBG Grant	240,000	0	(240,000)	0%
451005	System Development Charge	555,650	394,853	(160,798)	71%
451042	Sunset Sewer Connection Fee	1,000	0	(1,000)	0%
470105	Interest	5,000	1,650	(3,350)	33%
495005	Fund Bal Avail. for Approp.	1,179,962	1,208,508	28,546	102%
	REVENUES TOTAL	1,981,612	1,605,010	(376,602)	81%
EXPENDITURES					
520557	Intergovernmental Services	(444,520)	(315,882)	128,638	71%
550760	Construction Projects	(836,000)	(637)	835,363	0%
562002	Principal-CWS/City Sunset Drive	(108,912)	(53,873)	55,039	49%
562042	Interest - CWS/City Sunset Drive	(13,082)	(7,125)	5,957	54%
570127	Transfer to Other Funds	(261,803)	(261,803)	0	100%
580206	Contingency	(100,000)	0	100,000	0%
590304	Unapp Fund Balance	(217,295)	0	217,295	0%
	EXPENDITURES TOTAL	(1,981,612)	(639,320)	1,342,292	32%
	Department Net Totals	0	965,691		
	Fund Total Revenues	1,981,612	1,605,010		
	Fund Total Expenditures	(1,981,612)	(639,320)		
	Fund Net	0	965,691		

630 - Water Fund

Department #53 - Water

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440105	Residential Sales	2,530,488	1,387,980	(1,142,508)	55%
440106	Commercial Sales	797,582	472,734	(324,848)	59%
440118	Multi-Family Sales	379,455	200,936	(178,519)	53%
440120	Industrial Service - Special Contr	309,384	155,918	(153,466)	50%
440160	Non-Metered Sales	39,669	4,404	(35,265)	11%
440165	Connection Charges	46,305	26,266	(20,039)	57%
440190	External Work Performed	0	516	516	0%
440305	Building Rental Income	48,862	18,020	(30,842)	37%
445025	Timber Sales - Even Fiscal Years	1,245,000	711,587	(533,413)	57%
450057	Other	5,000	7,400	2,400	148%
470105	Interest	18,500	10,977	(7,524)	59%
495005	Fund Bal Avail. for Approp.	4,508,794	4,978,125	469,331	110%
	REVENUES TOTAL	9,929,039	7,974,861	(1,954,178)	80%
EXPENDITURES					
511005	Regular Employee Wages	(555,122)	(241,092)	314,030	43%
511015	Overtime	(29,560)	(13,236)	16,324	45%
512005	Health & Dental Benefits	(165,193)	(78,011)	87,182	47%
512008	Health Reimb Arrangement	(6,352)	(3,022)	3,330	48%
512010	Retirement	(115,817)	(49,158)	66,659	42%
512015	FICA	(44,728)	(19,405)	25,323	43%
512020	Workers Comp	(13,101)	(13,101)	0	100%
512025	Other Benefits	(5,645)	(1,964)	3,681	35%
512030	Other Payroll Taxes	(4,536)	(2,010)	2,526	44%
520110	Operating Supplies	(105,850)	(9,096)	96,754	9%
520120	Organization Business Expense	(250)	(352)	(102)	141%
520130	Personnel Uniforms & Equipment	(8,200)	(3,929)	4,271	48%
520150	Utilities	(87,415)	(34,171)	53,244	39%
520190	Computer Software	(4,400)	(1,067)	3,333	24%
520220	Small Equipment	(19,200)	(10,660)	8,540	56%
520240	Construction Supplies	(86,700)	(53,517)	33,183	62%
520250	JWC Water Purchases	(250,000)	(125,961)	124,039	50%
520503	Printing	(2,800)	(1,182)	1,618	42%
520506	Postage	(1,100)	(770)	330	70%
520509	Telephone	(5,186)	(2,427)	2,759	47%
520521	Public Information	(250)	(181)	69	73%
520524	Publications	(300)	0	300	0%
520530	Memberships	(4,220)	(620)	3,600	15%
520533	Recruiting Expenses	0	(1,000)	(1,000)	0%
520548	Watershed Management	(26,300)	(5,549)	20,751	21%
520550	Watershed Maintenance	(20,000)	(7,340)	12,660	37%
520551	Timber Harvesting - Odd Fiscal Ye	(500,000)	(250,214)	249,786	50%
520557	Intergovernmental Services	(82,307)	(26,803)	55,504	33%
520578	Insurance & Bonds	(28,361)	(28,361)	0	100%
521003	Training/Conferences	(8,700)	(150)	8,550	2%
521113	Attorney Services	(6,500)	(478)	6,023	7%
521150	Professional Services	(249,100)	(16,215)	232,885	7%
521165	Contracts for Services	(20,000)	0	20,000	0%
521168	Misc Medical Services	0	(667)	(667)	0%
521172	Bank Service Fees	(18,000)	(8,881)	9,119	49%
522003	Equipment Maint & Oper Supplies	(7,600)	(2,624)	4,976	35%

630 - Water Fund

Department #53 - Water

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
522012	Fuel/Oil	(200)	(89)	111	44%
522021	Equipment Fund Charges	(157,881)	(78,939)	78,942	50%
522022	Information Systems Fund Charge	(14,699)	(7,350)	7,349	50%
522023	General Fund Admin Services	(906,075)	(906,075)	0	100%
522303	Custodial	(800)	0	800	0%
522306	Rents & Leases	(3,000)	0	3,000	0%
522312	Facility Maintenance Supplies	(14,700)	(8,502)	6,198	58%
522315	Facility Mnt/Repairs	(27,000)	(4,629)	22,371	17%
550181	Major Tools & Work Equipment	(415,000)	(208,256)	206,744	50%
550660	Joint - Capital Equipment	(142,941)	0	142,941	0%
550663	JWC - Other Projects	0	(65,057)	(65,057)	0%
550666	Water Line Extension	(100,000)	0	100,000	0%
550669	Water Treatment Plant Equip	(219,000)	(120,607)	98,393	55%
550672	Watershed Capital	(130,000)	0	130,000	0%
550760	Construction Projects	0	(22,398)	(22,398)	0%
562011	2013 Refunding FFCO - Principal	(290,000)	0	290,000	0%
562030	Principal - Scoggins Reservoir	(35,348)	(34,154)	1,194	97%
562046	2013 Refunding FFCO - Interest	(70,281)	(35,141)	35,141	50%
562070	Interest - Scoggins Reservoir	(37,449)	(38,643)	(1,194)	103%
570127	Transfer to Other Funds	(58,199)	(52,736)	5,463	91%
570130	In-Lieu of Tax	(221,091)	(221,091)	0	100%
580203	JWC - Contingency	(333,250)	0	333,250	0%
580206	Contingency	(450,000)	0	450,000	0%
580212	Debt Service Contingency	(440,000)	0	440,000	0%
590304	Unapp Fund Balance	(3,379,332)	0	3,379,332	0%
EXPENDITURES TOTAL		(9,929,039)	(2,816,878)	7,112,161	28%
Department Net Totals		0	5,157,983		
Fund Total Revenues		9,929,039	7,974,861		
Fund Total Expenditures		(9,929,039)	(2,816,878)		
Fund Net		0	5,157,983		

632 - Water SDC Fund

Department #53 - Water

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
451005	System Development Charge	395,000	342,165	(52,835)	87%
470105	Interest	17,000	10,593	(6,407)	62%
495005	Fund Bal Avail. for Approp.	3,911,523	4,002,253	90,730	102%
	REVENUES TOTAL	4,323,523	4,355,011	31,488	101%
EXPENDITURES					
550760	Construction Projects	(388,000)	0	388,000	0%
570103	Transfer to General Fund	(1,200)	(1,200)	0	100%
580206	Contingency	(100,000)	0	100,000	0%
590304	Unapp Fund Balance	(3,834,323)	0	3,834,323	0%
	EXPENDITURES TOTAL	(4,323,523)	(1,200)	4,322,323	0%
	Department Net Totals	0	4,353,811		
	Fund Total Revenues	4,323,523	4,355,011		
	Fund Total Expenditures	(4,323,523)	(1,200)		
	Fund Net	0	4,353,811		

640 - Surface Water Management Fund

Department #55 - Surf Water Mgmt

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440101	Surface Water Management Fees	1,246,000	554,071	(691,929)	44%
450057	Other	0	1,054	1,054	0%
470105	Interest	1,700	104	(1,596)	6%
495005	Fund Bal Avail. for Approp.	407,617	491,556	83,939	121%
	REVENUES TOTAL	1,655,317	1,046,784	(608,533)	63%
EXPENDITURES					
511005	Regular Employee Wages	(227,370)	(138,669)	88,701	61%
511015	Overtime	0	(1,111)	(1,111)	0%
512005	Health & Dental Benefits	(72,978)	(35,324)	37,654	48%
512008	Health Reimb Arrangement	(2,674)	(1,343)	1,331	50%
512010	Retirement	(49,045)	(29,993)	19,052	61%
512015	FICA	(17,394)	(10,666)	6,728	61%
512020	Workers Comp	(7,744)	(7,744)	0	100%
512025	Other Benefits	(2,151)	(733)	1,418	34%
512030	Other Payroll Taxes	(1,761)	(833)	928	47%
520110	Operating Supplies	(2,000)	(329)	1,671	16%
520120	Organization Business Expense	(150)	0	150	0%
520130	Personnel Uniforms & Equipment	(3,000)	(1,938)	1,062	65%
520190	Computer Software	(3,300)	(802)	2,498	24%
520220	Small Equipment	(6,000)	(3,351)	2,649	56%
520240	Construction Supplies	(12,000)	(1,381)	10,619	12%
520503	Printing	(250)	(62)	188	25%
520506	Postage	(50)	0	50	0%
520509	Telephone	(800)	(84)	716	10%
520521	Public Information	(2,000)	(702)	1,299	35%
520524	Publications	(300)	0	300	0%
520530	Memberships	(200)	0	200	0%
520557	Intergovernmental Services	(274,976)	(112,851)	162,125	41%
520578	Insurance & Bonds	(10,768)	(10,768)	0	100%
521003	Training/Conferences	(2,000)	(270)	1,730	14%
521150	Professional Services	(55,000)	(3,495)	51,505	6%
521168	Misc Medical Services	(500)	(221)	279	44%
521172	Bank Service Fees	(3,800)	(1,683)	2,117	44%
522003	Equipment Maint & Oper Supplies	(2,000)	(72)	1,928	4%
522012	Fuel/Oil	(100)	(78)	22	78%
522021	Equipment Fund Charges	(183,095)	(91,547)	91,548	50%
522022	Information Systems Fund Charge	(1,311)	(656)	655	50%
522023	General Fund Admin Services	(180,468)	(180,468)	0	100%
522306	Rents & Leases	(400)	0	400	0%
522309	Building/Facility Rental	(3,200)	(1,598)	1,602	50%
522312	Facility Maintenance Supplies	(500)	0	500	0%
550863	Storm Sewer Construction	(81,750)	0	81,750	0%
570127	Transfer to Other Funds	(25,000)	0	25,000	0%
570130	In-Lieu of Tax	(35,729)	(35,729)	0	100%
580206	Contingency	(60,000)	0	60,000	0%
590304	Unapp Fund Balance	(323,553)	0	323,553	0%
	EXPENDITURES TOTAL	(1,655,317)	(674,499)	980,818	41%
	Department Net Totals	0	372,285		

640 - Surface Water Management Fund

Department #55 - Surf Water Mgmt

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
	Fund Total Revenues	1,655,317	1,046,784		
	Fund Total Expenditures	(1,655,317)	(674,499)		
	Fund Net	0	372,285		

642 - SWM SDC Fund

Department #55 - Surf Water Mgmt

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
451006	SWM SDC Quality	0	450	450	0%
451007	SWM SDC Quantity	21,000	18,153	(2,847)	86%
470105	Interest	2,000	1,071	(929)	54%
495005	Fund Bal Avail. for Approp.	417,803	424,420	6,617	102%
	REVENUES TOTAL	440,803	444,095	3,292	101%
EXPENDITURES					
550860	Quantity System Improvements	(75,000)	0	75,000	0%
550861	Quality System Improvements	(75,000)	0	75,000	0%
550863	Storm Sewer Construction	(161,750)	(80)	161,670	0%
580207	SWM SDC Cont. Quality	(89,053)	0	89,053	0%
580208	SWM SDC Cont. Quantity	(40,000)	0	40,000	0%
	EXPENDITURES TOTAL	(440,803)	(80)	440,723	0%
	Department Net Totals	0	444,015		
	Fund Total Revenues	440,803	444,095		
	Fund Total Expenditures	(440,803)	(80)		
	Fund Net	0	444,015		

710 - Information Systems Fund
Department #12 - Administration Services
For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440225	Equipment Charges	403,986	201,999	(201,987)	50%
470105	Interest	2,000	860	(1,140)	43%
495005	Fund Bal Avail. for Approp.	450,126	447,137	(2,989)	99%
	REVENUES TOTAL	856,112	649,996	(206,116)	76%
EXPENDITURES					
520110	Operating Supplies	(500)	(1,370)	(870)	274%
520190	Computer Software	(62,900)	(45,945)	16,955	73%
520200	Computer Software Maintenance	(59,921)	(30,749)	29,172	51%
520210	Computer Supplies	(28,000)	(6,536)	21,464	23%
520220	Small Equipment	(46,243)	(18,273)	27,970	40%
520506	Postage	0	(14)	(14)	0%
520509	Telephone	(3,000)	0	3,000	0%
520557	Intergovernmental Services	(9,939)	(11,173)	(1,234)	112%
521150	Professional Services	(70,000)	(9,925)	60,075	14%
522003	Equipment Maint & Oper Supplies	(8,028)	0	8,028	0%
550051	Office Equipment & Furniture	(45,700)	(6,732)	38,968	15%
580206	Contingency	(40,000)	0	40,000	0%
590304	Unapp Fund Balance	(481,881)	0	481,881	0%
	EXPENDITURES TOTAL	(856,112)	(130,715)	725,397	15%
	Department Net Totals	0	519,280		
	Fund Total Revenues	856,112	649,996		
	Fund Total Expenditures	(856,112)	(130,715)		
	Fund Net	0	519,280		

720 - Equipment Fund

Department #56 - Equipment Rental & Repl

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
440190	External Work Performed	1,000	0	(1,000)	0%
440220	Equipment Rental	553,127	276,565	(276,562)	50%
440221	City Hall Vehicle Replacement	19,500	9,750	(9,750)	50%
440222	Parks Veh & Equipment Replacen	29,633	14,819	(14,814)	50%
440224	Public Works Veh & Equipment R	265,251	108,058	(157,193)	41%
440227	Light/Fire Maintenance	50,000	30,264	(19,736)	61%
440228	Sale of Gas & Oil	66,000	28,129	(37,871)	43%
445015	Sale of Equipment	10,000	4,999	(5,001)	50%
450057	Other	0	747	747	0%
470105	Interest	4,000	2,812	(1,188)	70%
481005	Transfer from Other Funds	228,180	78,145	(150,036)	34%
495005	Fund Bal Avail. for Approp.	1,108,224	431,776	(676,448)	39%
495010	City Hall Veh Fund Bal Avail	0	79,360	79,360	0%
495011	Parks Veh Fund Bal Avail	0	99,623	99,623	0%
495012	Police Veh Fund Bal Avail	0	159,183	159,183	0%
495013	PW Veh Fund Bal Avail	0	279,223	279,223	0%
	REVENUES TOTAL	2,334,915	1,603,454	(731,461)	69%
EXPENDITURES					
511005	Regular Employee Wages	(144,540)	(65,371)	79,169	45%
511015	Overtime	(2,069)	0	2,069	0%
512005	Health & Dental Benefits	(45,186)	(22,498)	22,688	50%
512008	Health Reimb Arrangement	(1,605)	(843)	762	53%
512010	Retirement	(27,343)	(12,391)	14,952	45%
512015	FICA	(11,216)	(4,961)	6,255	44%
512020	Workers Comp	(3,207)	(3,207)	0	100%
512025	Other Benefits	(1,604)	(582)	1,022	36%
512030	Other Payroll Taxes	(1,133)	(506)	627	45%
520110	Operating Supplies	(1,500)	(431)	1,069	29%
520130	Personnel Uniforms & Equipment	(1,000)	(365)	635	37%
520190	Computer Software	(7,000)	(1,411)	5,589	20%
520220	Small Equipment	(2,000)	(477)	1,523	24%
520503	Printing	(100)	(29)	71	29%
520506	Postage	(1,000)	(31)	969	3%
520509	Telephone	(350)	(153)	197	44%
520557	Intergovernmental Services	(1,000)	(347)	653	35%
520578	Insurance & Bonds	(28,686)	(28,936)	(250)	101%
521003	Training/Conferences	(1,000)	288	1,288	-29%
521150	Professional Services	(3,000)	(1,292)	1,708	43%
521168	Misc Medical Services	0	(38)	(38)	0%
521172	Bank Service Fees	(200)	(111)	89	56%
522003	Equipment Maint & Oper Supplies	(1,600)	(41)	1,559	3%
522009	Vehicle Maint & Oper Supplies	(68,000)	(22,074)	45,926	32%
522010	Vehicle Maint External	(25,000)	(7,314)	17,686	29%
522012	Fuel/Oil	(250,000)	(64,535)	185,465	26%
522022	Information Systems Fund Charge	(3,771)	(1,886)	1,885	50%
522306	Rents & Leases	(200)	0	200	0%
522309	Building/Facility Rental	(23,400)	(11,700)	11,700	50%
550181	Major Tools & Work Equipment	(14,000)	(13,434)	566	96%
551262	City Hall Veh Replacement	(50,000)	(22,937)	27,064	46%
551263	Parks Veh & Equipmnt Replacem	(72,500)	(58,147)	14,353	80%

720 - Equipment Fund

Department #56 - Equipment Rental & Repl

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
551264	Police Veh & Equipmnt Replacem	(154,000)	(85,867)	68,133	56%
551265	Public Works Veh & Equipmnt Re	(625,000)	(156,278)	468,722	25%
580206	Contingency	(250,000)	0	250,000	0%
590304	Unapp Fund Balance	(463,572)	0	463,572	0%
	EXPENDITURES TOTAL	(2,285,782)	(587,906)	1,697,876	26%
	Department Net Totals	49,133	1,015,548		
	Fund Total Revenues	2,334,915	1,603,454		
	Fund Total Expenditures	(2,285,782)	(587,906)		
	Fund Net	49,133	1,015,548		

730 - City Utility Fund

Department #12 - Administration Services

For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
481005	Transfer from Other Funds	150,598	66,995	(83,603)	44%
481010	Transfer from Water Fund	58,199	52,736	(5,463)	91%
	REVENUES TOTAL	208,797	119,731	(89,066)	57%
EXPENDITURES					
520151	City Hall Utilities	(30,750)	(11,722)	19,028	38%
520152	Library Utilities	(32,250)	(15,848)	16,402	49%
520153	Aquatics Utilities	(46,000)	(23,683)	22,317	51%
520154	Parks Utilities	(57,000)	(49,871)	7,129	87%
520155	Police Utilities	(23,100)	(9,769)	13,331	42%
520156	Fire Utilities	(16,097)	(7,810)	8,287	49%
520157	Engineering Building Utilities	(3,600)	(1,029)	2,571	29%
	EXPENDITURES TOTAL	(208,797)	(119,731)	89,066	57%
	Department Net Totals	0	0		
	Fund Total Revenues	208,797	119,731		
	Fund Total Expenditures	(208,797)	(119,731)		
	Fund Net	0	0		

740 - Risk Management Fund
Department #12 - Administration Services
For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
444126	P/L Insurance - General Fund	142,921	142,921	0	100%
444127	P/L Insurance - Other Funds	142,169	142,169	0	100%
444128	WC Insurance - General Fund	117,900	118,654	754	101%
444129	WC Insurance - Other Funds	61,475	56,735	(4,740)	92%
470105	Interest	2,525	1,216	(1,309)	48%
471035	SAIF Dividend	0	45,599	45,599	0%
472005	Miscellaneous	0	7,976	7,976	0%
495005	Fund Bal Avail. for Approp.	600,053	598,257	(1,796)	100%
	REVENUES TOTAL	1,067,043	1,113,527	46,484	104%
EXPENDITURES					
520585	Property/Liability Premiums	(288,762)	(294,595)	(5,833)	102%
520586	P/L Claims - General Fund	(5,000)	(6,400)	(1,400)	128%
520589	Risk Management Activities	(14,234)	0	14,234	0%
520590	Worker's Compensation Premium	(181,065)	(188,633)	(7,568)	104%
520591	WC Claims - General Fund	(6,000)	(3,289)	2,711	55%
550460	Accounting System	(344,437)	(204)	344,233	0%
570103	Transfer to General Fund	(40,000)	(40,000)	0	100%
580206	Contingency	(50,000)	0	50,000	0%
590303	Reserved Fund Balance	(137,545)	0	137,545	0%
	EXPENDITURES TOTAL	(1,067,043)	(533,122)	533,921	50%
	Department Net Totals	0	580,406		
	Fund Total Revenues	1,067,043	1,113,527		
	Fund Total Expenditures	(1,067,043)	(533,122)		
	Fund Net	0	580,406		

850 - Forest Grove Urban Renewal Agency
 Department #85 - Forest Grove Urban Renewal Agency
 For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
REVENUES					
411003	Property Taxes	43,021	27,079	(15,942)	63%
470105	Interest	50	36	(14)	72%
	REVENUES TOTAL	43,071	27,115	(15,956)	63%
EXPENDITURES					
520110	Operating Supplies	(250)	0	250	0%
520270	Miscellaneous	(6,446)	0	6,446	0%
520530	Memberships	(250)	0	250	0%
520557	Intergovernmental Services	(5,000)	0	5,000	0%
521113	Attorney Services	(8,000)	0	8,000	0%
521171	Financial Services, Auditing, Inves	(2,500)	0	2,500	0%
562085	Interest - Forest Grove Urban Rer	(20,625)	0	20,625	0%
	EXPENDITURES TOTAL	(43,071)	0	43,071	0%
	Department Net Totals	0	27,115		
	Fund Total Revenues	43,071	27,115		
	Fund Total Expenditures	(43,071)	0		
	Fund Net	0	27,115		

850 - Forest Grove Urban Renewal Agency
Department #85 - Forest Grove Urban Renewal Agency
For the Period Ending: December, 2015

Budget Year Elapsed → 50%

G/L Account #	Account Name	Budget	YTD Activity	Balance	% Used
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ORDINANCE NO. 2016-02

ORDINANCE AMENDING CITY OF FOREST GROVE TRANSPORTATION SYSTEM PLAN FOR INCLUSION OF THE COUNCIL CREEK REGIONAL TRAIL PREFERRED ALIGNMENT THROUGH CITY OF FOREST GROVE; FILE NO. 311-15-00033-PLNG

WHEREAS, in 2002, the Council Creek Trail was designated a regional trail by Metro and furthermore, Washington County, City of Cornelius, City of Banks, City of Hillsboro, and City of Forest Grove had existing plans that identified the Council Creek Regional Trail concept; and

WHEREAS, the City of Forest Grove solicited input on the Council Creek Regional Trail concept and partnered with other Agencies, including Metro, State Parks, Washington County, City of Hillsboro, City of Cornelius, and City of Banks; and

WHEREAS, in 2008, the City of Forest Grove Council and the Partner Agencies adopted resolutions and provided letters of support for implementing the Council Creek Regional Trail; and

WHEREAS, in 2009, the Oregon Department of Transportation (ODOT) accepted a City of Forest Grove application submitted in 2009, for funding the Council Creek Regional Trail Master Plan; and

WHEREAS, in 2012, the City entered into a Local Agency Agreement with ODOT for the purposes of receiving the Council Creek Regional Trail Master Plan funding; and

WHEREAS, the Council Creek Regional Trail Master Plan was a coordinated effort between local, regional and state governments, local advisory committees, and included an extensive public involvement process with three open house events and a month long public review period; and

WHEREAS, in August 2015, the Council Creek Regional Trail Master Plan was completed and Published; and

WHEREAS, the Planning Commission recommendation on February 15, 2016, to amend the text of the Forest Grove Transportation System Plan describing and showing the preferred route of the Council Creek Regional Trail through Forest Grove; and

WHEREAS, the City Council held a duly-noticed Public Hearing on the proposed recommendation on March 14 and continued the hearing on March 28, 2016.

NOW THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1: The City Council of the City of Forest Grove hereby amends the Forest Grove Transportation System to reflect the Council Creek Regional Trail Master Plan preferred alignment through Forest Grove as summarized on Exhibit A.

Section 2. All other ordinances or portions thereof inconsistent or conflicting with this ordinance or any portions hereof are hereby repealed to the extent of such inconsistency or conflict.

Section 3. This ordinance shall be effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 14th day of March, 2016.

PASSED the second reading this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

Ordinance No. 2016-02
Exhibit A
TRANSPORTATION SYSTEM PLAN AMENDMENTS

The TSP text amendments are summarized below. The amendments affect TSP Chapter 5 (Pedestrian System Plan) and Chapter 6 (Bicycle System Plan)

TSP Chapter 5: Pedestrian System Plan

Amend Table 5-1 (Pedestrian System Projects and Programs) to remove the Council Creek Trail Feasibility Study as this has been completed. Text for removal is shown with strikeout and text to add is shown with double underline.

Project	Segment	Description	Planning-Level Cost Estimate (thousands)
Council Creek Regional Trail Feasibility Study <u>Improvements</u>	N/A <u>Three</u>	Conduct feasibility study evaluating potential alignments for the Council Creek Trail in Forest Grove Construct Council Creek Regional Trail through Forest Grove consistent with the Council Creek Regional Trail Master Plan (August 2015).	\$200 <u>\$5,200</u>
Accessway Improvements	Citywide	Conduct citywide inventory of existing neighborhood accessways, and implement improvements (e.g., paving, re-paving, etc.) as needed	\$500
Safe Routes to School improvements	N/A	Inventory bicycle/pedestrian facilities near Forest Grove schools, and identify specific deficiencies that complicate bicyclist and pedestrian travel. Design and construct infrastructure improvements, including shared use paths, neighborhood accessways, bike lanes, sidewalks, curb ramps, crosswalks, and other intersection improvements where necessary. Assign higher prioritization to projects along major bike- and walk-to-school routes	\$1,000
Sidewalk Infill Program	Citywide	Fund an annual Sidewalk Infill Program to complete sidewalk gaps on existing streets	\$50 ⁴
ADA Transition Plan	Citywide	Develop an ADA Transition Plan identifying specific projects/strategies for bringing existing sidewalks and other	\$50

		pedestrian facilities into compliance with ADA standards	
Spot Improvement Program	Citywide	Fund an annual Spot Improvement Program to address bicycle/pedestrian system needs	\$50 ⁴
Bikeway/Walkway Maintenance Program	Citywide	Develop and implement an annual Maintenance Program to provide regularly-scheduled maintenance activities for the on- and off-street bikeway and walkway system	\$20 ⁴
Total			\$5,155 \$10,355

Add a new Figure 5-4 and Council Creek Regional Trail Preferred Alignment through the Forest Grove Segment 3, shown below, to the Pedestrian and Bicycle System Plan Chapters of the TSP.

Council Creek Regional Trail - Forest Grove Segment 3



Council Creek Regional Trail Master Plan
Preferred Alternative
Segment 3
Forest Grove
August 2015

Trail Type

- Multiuse Trail
- Street-adjacent Multiuse
- On-Street
- Multiuse Rail-with-Trail
- Multiuse Boardwalk
- Bridge or Undercrossing
- Conceptual Trailhead Location

Crossing Type

- X Collector/Arterial Road Crossing
- X Major Stream Crossing
- X Minor Stream Crossing
- ⚠ New Railroad Crossing
- Trail Segment Boundary
- Railroad
- Taxlot Boundary

- Park
- Natural Area
- Private Recreation Area
- Cemetery
- Public Land
- Streams
- Waterbody
- Wetland Area
- FEMA 100 Yr. Flood Plain

TSP Chapter 6: Bicycle System Plan

Amend Table 6-2 (Bicycle System Projects and Program) to add the Council Creek Regional Trail:

Project	Segment	Description	Planning Level Cost Estimate (thousands)
Pacific Ave.	B St. to E St.	Re-stripe roadway to provide bike lanes	\$7
Maple St. / Fern Hill Rd.	Hwy. 47 to Taylor Way	Re-stripe roadway to provide bike lanes	\$15
B St. ¹	Gales Cr. bridge to 19 th Avenue	Re-stripe roadway to provide bike lanes	\$13
Hawthorne St.	26 th Ave. to Pacific Ave.	Re-stripe roadway to provide bike lanes	\$12
Thatcher Rd.	Gales Creek Rd. to David Hill Rd.	Re-stripe roadway to provide bike lanes	\$15
Willamina Ave.	Thatcher Rd. to Sunset Dr.	Re-stripe roadway to provide bike lanes	\$18
Gales Cr. Rd.	Western UGB to Forest Gale Dr.	Construct shoulder bikeway	\$388
Thatcher Rd.	David Hill Rd. to northern UGB	Construct shoulder bikeway	\$582
Fern Hill Rd.	Southern UGB to Taylor Way	Construct shoulder bikeway	\$394
18th Ave./17th Place	B St. to Hwy. 47 Path	Develop Bicycle Boulevard	\$77
Cedar St.	Hwy. 47 Path to 24th Ave.	Develop Bicycle Boulevard	\$65
B St.	19th Ave. to David Hill Road	Develop Bicycle Boulevard	\$70
Willamina Ave./Goff Rd./23rd Ave.	Gales Cr. Rd. to Main St.	Develop Bicycle Boulevard	\$83
Bicycle Wayfinding Signage Plan	N/A	Develop citywide bicycle Wayfinding Signage Plan identifying: appropriate locations for signs, destinations to be highlighted on each sign, and approximate distance and riding time to each destination	\$20

Zoning Ordinance bicycle parking requirements update	N/A	Update Zoning Ordinance to establish short-term bicycle parking requirements for additional individual land uses, and to establish long-term parking requirements	\$10
<u>Council Creek Regional Trail Improvements</u>	<u>Three</u>	<u>Construct Council Creek Regional Trail through Forest Grove consistent with the Council Creek Regional Trail Master Plan (May 2015).</u>	<u>\$5,200</u>
Total			\$1,769 <u>6,979</u>



CITY RECORDER USE ONLY:

AGENDA ITEM #: 7,8,9,11

FINAL ACTION: 5/12

A place where families and businesses thrive.

CITY COUNCIL STAFF REPORT

TO: City Council

FROM: Jesse VanderZanden, City Manager

MEETING DATE: March 28, 2016

PROJECT TEAM: Jon Holan, Community Development Director

SUBJECT TITLE: Transmittal of Public Testimony on Mixed Use Code/Marijuana Options

ACTION REQUESTED:

	Ordinance	Resolution	Motion	X	Informational
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X all that apply

ISSUE STATEMENT:

Staff is transmitting written testimony received from Michael Goff in support of the proposed mixed use code amendments (Ordinances 2016-03, 2016-04 and 2016-05).

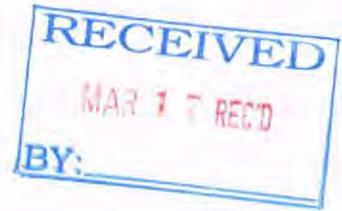
In addition, staff is working with the City Attorney's office about potential options the Council may want to consider in light of the testimony received from TTM at the last hearing. Once we receive all the input from the City Attorney, staff will be transmitting a memo with options to the Council.

STAFF RECOMMENDATION:

Please consider the attached comments from Michael Goff as part of the Council decision on the proposed Development Code amendments.

ATTACHMENT(s):

Letter from Michael Goff



Michael Goff
1263 Goff Road, Forest Grove, OR 97116
206-554-1989
michael.k.goff@gmail.com

Subject: Testimony in favors of Ordinances 2016-03, 2016-04, and 2016-05

Dear Mayor Truax, Council President Johnston, and Councilors Kidd, Lowe, Thompson, Uhing, and Wenzl,

I am writing in favor of Ordinances 2016-03, 2016-04, and 2016-05. These important measures will allow for new mixed used development in Forest Grove, and they have the following virtues.

- Higher density development fills an urgent need for new housing in the Portland Metro region. From 2005 to 2014, median rent in the Portland Metro region went from 101% of the national average to 108%, and vacancy rates in 2014 have plunged to 3.57% in Oregon and 2.21% in Portland, far below the national average of 6.32% [1].
- Our region has fallen behind the nation in tackling homelessness, and a lack of housing is a major reason for this [2].
- New housing in Forest Grove, particularly denser, mixed-used development, will strengthen our economy and reduce our environmental impacts.
- The system of Density Incentives offers a way to achieve both density and other public goals while retaining flexibility.

As additional zoning changes are developed, I urge you to consider the extensive unmet need for affordable housing throughout our region and to continue to pave the way for dense, mixed-use development.

Thank you.

Michael Goff

[1] Department of Numbers. "Portland Oregon Residential Rent and Rental Statistics". Accessed March 17, 2016. <http://www.deptofnumbers.com/rent/oregon/portland/>
[2] Anna Griffin. "Our Homeless Crisis". The Oregonian, accessed March 17, 2016. <http://www.oregonlive.com/portland-homeless/>

ORDINANCE NO. 2016-03**ORDINANCE AMENDING FOREST GROVE DEVELOPMENT CODE ARTICLE 3,
ARTICLE 4, ARTICLE 7 AND ARTICLE 12 AND DESIGN GUIDELINE HANDBOOK
TO IMPLEMENT POLICIES OF THE FOREST GROVE COMPREHENSIVE PLAN
FILE NO. 311-15-00020-PLNG**

WHEREAS, The City of Forest Grove approved Ordinance 2014-01 and 2014-02 updating the Forest Grove Comprehensive Plan; and

WHEREAS, the updated Forest Grove Comprehensive Plan requires specific revisions to the Forest Grove Development Code to implement policies contained in the updated Forest Grove Comprehensive Plan; and

WHEREAS, the City prepared the required amendments and forwarded the proposed amendments to the Planning Commission for consideration on January 5, 2016; and

WHEREAS, the City desires to make other amendments to the Development Code to promote the efficient use of land within commercial areas, encourage housing near transit and shopping; and

WHEREAS, the Planning Commission held a public hearing on the proposed amendments on January 5, 2016; and

WHEREAS, the Planning Commission adopted Planning Commission Findings and Decision Number 16-01 recommending approval of the proposed Development Code amendments; and

WHEREAS, the City mailed by first class mail notice required under Measure 56 to affected property owners on January 19, 2016; and

WHEREAS, the City Council held a duly-noticed Public Hearing on the proposed ordinance on March 14, 2016, and continued the hearing on March 28, 2016.

NOW THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1. The City Council hereby adopts the Planning Commission's Findings and Decision dated January 12, 2016.

Section 2. The City Council of the City of Forest Grove hereby adopts the text amendments to the Development Code, Articles 3, 4, 7 and 12 as shown on Exhibit A.

Section 3. The City Council hereby finds that the proposed amendments are consistent with and meet the provisions of Development Code Section 10.2.630 *Review Criteria Pertaining to Zoning Text Amendments* as shown on Exhibit B.

Section 4. All other ordinances or portions thereof inconsistent or conflicting with this ordinance or any portions hereof are hereby repealed to the extent of such inconsistency or conflict.

Section 5. This ordinance shall be effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 14th day of March, 2016.

PASSED the second reading this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

City of Forest Grove Development Code – Draft Amendments	Commentary
<p>Article 1 INTRODUCTION & PROCEDURES</p> <p>10.1.000 HOW TO USE THIS CODE</p> <p>This Development Code (Code) is organized as a reference document and is not intended to be read from cover to cover. Instead, it is organized so you may look up only the parts you need. The list of articles in the table of contents is very important, as are the section listings at the beginning of each article. Tables and charts are used in many places to summarize information.</p> <p>The Code is divided into nine articles, with each article containing related information. Note that numbers 9 – 11 are held for future articles:</p> <p>Article 1 - Introduction & Procedures Article 2 - Land Use Reviews Article 3 - Zoning Districts Article 4 - Overlay Districts Article 5 - Special Provisions Article 6 - Land Divisions Article 7 - Miscellaneous Provisions Article 8 - General Development Standards Article 12 - Use Categories & Definitions</p> <p>Article 1 provides basic information on the purpose and legal framework of the Code and describes how land use permits are classified for review under four different categories of procedure (Type I – Type IV). Information on application requirements, application review, public notice, public hearings and appeals are included in this article along with general provisions on administering the Code.</p> <p>Article 2 describes the various land use reviews and assigns decision authority. Some reviews may be applied for at the discretion of the applicant, such as a conditional use or adjustment request. Other reviews are mandatory in certain situations, such as design review. Article 2 includes the approval criteria for the land use reviews and cross-references the applicable procedures from Article 1.</p> <p>Article 3 contains the standards for the different zoning districts. The districts are grouped into the following categories:</p> <ul style="list-style-type: none"> • Residential Zones • Institutional Zone • Commercial <u>and Mixed Use</u> Zones • Town Center Zones • Industrial Zones <p>Article 3 states which uses are permitted in each district, which uses are allowed in limited situations, which are conditional uses, and which are prohibited. The general development standards for each district are also included in Article 3. The development standards include requirements such as maximum building heights and setbacks.</p> <p>Article 4 contains the standards for overlay districts. Overlay districts consist of regulations that address specific subjects that may be applicable in a variety of areas in the City. The Code includes two <u>three</u> overlay districts:</p>	<p>No changes are proposed to Articles 1 and 2.</p> <p>In Article 3 a new mixed use zone has been added to the Commercial Zones and amendments to consolidate the Town Center Transition (TCT) and Town Center Support (TCS) zones.</p>

City of Forest Grove Development Code – Draft Amendments	Commentary
<ul style="list-style-type: none"> • Master Plan • Planned Developments • <u>Mixed Use Planned Developments</u> <p>Overlay districts apply in conjunction with the base zoning district and can modify the regulations and standards of the base district. The Official Zoning Map identifies the location of the zoning districts and overlay districts.</p> <p>Article 5 establishes the Natural Resource Areas process to protect environmentally sensitive areas. Special provisions and standards for historic landmarks, manufactured home parks and tree protection are also included in this Article:</p> <ul style="list-style-type: none"> • Natural Resource Areas • Historic Landmarks • Tree Protection • Manufactured Dwelling Parks • Solar Access • Recreational Vehicle Parks <p>Review procedures, approval criteria and development and design standards for special provisions are cross-referenced or included in this article.</p> <p>Article 6 deals with land divisions. This article describes submittal requirements, approval criteria and design standards for lot line adjustments, partitions and subdivisions. Article 1 procedures for review of land divisions are cross-referenced. Article 8 standards for public improvements are also cross-referenced.</p> <p>Article 7 contains the regulations for specific uses (such as home occupations and accessory structures) that may be developed in several zoning districts. The regulations in Article 7 generally supplement the regulations of the Article 3 zoning districts.</p> <p>Article 8 describes the general development standards that are applicable to all new development or intensification of existing development (including land divisions and development on existing lots). This article includes standards for parking, landscaping, public improvements, signs, and other topics. The general development standards are grouped in Article 8 to provide consolidated information and less repetition in code language.</p> <p>Articles 9 – 11 are held for future topics.</p> <p>Article 12 describes the framework for the classification of individual uses into broad land use categories. This article also includes definitions of words that are not in common use or that have a specific meaning in the Code.</p>	<p>A new overlay district is proposed to be included in Article 4.</p> <p>A new density bonus section is proposed for Article 7. Alternatively, this section could be included in Article 3. Also 10.7.300 (cell towers) has been amended to include NMU.</p> <p>Article 8 has been amended to address parking for MF in the Town Center.</p> <p>Article 12 has been amended to add a definition of Car-Share Organization and modify definition of Net Density</p>

City of Forest Grove Development Code – Draft Amendments	Commentary																																																				
Article 2 LAND USE REVIEWS	No changes																																																				
Article 3 ZONING DISTRICTS																																																					
<p>10.3.010 CLASSIFICATION OF ZONES</p> <p>All areas within the corporate limits of the City of Forest Grove are divided into the following zones:</p> <p style="text-align: center;">Table 3-1 Zoning Districts</p> <table border="1" data-bbox="375 619 1019 1874"> <thead> <tr> <th><u>Zoning District</u></th> <th><u>Map Symbol</u></th> </tr> </thead> <tbody> <tr><td>Single-Family Residential</td><td>R-5</td></tr> <tr><td>Single-Family Residential</td><td>R-7</td></tr> <tr><td>Single-Family Residential</td><td>R-10</td></tr> <tr><td>Suburban Residential</td><td>SR</td></tr> <tr><td colspan="2"> </td></tr> <tr><td>Multi-Unit (Low) Residential</td><td>RML</td></tr> <tr><td>Multi-Unit (High) Residential</td><td>RMH</td></tr> <tr><td colspan="2"> </td></tr> <tr><td>Institutional</td><td>INST</td></tr> <tr><td colspan="2"> </td></tr> <tr><td>Community Commercial</td><td>CC</td></tr> <tr><td>Commercial – Neighborhood</td><td>NC</td></tr> <tr><td><u>Neighborhood Mixed Use</u></td><td><u>NMU</u></td></tr> <tr><td colspan="2"> </td></tr> <tr><td>Town Center Core</td><td>TCC</td></tr> <tr><td>Town Center Support</td><td>TCS</td></tr> <tr><td>Town Center Transition</td><td>TCT</td></tr> <tr><td colspan="2"> </td></tr> <tr><td>Light Industrial</td><td>LI</td></tr> <tr><td>General Industrial</td><td>GI</td></tr> <tr><td colspan="2"> </td></tr> <tr> <th><u>Overlay District (Article 4)</u></th> <th><u>Map Symbol</u></th> </tr> <tr><td>Master Plan</td><td>MP</td></tr> <tr><td>Planned Development</td><td>PD</td></tr> <tr><td><u>Mixed Use Planned Development</u></td><td><u>MUPD</u></td></tr> </tbody> </table>	<u>Zoning District</u>	<u>Map Symbol</u>	Single-Family Residential	R-5	Single-Family Residential	R-7	Single-Family Residential	R-10	Suburban Residential	SR			Multi-Unit (Low) Residential	RML	Multi-Unit (High) Residential	RMH			Institutional	INST			Community Commercial	CC	Commercial – Neighborhood	NC	<u>Neighborhood Mixed Use</u>	<u>NMU</u>			Town Center Core	TCC	Town Center Support	TCS	Town Center Transition	TCT			Light Industrial	LI	General Industrial	GI			<u>Overlay District (Article 4)</u>	<u>Map Symbol</u>	Master Plan	MP	Planned Development	PD	<u>Mixed Use Planned Development</u>	<u>MUPD</u>	<p>New Neighborhood Mixed Use zone added to table.</p> <p>TCS zone deleted as part of consolidation of Town Center zones.</p>
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City of Forest Grove Development Code – Draft Amendments	Commentary
<p>COMMERCIAL AND MIXED USE ZONES</p> <p>10.3.300 PURPOSE</p> <p>The City of Forest Grove has established two commercial zones to implement the Commercial designation of the Comprehensive Plan and one zone to implement the Mixed Use designation of the Comprehensive Plan. The Neighborhood Commercial zone provides for limited commercial activities at a neighborhood scale. The Community Commercial zone is established to accommodate commercial uses with a community market focus and promote a concentration of mixed uses along the regional transit corridor. <u>The Neighborhood Mixed Use zone is established to encourage the development of pedestrian-friendly mixed use neighborhoods.</u> Additional commercial development opportunities are provided in the Town Center Zones.</p>	<p>A new mixed use zone is proposed to be added to the Commercial Zones section of the code.</p>
<p>10.3.310 LIST OF COMMERCIAL AND MIXED USE ZONES</p> <p>A. <u>Neighborhood Commercial (NC)</u></p> <p>The NC zone is established to provide for small to medium sized shopping and service facilities adjacent to residential neighborhoods. The district is intended to meet the convenience shopping and service needs of the immediate neighborhood and to have minimal negative impacts on surrounding residential uses. NC zones should be located on arterial or collector streets, preferably at an intersection. NC zones should be spaced at approximately one-half (1/2) mile intervals and each zoned area should be limited to a total size of approximately two (2) acres.</p> <p>B. <u>Community Commercial (CC)</u></p> <p>The CC zone is established to promote a concentration of mixed uses – including retail, service, office and residential uses – along the regional transit corridor. The link between land use and transit is intended to result in an efficient development pattern that supports the regional transit system and makes progress in reducing traffic congestion and air pollution. The location, mix and configuration of land uses are designed to encourage convenient alternatives to the auto, a safe and attractive streetscape, and a more livable community.</p> <p>C. <u>Neighborhood Mixed Use (NMU)</u></p> <p><u>The Neighborhood Mixed Use zone is established to support the development of pedestrian-friendly mixed use neighborhoods with a diversity in the mix of housing types and neighborhood-scale retail sales and service, office, civic or recreational uses. Most non-residential uses must be located within a “Village Center”. The Village Center is intended to serve as the center of the neighborhood providing convenient access to goods and services as well as “third places” where residents can gather. The NMU zone implements the Comprehensive Plan’s Mixed Use designation. Mixed Use Planned Development approval is required in the NMU zone in order to ensure that the objectives of the Comprehensive Plan are addressed.</u></p>	<p>Purpose statement for the NMU zone emphasizes mixed use and pedestrian orientation.</p>
<p>10.3.320 USE REGULATIONS</p> <p>Refer to Article 12 for information on the characteristics of uses included in each of the Use Categories.</p> <p>A. <u>Permitted Uses.</u> Uses allowed in the Commercial zones are listed in Table 3-10 with a “P”. These uses are allowed if they comply with the development standards and other regulations of this Code.</p> <p>B. <u>Limited Uses.</u> Uses that are allowed subject to specific limitations are listed in Table 3-10 with an “L”. These uses are allowed if they comply with the limitations listed in the footnotes to the table</p>	

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and the development standards and other regulations of this Code.

- C. **Conditional Uses.** Uses that are allowed if approved through the conditional use process are listed in Table 3-10 with a “C”. These uses are allowed provided they comply with the conditional use approval criteria, the development standards, and other regulations of this Code. Section 10.2.200 contains the conditional use process and approval criteria.
- D. **Not Permitted Uses.** Uses listed in Table 3-10 with an “N” are not permitted or prohibited. Existing uses may be subject to the regulations of Section 10.7.100, Nonconforming Development.
- E. **Accessory Uses.** Uses that are accessory to a primary use are allowed if they comply with specific regulations for accessory uses and all development standards.

TABLE 3-10 Commercial and Mixed Use Zones Use Table

USE CATEGORY	NC	CC	NMU
RESIDENTIAL	L ^[1]	L ^[2]	<u>P/L</u> ^[13]
Household Living			
Group Living	N	P	<u>N</u>
Transitional Housing	N	C	<u>N</u>
Home Occupation	L ^[3]	L ^[3]	<u>L</u> ^[3]
Bed and Breakfast	L ^[4]	P	<u>L</u> ^[4]
CIVIC / INSTITUTIONAL			
Basic Utilities	P	P	<u>P</u>
Major Utility Transmission Facilities	C	C	<u>C</u>
Colleges	N	C	<u>N</u>
Community Recreation	N	P	<u>L</u> ^[14]
Cultural Institutions	P	P	<u>L</u> ^[14]
Day Care	P	P	<u>L</u> ^[13]
Emergency Services	C	C	<u>L</u> ^[14]
Postal Services	C	P	<u>L</u> ^[14]
Religious Institutions	C	P	<u>L</u> ^[14]
Schools	C	C	<u>L</u> ^[14]
Social/ Fraternal Clubs / Lodges	C	P	<u>L</u> ^[14]
COMMERCIAL			
Commercial Lodging	N	L ^[5]	<u>L</u> ^[14]
Eating and Drinking Establishments	L ^[6]	P	<u>L</u> ^[14]
Entertainment – Oriented:			
- Major Event Entertainment	N	N	<u>N</u>
- Outdoor Entertainment	N	N	<u>N</u>
- Indoor Entertainment	N	P	<u>L</u> ^[14]
General Retail:			

New NMU allows a wide range of uses with an approved MUPD.

Those uses with footnote 13 are allowed anywhere within the MUPD.

Those uses with footnote 14 are only allowed within a Village Center.

Those uses with footnote 15 are restricted to uses existing as of the date of the ordinance is adopted (i.e., no new uses).

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- Sales – Oriented	P	P	<u>L^[14]</u>	
- Personal Services	P	P	<u>L^[14]</u>	
- Repair – Oriented	P	P	<u>L^[14]</u>	
- Bulk Sales	N	P	<u>N</u>	
- Outdoor Sales	^[7] L	^[7] L	<u>L^[14]</u>	
- Animal – Related	N	P		
Medical Centers	N	P	<u>N</u>	
Motor Vehicle Related:		^[8] L	<u>N</u>	
- Motor Vehicles Sale / Rental	N			
- Motor Vehicle Servicing / Repair	N	P	<u>L^[15]</u>	
- Motor Vehicle Fuel Sales	^[9] P	P	<u>N</u>	
Non-Accessory Parking	N	P	<u>N</u>	
Office	P	P	<u>L^[14]</u>	
Self-Service Storage	N	C	<u>N</u>	
INDUSTRIAL				
Industrial Services	N	N	<u>N</u>	
Manufacturing and Production:		^[10] C	<u>N</u>	
- Light Industrial	N			
- General Industrial	N	N	<u>N</u>	
Railroad Yards	N	N	<u>N</u>	
Research and Development	N	N	<u>N</u>	
Warehouse / Freight Movement	N	N	<u>N</u>	
Waste – Related	N	N	<u>N</u>	
Wholesale Sales	N	N	<u>N</u>	
OTHER				
Agriculture / Horticulture	^[12] L	^[12] L	<u>L^[12]</u>	
Cemeteries	N	N	<u>N</u>	
Detention Facilities	N	N	<u>N</u>	
Mining	N	N	<u>N</u>	
Wireless Communication Facilities	^[11] L	^[11] L	<u>L^[12]</u>	

P=Permitted L=Limited C=Conditional Use N=Not Permitted

Footnotes:

- [1] Residential units are permitted in conjunction with a mixed-use development in the NC zone, at a minimum density of 3.48 and a maximum density of 4.35 dwelling units/net acre.
- [2] Residential units are permitted as a stand-alone use or as part of a mixed-use development in the CC zone, at a minimum density of 16.22 units/net acre and a maximum density of ~~20.28~~ 30 units/net acre. Stand-alone residential projects shall have a minimum density of 16.22 units/net acre. There is no minimum density requirement when residential units are constructed over first floor commercial uses.
- [3] Home occupations permitted as an accessory use to residential development, subject to compliance with the home occupation standards in Article 7.
- [4] Bed & breakfast inn limited to three (3) guest rooms in the NC zone, subject to compliance with the bed & breakfast inn standards in Article 7.
- [5] Recreational Vehicle Parks require obtaining a conditional use permit and compliance with the

The proposed amendment to Footnote 2 increases the density in the CC zone from 20.28 to 30 units/net acre.

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<p>requirements of Section 10.5.500 et. seq. All other commercial lodging uses are permitted.</p> <p>[6] Restaurants are permitted in the NC zone (drive-through service is prohibited).</p> <p>[7] Outdoor sales in the NC zone are limited to plants and produce. Outdoor sales areas in the CC zone must be set back at least ten (10) feet from street lot lines and lot lines abutting residential zones and the setback area must be landscaped.</p> <p>[8] Cleaning, sales and repair of motor vehicles and light equipment is permitted outright in the CC zone; sales and rental of heavy vehicles and farm equipment and/or storage of recreational vehicles and boats permitted with conditional use approval.</p> <p>[9] Automobile service station in the NC zone is limited to fuel sales and incidental repair service. [10] As a conditional use pursuant to Section 10.2.200 et. seq., light industrial uses limited within a building no larger than 5,000 square feet in size with no visible emissions or odor outside the building, and with the added criteria that such use does not detract from the commercial viability of the area.</p> <p>[11] Wireless communication facilities are regulated by the standards in Article 7.</p> <p>[12] Domesticated fowl are allowed in conjunction with existing single-family uses and primarily for personal use. Domesticated fowl are allowed subject to these conditions.</p> <ul style="list-style-type: none"> a. Up to 4 adult fowl over 6 months of age may be kept on any lot with a minimum area of 5,000 square feet. One additional adult fowl shall be permitted for each 2,000 square feet of additional lot area, up to a maximum of 12 fowl. b. No roosters shall be permitted. c. Animal waste matter shall not be allowed to accumulate. d. All animal food shall be stored in metal or other rodent-proof containers. e. Fencing shall be designed and constructed to confine all animals to the owner’s property. f. All structures that house fowl shall be located at least 20 feet from all residences (except the animal owner’s). g. All structures that house fowl shall be located at least 5 feet from any side or rear property line. <p>[13] <u>Residential and day care uses – Existing uses are permitted outright. New uses are permitted within a Mixed Use Planned Development approved in accordance with Section 10.4.300. Residential density shall be established in the Mixed Use Planned Development.</u></p> <p>[14] <u>Use is only permitted within the Village Center of a Mixed Use Planned Development approved in accordance with Section 10.4.300.</u></p> <p>[15] <u>Restricted to uses existing as of [DATE OF ADOPTED ORDINANCE]</u></p>	<p>Those uses with footnote 13 are allowed anywhere within the MUPD.</p> <p>Those uses with footnote 14 are only allowed within a Village Center.</p> <p>Those uses with footnote 15 are restricted to uses existing as of the date of the ordinance is adopted (i.e., no new uses).</p>

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10.3.330 COMMERCIAL AND MIXED USE ZONE DEVELOPMENT STANDARDS

A. Purpose

The development standards for the commercial and mixed use zones are intended to promote efficient site planning, control the overall scale of buildings, and promote streetscapes that are consistent with the desired character of the two zones.

B. Development Standards

Development standards for the NC, ~~and~~ CC and NMU zones are summarized below:

**TABLE 3-11
Commercial and Mixed Use Zones: Dimensional Requirements**

STANDARD	NC	CC	NMU
Maximum Use Size ^[1]	2,000 square feet	No maximum	<u>No maximum</u> ^[5]
Minimum Lot Size	5,000 square feet	5,000 square feet	<u>No minimum</u> ^[5]
Minimum Lot Width	50 feet	50 feet	<u>No minimum</u> ^[5]
Minimum Lot Depth	100 feet	None	<u>No minimum</u> ^[5]
Minimum Setbacks ^[2]			
- Front	14 feet	None	<u>No minimum</u> ^[5]
- Interior Side	5 feet	None	<u>No minimum</u> ^[5]
- Corner (street side)	14 feet	None	<u>No minimum</u> ^[5]
- Rear	15 feet	None	<u>No minimum</u> ^[5]
Maximum Setback	20 feet	See footnote [3]	<u>No minimum</u> ^[5]
Maximum Building Height ^[4]	35 feet	45 feet	<u>35 feet</u> <u>45 feet</u> ^[6]
Minimum Landscaped Area	15% of site	15% of site	<u>No minimum</u> ^[5]

As noted in footnote 5, Development standards will be established through the MUPD process.

Footnote 6 provides alternative height restriction for buildings in Village Center with MUPD approval.

Footnotes:

[1] Individual uses in the NC zone are limited to a maximum gross floor area of 2,000 square feet by right. Uses larger than 2,000 square feet required conditional use permit approval.

[2] New development in the NC and CC zones is subject to Design Review and the Screening & Buffering standards in Article 8. Side or rear yard setbacks may be required where the CC zone abuts a Residential zone. The need for a side or rear yard setback to provide privacy, access to sunlight and a transition between zones shall be evaluated in the Design Review Process.

[3] To ensure that new development is oriented to the street, maximum building setback standards are established in the CC zone, as follows:

a) For sites with one building, a minimum of twenty feet or 50% of the face of the

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<p>building, whichever is greater, shall not exceed the maximum front yard setback of 30 feet. The primary entrance shall be contained within that portion of the building meeting the maximum setback requirement.</p> <p>b) For sites with more than one building, the primary entrance of the building with the largest square footage shall be set back no more that 10% of the depth of the average depth of the lot or 50 feet, whichever is lesser. Satellite buildings shall comply with the setback requirement of (a) above.</p> <p>c) There is no maximum setback for the area east of Oak Street.</p> <p>[4] A step-down in building heights may be required when a new commercial building in the CC zone is proposed within twenty (20) feet of a lower density residential zone (SR, R-10, R-7, and R-5). The need for a step-down in building heights to provide privacy, access to sunlight and a transition between zones shall be evaluated in the Design Review Process. The screening and buffering standards in Article 8 will be used as a guideline.</p> <p>[5] <u>New development in the NMU zone requires approval of a Mixed Use Planned Development (MUPD) in accordance with Section 10.4.300. Development standards are established through the MUPD process.</u></p> <p>[6] <u>Maximum 45 feet building height only within Village Center with approved MUPD.</u></p>		
10.3.340	COMMERCIAL ZONE DESIGN STANDARDS & GUIDELINES	
<p>Commercial development in the NC and CC zones is subject to the design review process in Article 2 and standards in Article 8.</p>		
10.3.400	PURPOSE	
<p>The purpose of the Town Center zones is to implement the Forest Grove Town Center Plan and to reinforce the historic role of the downtown as the civic, financial and business center. Three <u>Two</u> Town Center zones are adopted to reflect the distinctions between different areas of the Town Center and to focus pedestrian-oriented retail uses to the traditional downtown core along Main Street. Specific development and design standards are adopted for the Town Center zones to reflect the established storefront character of the area and to enhance an active and attractive pedestrian environment for shoppers, employees and residents.</p>		
10.3.410	LIST OF TOWN CENTER ZONES	
A.	<u>Town Center Core (TCC)</u>	<p>Proposed amendments delete TCS in favor of TCT and make some changes to TCT to accommodate the consolidation of TCS and TCT zoned lands.</p>
<p>The Town Center Core zone encompasses the traditional downtown core along Main Street. The purpose of this zone is to provide a concentration of retail and office uses in a defined area. Retail, service uses are required on the ground floor, with an emphasis on those uses that serve a walk-in clientele. Office and/or residential uses are allowed on upper floors. The design standards for this zone require a continuous storefront facade featuring streetscape amenities for the pedestrian.</p>		
B.	<u>Town Center Transition (TCT)</u>	
<p>The Town Center Transition zone is established to increase employment and housing opportunities in close proximity to the Town Center Core. A mix of retail, office, light industrial and residential uses are allowed in the TCT zone, but ground floor retail uses are permitted but not required. Consistent streetscape improvements such as street lighting, street trees and sidewalks will link the Town Center Transition zone with the Town Center Core.</p>		

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~~C. Town Center Support (TCS)~~

~~The Town Center Support zone is established to increase employment and housing opportunities that support the Town Center Core. A mix of office, light industrial and residential uses are allowed in the TCS zone. Ground floor retail uses are permitted but not required. Pedestrian and bicycle improvements will link the Town Center Support zone with the Town Center Core.~~

10.3.420 USE REGULATIONS

Refer to Article 12 for characteristics of uses included in each of the Use Categories.

- A. Permitted Uses. Uses allowed in the Town Center zones are listed in Table 3-12 with a “P”. These uses are allowed if they comply with the development standards and other regulations of this Code.
- B. Limited Uses. Uses that are allowed subject to specific limitations are listed in Table 3-12 with an “L”. These uses are allowed if they comply with the limitations listed in the footnotes to the table and the development standards and other regulations of this Code.
- C. Conditional Uses. Uses that are allowed if approved through the conditional use process are listed in Table 3-12 with a “C”. These uses are allowed provided they comply with the conditional use approval criteria, the development standards, and other regulations of this Code. The conditional use process and approval criteria are stated in Section 10.2.200.
- D. Not Permitted Uses. Uses listed in Table 3-12 with an “N” are not permitted. Existing uses may be subject to Section 10.7.100, Nonconforming Development.
- E. Accessory Uses. Uses that are accessory to a primary use are allowed if they comply with specific regulations for accessory uses and all development standards.

**TABLE 3-12
Town Center Zones: Use Table**

USE CATEGORY	TC-Core	TC-Transition	TC-Support
<u>RESIDENTIAL</u> Household Living	L ^[1]	L ^[1]	L ^[H]
Group Living	p ^[1]	P	P
Transitional Housing	N	C	C
Home Occupation	L ^[2]	L ^[2]	L ^[2]
Bed and Breakfast	C ^[2]	P	P
<u>CIVIC / INSTITUTIONAL</u> Basic Utilities	P	P	P
Major Utility Transmission Facilities	C	C	C
Colleges	C	C	C
Community Recreation	N	P	P
Cultural Institutions	P	P	P
Day Care	P	P	P

Proposed amendments delete TC Support (TCS) and consolidate with TC Transition (TCT).

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Emergency Services	C	C	C	<p>Proposed amendments split footnote 3 into 3 and 4 and renumbered all subsequent footnotes.</p> <p>Clarified that footnote 5 which prohibits drive through service from restaurants applies in TCT. This requirement when applied to the expanded TCT area may create some non-conforming uses. This issue should be discussed.</p>
Postal Services	C	P	P	
Religious Institutions	C	P	P	
Schools	C	C	C	
Social/ Fraternal Clubs / Lodges	C	P	P	
COMMERCIAL				
Commercial Lodging	L ^[3 4]	L ^[3 4]	L ^[3]	
Eating and Drinking Establishments	P ^{[4][5]}	P ^{[4][5]}	P	
Entertainment – Oriented:				
- Major Event Entertainment	N	N C	C	
- Outdoor Entertainment	N	N	N	
- Indoor Entertainment	P	P	P	
General Retail:				
- Sales – Oriented	P	P	P	
- Personal Services	P	P	P	
- Repair – Oriented	P	P	P	
- Bulk Sales	L ^[5 6]	L ^[5 6]	L ^[5]	
- Outdoor Sales	N	N	N	
- Animal – Related	N	N	N	
Medical Centers	N	C	C	
Motor Vehicle Related:				
- Motor Vehicles Sale / Rental	N	N P/C ^[12]	N	
- Motor Vehicle Servicing / Repair	N	N C ^[13]	N	
- Motor Vehicle Fuel Sales	N	N C ^[14]	N	
Non-Accessory Parking	N	C	C	
Office	L ^[3]	P	P	
Self-Service Storage	N	N	N	
INDUSTRIAL				
Industrial Services	N	N	N	
Manufacturing and Production:				
- Light Industrial	N	C	C	
- General Industrial	N	N	N	
Call Centers	L ^{[6][7][8]}	L ^[6 7]	L ^[6]	
Railroad Yards	N	N	N	
Research and Development	N	N C	P	
Warehouse / Freight Movement	N	N	N	
Waste – Related	N	N	N	
Wholesale Sales	N	N	N	
OTHER				
Agriculture / Horticulture	N	N	N	
Cemeteries	N	N	N	
Detention Facilities	N	N	N	
Mining	N	N	N	

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Wireless Communication Facilities	L ^[8 9]	L ^[8 9]	L ^[8]	
Information	L ^{[9][10]} [11]	L ^[9 10]	L ^[9]	
<p>P=Permitted L=Limited C=Conditional Use N=Not Permitted</p> <p>Footnotes:</p> <p>[1] New dwellings in the TCC zone are only permitted on or above the 2nd floor. There are no minimum density requirements when housing is part of a mixed-use building. In the TCT zone and TCS zones new dwellings are permitted as “stand-alone” developments or as part of mixed-use developments, but must meet density requirements.</p> <p>[2] Home occupations are permitted as an accessory use to residential uses, subject to compliance with the home occupation standards in Article 7.</p> <p>[3] Offices only permitted as part of a ground-floor retail or personal service use or as a stand-alone use above the first floor in the TC-Core zone. Recreational vehicle parks are prohibited in all districts.</p> <p>[4] <u>Recreational vehicle parks are prohibited in all districts.</u></p> <p>[4-5] Drive through service is prohibited from restaurants in the TC-Core and TC-Transition zones.</p> <p>[5 6] Bulk sales stores with ground floor building footprint small than 10,000 square feet are permitted. All merchandise must be enclosed within a building. All other bulk sales are prohibited.</p> <p>[6 7] Permitted where there are no off premise impacts and no product is transported from the site. Centers with any offsite impacts or transport products from the site are to be located in either Light or General Industrial districts.</p> <p>[7 8] Call Centers shall not be allowed on the ground floor in the TC-Core Zone District.</p> <p>[8 9] Wireless communication facilities are regulated by the standards in Article 7.</p> <p>[9 10] Permitted where there are no off premise impacts.</p> <p>[10 11] Information business is not allowed on the ground floor in the TC-Core Zone District.</p> <p>[12] <u>Motor Vehicles Sales/Rental uses may be allowed as a conditional use if conducted entirely indoors.</u></p> <p>[13] <u>Motor Vehicle Servicing/Repair uses existing as of [DATE OF ADOPTED ORDINANCE] are allowed to continue and may expand within the boundaries of the existing lots 1N436B002900, 1N436B002901, 1N436B003000 with conditional use approval. All other Motor Vehicle Servicing/Repair uses are prohibited.</u></p> <p>[14] <u>Motor Vehicle Fuel Sales uses existing as of [DATE OF ADOPTED ORDINANCE] are allowed to continue and may expand within the boundaries of the existing lot 1S306BA01300 with conditional use approval. All other Motor Vehicle Fuel Sales uses are prohibited.</u></p>				
				<p>Split footnote 3 into 3 and 4 and renumbered all subsequent footnotes.</p> <p>Currently, Footnote 5 (now #6) is not applied to the TCT (or TCS) zone in the table; however, the text of the footnote references TCT. Proposed amendment would make it clear that the prohibition on drive-through service restaurants applies in TCT.</p> <p>Footnotes 13 and 14 allow the continuation and expansion of the existing gas station and auto repair on their existing lots.</p>

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10.3.430 TOWN CENTER ZONE DEVELOPMENT STANDARDS

A. Purpose

The development standards are intended to promote efficient use of land and more intensive development. The standards establish minimum and maximum building heights and maximum building setbacks to reinforce the scale and storefront character of existing historic buildings and to support a pedestrian-oriented environment.

B. Development Standards

Development standards for the Town Center Core (TCC); and Town Center Transition (TCT) ~~and Town Center Support (TCS)~~ zones are summarized below.

**TABLE 3-13
Town Center Zones: Dimensional Requirements**

STANDARD	TCC	TCT	TCS
Floor Area Ratio ^[1] - Minimum - Maximum	1:1 3:1 <u>4:1</u>	0.75 <u>0.5</u> :1 3:1 <u>4:1</u>	0.5 :1 3:1
Building Height (all parts) - Minimum - Maximum	2 stories 4 stories	16 feet 4 stories	16 feet 4 stories
Residential Density ^[2] - Minimum - Maximum	None 20.28 <u>40</u> units /acre ^[7]	16.22 units / acre 20.28 <u>40</u> units / acre ^[7]	16.22 units / acre 20.28 units / acre
Front Setback ^[3] - Minimum - Maximum	0 15 feet	0 15 feet	0 15 feet
Side and Rear Setback ^[4]	0	0	0
Parking ^[5]	Exempt	Exempt	Exempt
Landscaping ^[6]	5 % of lot	5 % of lot	5 % of lot

Footnotes:

[1] Floor area ratio is defined as the ratio of building square footage to gross site square footage. For example, a 5,000 square foot building is required on a 5,000 square foot site (FAR of 1:1); a ~~15,000~~ 20,000 square foot building is allowed (FAR of ~~3:1~~ 4:1).

[2] All densities are based on net acres.

[3] A larger front yard setback may be approved through Design Review if the setback area incorporates enhanced pedestrian spaces and amenities such as plazas, arcades, courtyards, outdoor cafes, widened sidewalks, benches, shelters, street furniture, public art, or kiosks. No parking is allowed between building and public right-of-way.

[4] Side and rear yard setbacks may be required through Design Review when needed to provide a transition

TCS deleted from the table.

Maximum FAR increased from 3:1 to 4:1 for TCC and TCT. Minimum FAR for TCT reduced to 0.5:1.

Base residential density increased 40 du/net ac.

Minimum residential density in TCT proposed to be retained at the current requirement of 16.22 units/acre

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<p>between zones or different land uses. The screening and buffering standards in Article 8 will be used as a guideline.</p> <p>[5] Except for multifamily residential uses, off-street <u>Off-street</u> parking is not required in any of the three Town Center Zones. When off-street parking is provided, it shall be located to the side or rear of buildings, in shared parking lots or in parking structures. Parking and/or maneuvering areas shall not be located between the front facade of the building and the street/sidewalk. <u>Parking for multifamily residential uses shall be provided in accordance with Table 8-5 in Section 10.8.515</u></p> <p>[6] Required landscaping in the Town Center zones may include planters, hanging baskets, and architectural features such as benches and water fountains that are supportive of the Town Center pedestrian environment. Jointly improved landscaped areas are encouraged to facilitate continuity of landscape design.</p> <p>[7] <u>Residential density may be increased above the maximum in accordance with the density incentives in Section 10.7.400. Minimum density does not apply to projects on sites which are less than ½ acres in net area or which include a mix of uses.</u></p>	<p>Footnote 5 amended to specify that off-street parking will be required for MF residential uses in the TC zones.</p> <p>Footnote 7 references new density bonus section.</p>
10.3.440 TOWN CENTER ZONE DESIGN STANDARDS AND GUIDELINES	
<p>Development in all Town Center Zones is subject to the design review process provided in Article 2 and standards provided in Article 8.</p>	
ARTICLE 4 OVERLAY DISTRICTS	
10.4.000 OVERVIEW OF ARTICLE 4	
<p>This Article includes procedures and requirements related to zoning overlay districts. Properties with an overlay designation will also have an underlying zoning. Provision of the overlay district may add or modify requirements of the underlying zoning district. Forest Grove has two overlay districts:</p> <ul style="list-style-type: none"> ▪ Master Plan ▪ Planned Developments ▪ <u>Mixed Use Planned Developments</u> <p>The purpose of the Master Plan Zone is to promote and facilitate the coordinated development of larger-scale institutional facilities through adoption of a master plan. This Article also includes procedures and standards for Planned Developments (PD). The PD procedures are established to provide greater flexibility in the development of land for residential, commercial or industrial purposes than allowed outright by the base zones. <u>The Mixed Use Planned Development procedures are similar to the PD, but are specifically intended for use within the Neighborhood Mixed Use Zone.</u></p>	
<u>MIXED USE PLANNED DEVELOPMENT</u>	
10.4.300 PURPOSE	
<p><u>The purpose of the Mixed Use Planned Development is to ensure that sites zoned Neighborhood Mixed Use are developed into pedestrian-friendly mixed use neighborhoods. In order to accomplish that, Mixed Use Planned Developments need to provide:</u></p> <p><u>A. Diversity in the mix of housing types including single family houses, apartments, row houses,</u></p>	<p>New procedure for Mixed Use Planned Development based on City’s existing PD but with significant changes. MUPDs would</p>

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<p><u>cottages, senior housing, and residential units above commercial or office space;</u></p> <p><u>B. Pedestrian-orientation in the arrangement and placement of buildings, parking and circulation systems, land uses, and utilities;</u></p> <p><u>C. Pedestrian and bicycle access to, and through, the site and provide connectivity to adjacent areas for motorized and non-motorized modes of transportation;</u></p> <p><u>D. Neighborhood-scale retail sales and service, office, civic or recreational uses conveniently located for neighborhood access, thereby contributing to the livability of the area by reducing the amount of vehicle miles traveled to reach goods and services;</u></p> <p><u>E. Coordinated architectural styles, landscaping building forms and building relationships which help establish a cohesive sense of place;</u></p> <p><u>F. An urban form that emphasizes the efficient use of land and compact urban form;</u></p> <p><u>G. The applicant with reasonable assurance of ultimate approval before requiring detailed design and engineering, while providing the City with assurances that the project will retain the character envisioned at the time of approval; and</u></p> <p><u>H. A basis for discretionary review of an overall plan of development that can subsequently be developed in phases over time with administrative approval.</u></p>	<p>be required for all development within the new NMU Zone.</p> <p>New purpose statement emphasizes pedestrian oriented mixed use.</p>
<p><u>10.4.305 PROCEDURES</u></p> <p><u>A Mixed Use Planned Development (MUPD) is reviewed through a two-step process.</u></p> <p><u>A. Preliminary plan. The preliminary plan is reviewed under Type III procedures. The preliminary plan review examines the MUPD with respect to items such as the mix of uses; density, including the number, type, and location of dwelling units; the location and amount of non-residential uses; parking; impacts on surrounding areas; adequacy of services; and conceptual plan for service improvements. Preliminary plan approval will only be granted when there is a reasonable certainty that the MUPD will fulfill all applicable requirements of the City Codes.</u></p> <p><u>B. Final plan. The final plan for the MUPD is reviewed under Type I administrative procedures. The applicant must submit the detailed and technical information necessary to demonstrate that all applicable City standards, requirements, and conditions have been met. Approval will only be granted if the final plan is in substantial conformance with the preliminary plan.</u></p> <p><u>C. Concurrent land division. A MUPD may be filed and processed concurrently with a partition or subdivision application; however, no land division shall be approved unless it is found to be consistent with the applicable MUPD. All of the submittal requirements and review standards of Article 6 will apply to a concurrent MUPD/land division request. The tentative plat will be combined with the preliminary MUPD review and the final plat will be combined with the final MUPD review.</u></p> <p><u>D. Site development/design review. The MUPD approval shall remove the requirement for subsequent site development or design review of individual buildings, if the MUPD includes building elevations and sufficient information to demonstrate compliance with the applicable site development/design review standards. The MUPD decision shall expressly state whether individual buildings within the MUPD (such as commercial or multifamily buildings) shall require subsequent site development or design review approval.</u></p>	<p>Proposed procedures are generally comparable to current PD requirements.</p>

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<p><u>10.4.310 PROFESSIONAL DESIGN TEAM REQUIRED</u></p> <p><u>For MUPDs over three (3) gross acres in size, the MUPD applicant must certify, in writing, that a member of each of the following professions will be used in the planning and design process for the proposed MUPD:</u></p> <p>A. <u>A licensed architect or professional urban designer approved by the Planning Director.</u></p> <p>B. <u>A licensed landscape architect, a certified nurseryman, or landscape designer approved by the Director.</u></p> <p>C. <u>A registered civil engineer or land surveyor.</u></p> <p><u>One of the above professionals shall be designated by the applicant to act as a liaison between the Community Development Department, the design team, and the applicant during the two-step MUPD review process. The Planning Commission or City Council may require the expertise of other professionals on the design team if it is determined that the site merits special consideration to unique or adverse features or conditions.</u></p>	<p>Proposed requirement is comparable to PD. As drafted, this requirement would only apply to MUPDs over 3 acres in size.</p>																
<p><u>10.4.315 MUPD DEVELOPMENT STANDARDS</u></p> <p>A. <u>Base Zone Standards. The development standards of the base zone apply unless they are superseded by the standards of this section or an MUPD approval.</u></p> <p>B. <u>Site Size. There are no minimum or maximum size limitations for a MUPD.</u></p> <p>C. <u>Residential Density.</u></p> <p><u>1. The target, minimum and maximum residential density for each of the NMU zoned area is shown in Table 4-1. Dwelling units may be transferred between parcels as part of an approved MUPD provided the average density within the MUPD meets the standards in Table 4-1.</u></p> <p style="text-align: center;"><u>TABLE 4-1</u> <u>Residential Density</u></p> <table border="1" data-bbox="118 1410 1203 1587"> <thead> <tr> <th><u>NMU Zoned Area</u></th> <th><u>Minimum Density</u></th> <th><u>Target Density</u></th> <th><u>Maximum Density</u></th> </tr> </thead> <tbody> <tr> <td><u>Area 1 - David Hill</u></td> <td><u>9.6 units/net acre</u></td> <td><u>12 units/net acre</u></td> <td><u>13.8 units/net acre</u></td> </tr> <tr> <td><u>Area 2 - Gales Creek</u></td> <td><u>6.97 units/net acre</u></td> <td><u>8.71 units/net acre</u></td> <td><u>10.02 units/net acre</u></td> </tr> <tr> <td><u>Area 3 - Davidson</u></td> <td><u>9.6 units/net acre</u></td> <td><u>12 units/net acre</u></td> <td><u>13.8 units/net acre</u></td> </tr> </tbody> </table> <p>D. <u>Lot Sizes. There are no required minimum lot sizes.</u></p> <p>E. <u>Housing Types Allowed. All housing types are permitted. For MUPDs over three (3) gross acres in size, a mix of housing types is required unless otherwise approved by the Planning Commission. Multi-unit buildings shall be subject to Multi-Unit Residential Focus Area standards unless standards are modified through an approved MUPD.</u></p> <p>F. <u>Village Center. Commercial Uses and Institutional Uses (other than Basic Utilities, Major Utilities Transmission Facilities, Daycare, Home Occupations and Community Recreation) shall be located within a</u></p>	<u>NMU Zoned Area</u>	<u>Minimum Density</u>	<u>Target Density</u>	<u>Maximum Density</u>	<u>Area 1 - David Hill</u>	<u>9.6 units/net acre</u>	<u>12 units/net acre</u>	<u>13.8 units/net acre</u>	<u>Area 2 - Gales Creek</u>	<u>6.97 units/net acre</u>	<u>8.71 units/net acre</u>	<u>10.02 units/net acre</u>	<u>Area 3 - Davidson</u>	<u>9.6 units/net acre</u>	<u>12 units/net acre</u>	<u>13.8 units/net acre</u>	<p>Base zone standards are included in the new MUPD zone.</p> <p>An MUPD would be required prior to development in NMU Zone. There are both large and small sites in the areas designated on the CP map for Mixed Use. Standards need to be scalable.</p> <p>The TCT standards and guidelines will apply to Village Centers and the MF standards and guidelines will apply to multi-unit developments.</p>
<u>NMU Zoned Area</u>	<u>Minimum Density</u>	<u>Target Density</u>	<u>Maximum Density</u>														
<u>Area 1 - David Hill</u>	<u>9.6 units/net acre</u>	<u>12 units/net acre</u>	<u>13.8 units/net acre</u>														
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<p><u>Village Center.</u></p> <p>1. <u>The applicant shall propose designs for the Village Center that provide services at a neighborhood scale within a compact, pedestrian-friendly environment. The Design and Development Standards and/or Design Guidelines applicable to the Town Center Transition (TCT) zone shall be used as a basis for development within the Village Center unless modified through an approved MUPD.</u></p> <p>2. <u>The minimum and maximum floor area of commercial or institutional uses permitted within the Village Center is shown in Table 4-2.</u></p> <p>3. <u>The total area of the Village Center shall not exceed 50% of the buildable land within the MUPD or three (3) acres, whichever is greater. For the purposes of this calculation the area of the Village Center shall include the contiguous areas dedicated to commercial and institutional uses and related parking. Streets and open space shall not be included in the Village Center area calculations.</u></p> <p>4. <u>Only one (1) Village Center is permitted within each of the NMU Zoned Areas.</u></p> <p>5. <u>Institutional uses shall not exceed 10% of the maximum allowed square footage within the Village Center.</u></p> <p>6. <u>Within Area 3 (Davidson), the maximum building footprint is 50,000 square feet. One tenant in Area 3 may occupy up to 50,000 square feet of gross floor area. Multiple tenants in a single building are allowed.</u></p>	<p>However, standards can be modified through the MUPD approval.</p> <p>F. In order to avoid scatter or strip development or multiple lots, the proposed language allows commercial and civic uses only in a Village Center and allows only one Village Center in each of the Areas. Also, in order to keep the Village Centers relatively compact, they are limited to a maximum size of 50% of the buildable land or 3 acres whichever is greater.</p>												
<p>TABLE 4-2 <u>Commercial/Institutional Uses within the Village Center</u></p>													
<table border="1"> <thead> <tr> <th data-bbox="185 1229 472 1293"><u>NMU Zoned Area</u></th> <th data-bbox="472 1229 841 1293"><u>Minimum Square Footage</u></th> <th data-bbox="841 1229 1247 1293"><u>Maximum Square Footage</u></th> </tr> </thead> <tbody> <tr> <td data-bbox="185 1293 472 1336">Area 1 - David Hill</td> <td data-bbox="472 1293 841 1336">None</td> <td data-bbox="841 1293 1247 1336">15,000 SF Gross Floor Area</td> </tr> <tr> <td data-bbox="185 1336 472 1378">Area 2 - Gales Creek</td> <td data-bbox="472 1336 841 1378">None</td> <td data-bbox="841 1336 1247 1378">25,000 SF Gross Floor Area</td> </tr> <tr> <td data-bbox="185 1378 472 1415">Area 3 - Davidson</td> <td data-bbox="472 1378 841 1415">25,000 SF Gross Floor Area</td> <td data-bbox="841 1378 1247 1415">130,000 SF Gross Floor Area</td> </tr> </tbody> </table>	<u>NMU Zoned Area</u>	<u>Minimum Square Footage</u>	<u>Maximum Square Footage</u>	Area 1 - David Hill	None	15,000 SF Gross Floor Area	Area 2 - Gales Creek	None	25,000 SF Gross Floor Area	Area 3 - Davidson	25,000 SF Gross Floor Area	130,000 SF Gross Floor Area	
<u>NMU Zoned Area</u>	<u>Minimum Square Footage</u>	<u>Maximum Square Footage</u>											
Area 1 - David Hill	None	15,000 SF Gross Floor Area											
Area 2 - Gales Creek	None	25,000 SF Gross Floor Area											
Area 3 - Davidson	25,000 SF Gross Floor Area	130,000 SF Gross Floor Area											
<p>G. <u>Height. The height limits of the base zone apply.</u></p>	<p>K. In order to ensure a pedestrian friendly environment, parking must be located to the rear or side of non-residential buildings and residential developments are required to have alley</p>												
<p>H. <u>Building Setbacks. Building setbacks are established as part of the preliminary development plans approval.</u></p>													
<p>I. <u>Open Space. A MUPD shall include a minimum of 20% usable common open space, landscaped areas, and/or protected natural areas.</u></p>													
<p>J. <u>Pedestrian-Orientation and Circulation. A MUPD shall include a transportation system that emphasizes pedestrian mobility and accessibility, and demonstrates an effective and convenient system of pedestrian facilities that provides connectivity throughout the MUPD and to adjacent properties. The transportation system shall identify existing and proposed pedestrian connections and may include a combination of sidewalks, multi-use pathways and trails.</u></p>													

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<p><u>K. Parking. The base zone parking requirements apply except that shared parking is encouraged within the MUPD. Where the MUPD abuts land zoned for single family residential development, common parking and maneuvering areas must be set back at least twenty (20) feet from the lot line. No parking or vehicular circulation areas shall be located between the building and the street. Garages and off-street parking areas for residential developments shall be accessed by alleys unless otherwise approved by the Planning Commission</u></p> <p><u>L. Water Features. Water features such as streams or ponds must be left in a natural state unless altered to improve the natural values of the water feature or to improve stormwater drainage. Water features and their buffers should be kept in common ownership.</u></p> <p><u>M. Facilities and Services. It is the responsibility of the applicant to provide all service facilities necessary for the functioning of the MUPD. Service facilities such as streets, water supply facilities, sanitary sewers, and storm water detention facilities must be dedicated to the public if they are to provide service to any property not included in the MUPD. However, the review body may approve private service facilities with the consent of the appropriate service provider.</u></p> <p><u>N. Underground Utilities. All service facilities must be placed underground except those that by their nature must be on or above ground, such as fire hydrants and open water courses. The applicant is responsible for making the necessary arrangements with utility companies and other appropriate entities when installing all service facilities.</u></p> <p><u>O. Construction to Standards. All service facilities dedicated to the public must be constructed to City standards. All private service facilities must be designed by a qualified civil engineer to City standards or comparable design life as determined by the City Engineer.</u></p>	<p>access</p>
<p><u>10.4.320 PRELIMINARY PLAN REVIEW</u></p> <p><u>A. Procedure. Preliminary plan reviews are processed through a Type III procedure.</u></p> <p><u>B. Submittal Requirements. Applications for a preliminary plan review must contain the information stated below in addition to that required by Section 10.1.325.</u></p> <p><u>1. General statement. A statement of how the purpose of Section 10.4.300 will be achieved by the proposed MUPD. The statement should include sketches or illustrations of the proposed character of the development, a description of how the MUPD will relate to surrounding land uses and whether other land use reviews are requested.</u></p> <p><u>2. Summary report. A summary report identifying the different land uses, including the amount of land for housing, Village Center (if any) and other non-residential uses, open areas, streets and parking; the number and type of housing units; and a statement of how necessary services will be provided and whether the services will be publicly or privately owned and operated.</u></p> <p><u>3. Drawings of existing site conditions. A drawing or drawings must be submitted which display and inventory existing site conditions including the items listed below.</u></p> <p><u>a. Ground elevations shown with contour lines at two (2) -foot intervals or less.</u></p> <p><u>b. Areas of moderate or severe landslide potential, as identified on City maps or documented by an engineering geologist or geotechnical engineer.</u></p> <p><u>c. General soil types as identified on City maps or as documented by an engineering</u></p>	<p>Proposed requirements are generally comparable to current PD requirements.</p>

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<p><u>geologist of soils engineer.</u></p> <p>d. <u>Existing natural features, including rock outcroppings, trees and tree groves, fish and wildlife habitats, ponds, wetlands, and watercourses.</u></p> <p>e. <u>Existing on-site or abutting sanitary sewage, storm drainage, and water supply facilities. If such facilities are not on or abutting the site, indicate the direction and distance to the nearest ones.</u></p> <p>f. <u>Width, location, and purpose of all existing easements of record on or abutting the site.</u></p> <p>g. <u>A description of the traffic circulation system for all modes on or abutting the site, including street sizes, level of improvements, and condition of the streets.</u></p> <p>h. <u>A description of areas abutting the MUPD, indicating zoning districts, land uses, densities, circulation systems, public service facilities, natural features, and approximate locations of nearby structures.</u></p> <p>i. <u>Any additional information about existing site conditions required for a concurrent subdivision application.</u></p> <p>4. <u>MUPD Site Plan. The site plan must include the information stated below.</u></p> <p>a. <u>Setbacks and orientation to the street all buildings.</u></p> <p>b. <u>The traffic circulation system for all modes, including connections to existing public rights-of-way, off-street parking, and the ownership of streets, street trees and street lighting.</u></p> <p>c. <u>The general location of parking areas, identify ingress and egress locations, and the number of spaces to be provided.</u></p> <p>d. <u>Conceptual plans for pedestrian and bicycle circulation systems.</u></p> <p>e. <u>Conceptual plans for all necessary services, including their location and whether the services will be publicly or privately owned and maintained.</u></p> <p>f. <u>Conceptual plans for all facilities for the control and disposal of storm water and groundwater.</u></p> <p>g. <u>Conceptual plans for the location and design of public and private open areas or structures.</u></p> <p>h. <u>Treatment proposed for the periphery of the site, including the approximate amount, location, and type of any required landscaping.</u></p> <p>5. <u>Drawings. Drawings showing the existing site conditions and the proposed site plan must be at a reasonable size and scale to clearly show all required information. The drawings must display the following:</u></p>	

City of Forest Grove Development Code – Draft Amendments	Commentary
<p>a. <u>Name of the proposed PD;</u></p> <p>b. <u>Date, north arrow, and scale of the drawing;</u></p> <p>c. <u>Legal description of the PD sufficient to define its location and boundaries;</u></p> <p>d. <u>Names, addresses, and telephone numbers of the owner, applicant, and design team;</u></p> <p>e. <u>Appropriate identification of the drawing as a preliminary plan.</u></p> <p>6. <u>Building Elevations or Design Guidebook. The application shall include sample elevations for detached single family dwellings and specific elevations for all other buildings. In lieu of providing detailed elevations for each building, the application may include a Design Guidebook, created by a qualified architect or urban designer, which outlines the design standards that will be applicable to all development within the MUPD including architectural features for all building types and identify proposed color palates, materials and typical elevations.</u></p> <p>7. <u>Phased MUPDs. If the project will be built in phases, a Development Phasing Plan is required. The Development Phasing Plan shall:</u></p> <p>a. <u>Demonstrate how the required mix of uses will be provided through phasing, including the approximate locations, amount in square footage of non-residential uses (a size range may be provided), number of residential dwelling units, and phasing schedule of each use;</u></p> <p>b. <u>Demonstrate how on-site circulation, parking, landscaping and tree planting, lighting and other on-site improvements will function, after the completion of each phase and following complete buildout of the development site;</u></p> <p>c. <u>Identify the timeframe, in what order and how proposed public utilities, public facilities and other improvements and amenities necessary to support the project will be constructed, dedicated or reserved; and</u></p> <p>C. <u>Approval Criteria. The preliminary plan will be approved if the review body finds that the applicant has shown that all of the following approval criteria are met:</u></p> <p>1. <u>The plan fulfills the purpose for MUPDs stated in Section 10.4.300;</u></p> <p>2. <u>The plan meets the submittal requirements of Section 10.4.320 B;</u></p> <p>3. <u>Adequate public services exist or can be provided to serve the proposed MUPD; and</u></p> <p>4. <u>Where a tentative subdivision plat is requested, the requirements of Article 8 are met.</u></p> <p>D. <u>Time Limit. Preliminary plan approval is valid for three (3) years and may not be extended. The three (3)-year period will not begin until any appeals beyond the jurisdiction of the City are completed. Within the three (3) year time period, the applicant must submit a final development plan for the entire site, or for the first phase if the MUPD has been approved for phased development. The applicant must submit final development plans for any subsequent phases within the time limit specified for the phases.</u></p>	<p>6. The allowance for a Design Guidebook is unique to this MUPD process. It is intended to allow developers who are doing phased developments to delay preparing all of the building elevations.</p>
<p>10.4.325 <u>FINAL PLAN REVIEW</u></p>	<p>Proposed requirements</p>

City of Forest Grove Development Code – Draft Amendments	Commentary
<p><u>A. Final Plan Submittal. The applicant must present detailed plans, which meet the public facility standards of the City. The applicant must present other plans or studies required by the preliminary approval, such as a grading plan, soils engineer report, or detailed landscaping plans.</u></p> <p><u>B. Procedure. The final plan is reviewed under Type I procedures.</u></p> <p><u>C. Requirements. The final development plan will be approved if it meets the requirements stated below and is in substantial conformance with the approved preliminary plan and any conditions of the approval.</u></p> <p><u>1. Drawing quality. The final development plan must be drawn clearly and legibly at a size and scale that clearly shows all required information. The plan must be identified as the final MUPD plan.</u></p> <p><u>2. Additional information on the final plan. In addition to the information required on preliminary drawings or otherwise specified by law, the following information must be shown:</u></p> <p><u>a. Reference points of identified existing surveys by distances and bearings, and referenced to field book or map, including stakes, monuments, or other evidence found on the ground and used to determine the boundaries of the MUPD;</u></p> <p><u>b. The location and width of streets and easements intercepting the boundary of the MUPD;</u></p> <p><u>c. Easements and stormwater drainage reserves must be clearly identified and, if already of record, their recorded reference. The width of the easement, its length and bearing, and sufficient ties to locate the easement within the MUPD must be shown. If the easement is being dedicated by the plan, it must be properly referenced in the owner’s certificates of dedication; and</u></p> <p><u>d. Identification of land to be dedicated to the public.</u></p> <p><u>3. Additional certificates or drawings. The items stated below may be combined where appropriate.</u></p> <p><u>a. A certificate signed and acknowledged by all parties having any recorded title interest in the land and consenting to the preparation and recording of the MUPD.</u></p> <p><u>b. A certificate signed and acknowledged as above, dedicating the land intended for public use, if any.</u></p> <p><u>c. A title report issued by the title insurance company verifying ownership by the applicant of real property that is to be dedicated to the public.</u></p> <p><u>d. A certificate with the seal of, and signed by, the surveyor responsible for the survey.</u></p> <p><u>e. Other certificates required by law.</u></p> <p><u>f. A copy of any deed restrictions applicable to the MUPD.</u></p> <p><u>4. Detailed design plan for the MUPD site. A detailed design plan for the MUPD is required</u></p>	<p>are generally comparable to current PD requirements.</p>

City of Forest Grove Development Code – Draft Amendments	Commentary
<p><u>and must include the items stated below.</u></p> <p>a. <u>The location of proposed buildings and structures, parking areas and, where applicable, the location of allowable building areas of individual lots.</u></p> <p>b. <u>All building setback lines and height limits that are to be made part of the MUPD restrictions.</u></p> <p>c. <u>The location and type of proposed buildings, structures, or improvements in common open areas.</u></p> <p>d. <u>The location and design information for all proposed streets as required by Article 8.</u></p> <p>e. <u>A plan for water mains and fire hydrants.</u></p> <p>f. <u>A plan for sanitary sewage disposal.</u></p> <p>g. <u>A plan for storm water drainage.</u></p> <p>h. <u>A plan for additional improvements such as walkways and street lighting.</u></p> <p>i. <u>Required solar-related information if the MUPD is also subject to the solar regulations for new subdivisions.</u></p> <p>5. <u>Landscaping. A landscaping plan for common open areas, the perimeter of the MUPD, and other landscaped areas is required.</u></p> <p>a. <u>The plan must show areas that the applicant proposes to retain in natural vegetation. The plan must show the areas, sizes, numbers, and types of plant and other materials to be used for all landscaped areas.</u></p> <p>b. <u>The plan must address the revegetation of common open areas and perimeter areas disturbed during construction.</u></p> <p>c. <u>The plan must include a proposed schedule for required perimeter landscaping. A performance guarantee is required if the landscaping cannot be completed prior to the occupancy of buildings, or cannot be completed when required by the conditions of approval.</u></p> <p>6. <u>Geotechnical engineer’s report. A geotechnical engineer’s report consistent with the requirements of Section 10.8.310 must be submitted if the MUPD is in a moderate or severe landslide area, or if the report was required as a part of the preliminary approval. The City Engineer or the Building Official must approve the report.</u></p> <p>7. <u>CC & Rs. The Declaration of Covenants, Conditions, and Restrictions (CC & Rs) for the MUPD must be submitted. In addition, any other legal instruments for the protection and maintenance of common open areas, private streets, and private utilities if any, must be submitted. These legal instruments must be approved by the City Attorney to ensure that the City’s interests are protected.</u></p>	

City of Forest Grove Development Code – Draft Amendments	Commentary																																									
8. <u>Concurrent subdivision approval. Simultaneous final plat approval is permitted.</u>																																										
ARTICLE 5 SPECIAL PROVISIONS	No changes																																									
ARTICLE 6 LAND DIVISIONS	No changes																																									
ARTICLE 7 MISCELLANEOUS PROVISIONS																																										
<p>10.7.000 OVERVIEW OF ARTICLE 7 This Article includes miscellaneous provisions for special uses or exceptions that are not addressed elsewhere in the Code. The following list summarizes topics covered in this Article:</p> <ul style="list-style-type: none"> • Accessory Dwelling Units • Accessory Structures • Bed & Breakfast Inn • Fences • General Exceptions • Home Occupations • Manufactured Homes on Lots • Nonconforming Development • Solid Waste & Recycling Storage • Wireless Communication Facilities • <u>Residential Density Incentives</u> <p>These headings can assist the user in locating information. The table of contents contains a complete list of the material included in this Article.</p>																																										
<p>WIRELESS COMMUNICATION FACILITIES</p> <p>10.7.310 PROCEDURE Table 6-1 describes the type of wireless communication facility and review procedure by zone. The placement, construction, and/or modification of wireless communication facilities are subject to review and approval as indicated below. The two options for review are a Type I administrative decision or a Type III conditional use permit. NP indicates that the type of antenna or support structure is not permitted in that zone at that height.</p> <p style="text-align: center;">TABLE 7-1: WIRELESS FACILITY TYPES AND REVIEW PROCEDURES</p> <table border="1" data-bbox="61 1655 1344 1938"> <thead> <tr> <th rowspan="2">ANTENNA OR SUPPORT STRUCTURE</th> <th colspan="6">ZONE</th> </tr> <tr> <th>R</th> <th>Inst</th> <th>C</th> <th>TC</th> <th>I</th> <th><u>NMU</u></th> </tr> </thead> <tbody> <tr> <td>Antenna attached to existing structure (i.e., buildings, towers, grain elevators, or other structures)</td> <td>AA</td> <td>AA</td> <td>AA</td> <td>AA</td> <td>AA</td> <td><u>AA</u></td> </tr> <tr> <td>Monopoles – maximum height of 35 feet</td> <td>CU</td> <td>AA</td> <td>AA</td> <td>CU</td> <td>AA</td> <td><u>AA</u></td> </tr> <tr> <td>Monopoles – 36 feet to 70 feet in height</td> <td>NP</td> <td>NP</td> <td>CU</td> <td>CU</td> <td>AA</td> <td><u>NP</u></td> </tr> <tr> <td>Monopoles – greater than 70 feet</td> <td>NP</td> <td>NP</td> <td>CU</td> <td>NP</td> <td>AA</td> <td><u>NP</u></td> </tr> </tbody> </table>	ANTENNA OR SUPPORT STRUCTURE	ZONE						R	Inst	C	TC	I	<u>NMU</u>	Antenna attached to existing structure (i.e., buildings, towers, grain elevators, or other structures)	AA	AA	AA	AA	AA	<u>AA</u>	Monopoles – maximum height of 35 feet	CU	AA	AA	CU	AA	<u>AA</u>	Monopoles – 36 feet to 70 feet in height	NP	NP	CU	CU	AA	<u>NP</u>	Monopoles – greater than 70 feet	NP	NP	CU	NP	AA	<u>NP</u>	The new NMU zone has been added to the Table 7-1. NMU is subject to the same standards as Residential.
ANTENNA OR SUPPORT STRUCTURE		ZONE																																								
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City of Forest Grove Development Code – Draft Amendments Commentary

Footnotes:

R: Residential Zones
 C: Commercial Zones (except for NMU)
 TC: Town Center Zones
 I: Industrial Zones
 Inst: Institutional Zones
 NMU: Neighborhood Mixed Use Zones

AA: Administrative Action – Type I Process
 CU: Conditional Use Permit – Type III Process
 NP: Not Permitted

RESIDENTIAL DENSITY INCENTIVE ABOVE BASE REQUIREMENT

10.7.400 PURPOSE

The residential density incentive is intended to allow significantly higher densities within the Town Center, while ensuring that livability is preserved and sustainability is encouraged. Projects which receive density incentives are expected to exceed the City’s design standards.

10.7.405 APPLICABILITY

For proposed mixed use and multifamily residential developments within the Town Center, the Planning Commission may consider the request for a density incentive as part of Design Review at the applicant’s request.

10.7.410 STANDARDS

Available density incentives are described in Table 7-2. However, in no case shall the total density on a site exceed 100 units per net acre. In addition, the City may limit the total number and/or type of density incentives for which the property may qualify based on the following:

- A. Availability of adequate public facilities and services, including public water, sanitary sewer, storm drainage, police and fire protections
- B. Traffic impacts. Applicants may be required to provide a traffic impact analysis, prepared by a licensed traffic engineer, which includes appropriate mitigation measures.
- C. Compatibility with adjacent single-family residential zonings districts.

New density incentive (bonus) section. As written would only apply to Town Center zones, but in the future the City could consider applying to Community Commercial (CC) zone or Residential Multifamily.

Maximum density permitted with incentives is 100 units/net acre.

**TABLE 7-2
 DENSITY INCENTIVES**

<u>Tier 1: Density Incentives for Project Amenities</u>		
<u>Tier 1 density incentives are earned when a developer provides the project amenities listed in Table 7-3 to earn the number of points required for the density increases shown in this table.</u>	<u>Minimum Points Required</u>	<u>Density Increase</u>
	11 points	10 units/acre

City of Forest Grove Development Code – Draft Amendments			Commentary
<p><u>In order to qualify for a density incentive the proposed improvements must provide an amenity which is not otherwise required by the Development Code. Where a qualifying amenity requires that a minimum amount of area be provided, the same square footage may not be counted towards other amenities. The property owner may be required to execute a covenant with the City ensuring continuation and maintenance of the qualifying amenity by the property owner. Projects cannot qualify for multiple point values in the same amenity category.</u></p>	14 points	20 units/acre	<p>Maximum density of 100 du/net acre within the TC zones. The base density in the Town Center is proposed to be increased to 40 du/ac so the bonus would allow an increase of up to 60 additional du/ac.</p>
	19 points	30 units/acre	
	25 points	40 units/acre	
	30 points	60 units/acre	
<p><u>Tier 2: Density Incentives for Affordable Housing</u></p> <p><u>Tier 2 density incentives are earned when 20% of units are set aside for renters or purchasers earning no more than 80% of median income and paying no more than 30% of total household income in rent or mortgage.</u></p> <p><u>Such units shall be developed by a developer with experience undertaking market and non-profit low-income housing developments. This density bonus does not apply to institutional housing</u></p>			

**TABLE 7-3
AMENITY CATEGORIES AND VALUES**

<u>Amenity Category</u>	<u>Value</u>	<u>Description</u>
<u>Bicycle Amenities</u>	<u>1 point</u>	<u>Provide residents with enhanced bicycle amenities (parking and repair area). The area dedicated to long-term bicycle parking shall be sized to accommodate 0.5 bicycles per unit and must be covered and secure. The area dedicated to bicycle repair shall be a minimum of 50 square feet in area and designed and equipped to facilitate bicycle maintenance.</u>
<u>Energy Efficiency</u>	<u>2 points</u>	<u>Energy efficiency improvements compliant with Energy Trust of Oregon. Projects must enroll in the Energy Trust’s Design Assistance program during schematic design or earlier and commit to exceeding Oregon code requirements by a minimum of five percent.</u>
	<u>2 points</u>	<u>Achieve an Energy Star score of 7 or better, as calculated by the EPA online tool.</u>
	<u>3 points</u>	<u>Project designed to Net Zero Energy Certification.</u>
<u>“Green” Materials</u>	<u>2 points</u>	<u>Use environmentally sensitive (“Green”) materials on at least 50% of the building’s interior surfaces including:</u> <ul style="list-style-type: none"> • <u>Wall and Ceiling Latex Paint: Low-VOC</u> • <u>Paint with recycled content: minimum 50% post-consumer</u> • <u>Countertop with recycled content: 25% Post-consumer content</u> • <u>Carpet with post-consumer recycled content (+50%)</u> • <u>Renewable flooring materials: Linoleum, Cork, Bamboo, or Wool</u> <ul style="list-style-type: none"> • <u>Forest Stewardship Council certified wood Reclaimed Wood</u>
	<u>3 points</u>	<u>Use of “Green Material” on at least 75% of the building’s interior.</u>
<u>Low Impact Design</u>	<u>1 point</u>	<u>Manage all stormwater from the site using low impact design techniques from <i>Clean Water Services Low Impact Development Handbook for the</i></u>

Table 7-3 identifies the type and amount of each improvement required to qualify for points.

City of Forest Grove Development Code – Draft Amendments			Commentary
		<u>Tualatin Basin</u>	
<u>Ground Floor Retail</u>	<u>3 points</u>	<u>Provide at least 5,000 square feet of retail or space which is designed to be convertible to ground level retail. A density bonus for this amenity is available in the TCT zone only.</u>	
<u>Residential Gardening</u>	<u>2 points</u>	<u>Provide a community garden for use by residents. The garden must include raised beds (minimum size 3' by 5') with improved soil and a water source for irrigation. The garden may be at grade or rooftop, but must be located in an area with adequate sunlight (minimum 6 hours/day). The area dedicated to the community garden shall be sized to accommodate 0.3 raised beds per unit plus walkways.</u>	
<u>Rooftop Garden or Eco-Roof</u>	<u>2 points</u>	<u>Provide a rooftop garden or an eco-roof. The rooftop garden or eco-roof must cover at least 50 percent of the roof area of the building. Rooftop gardens must be accessible to residents and at least 30 percent of the garden area must contain plants. The remaining area must include seating areas and other amenities. Eco-roofs must be designed to reduce stormwater and be entirely covered with vegetation.</u>	
<u>Public Plaza/Outdoor Patio/Seating Area</u>	<u>2 points</u>	<u>Where ground floor retail is planned, provide a patio area for outdoor seating between the retail and the primary public street frontage which is designed to activate the street frontage. The patio and seating area must extend along at least 20 feet of the frontage and be at least 10 feet deep.</u>	
	<u>3 points</u>	<u>Provide a public plaza with a minimum area of 2,000 square feet. Seating areas and landscape plantings are required amenities. Water features are encouraged. To promote a sense of openness and safety, public plazas shall be completely visible from an adjacent street frontage.</u>	
<u>Structured Parking</u>	<u>10 points</u>	<u>At least 80% of the required parking is provided in a parking structure. Structures may be at- or below grade.</u>	
<u>LEED Certification</u>	<u>Silver: 8 points</u> <u>Gold: 10 points</u> <u>Platinum: 12 points</u>	<u>Project designed to achieve LEED Silver, Gold, or Platinum certification. Project features required to qualify for LEED certification cannot be used to qualify for points in other amenity categories.</u>	
<u>Other</u>	<u>TBD</u>	<u>Other amenity approved by Planning Commission</u>	

ARTICLE 8 GENERAL DEVELOPMENT STANDARDS

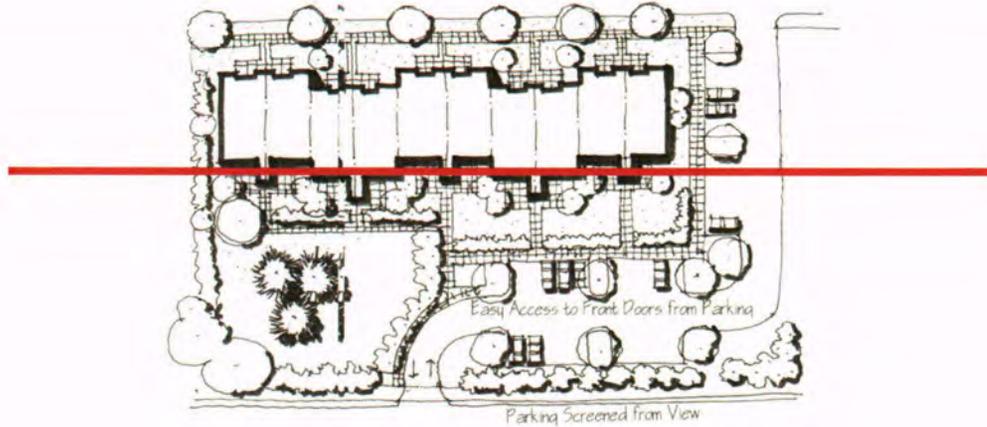
ACCESS AND CIRCULATION

10.8.140 SPECIFIC SITE ACCESS AND CIRCULATION STANDARDS

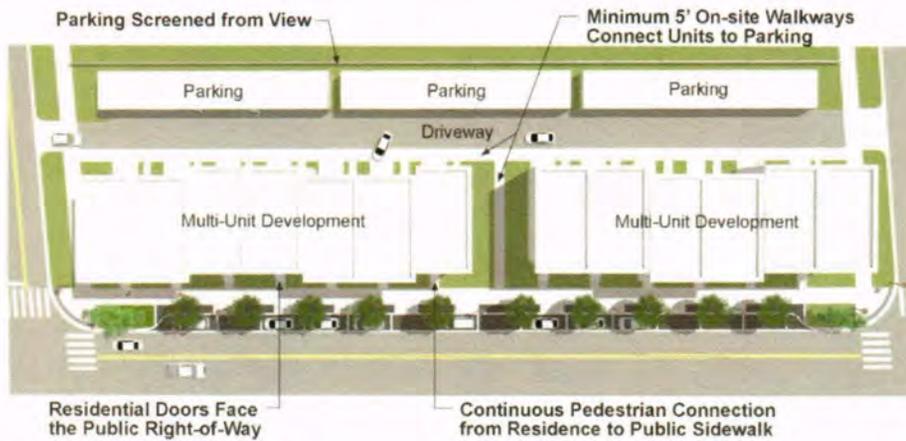
The following access and circulation standards apply specifically to certain types of development or apply within certain locations within the community.

- A. Multi-Unit Development. A functional and safe environment for vehicles and pedestrians is important within residential areas. Multi-unit development should be visual appealing from the adjacent street right-of-way and be functionally and aesthetically tied to adjacent residential neighborhoods.
 - 1. Circulation
 - a. A continuous pedestrian connection shall be provided from the front door of all residential buildings to the public sidewalk.
 - b. Separate pathways from dwellings a minimum distance of ten (10) feet. The separation is measured from the pathway edge closest to any dwelling unit.
 - c. On-site pedestrian walkways are required to be a minimum width of five (5) feet connecting dwelling units to parking/open space/recreation areas.
 - 2. Parking Areas
 - a. Separate physically and visually parking areas greater than 10,000 square feet in area with landscape planter bays at least eighteen feet in width. Individual parking areas may be connected by a driveway.
 - b. The sidewalk system shall connect the front door of all residential buildings to parking area(s).
 - 3. Pedestrian Environment
 - a. Residential doors which face a public right-of-way shall be setback a minimum of ~~2 feet~~ 3 feet plus any additional width needed to meet the minimum sidewalk requirements (where applicable) from a public sidewalk.
 - b. The following types of doors shall not provide the primary entrance into a dwelling unit: sliding glass or solid metal doors without glazing.
 - c. Residential stoops, porches or terraces shall be raised a maximum of 3 feet above grade at the adjacent right-of-way.
 - d. Residences with entry porches or terraces shall have a minimum area of ~~60~~ 40 square feet.
 - e. Residential entries shall be lowered a maximum of 4 feet below grade measured at adjacent right-of-way.

Figure 8-2: Example Site Layout for Multi-Unit Development



Existing Figure 8-2 to be deleted and replaced.



Updated Figure 8-2: Example Site Layout for Multi-Unit Development

Figure 8-3: Pedestrian Environment for Multi-Unit Development



A new Figure 8-3 has been added illustrating setback requirements

<p>4. Screening/Service Areas</p> <ul style="list-style-type: none"> a. All mechanical equipment shall be screened from view by an opaque fence or solid landscape screen 8 feet high. b. Service/refuse collection areas shall not be located within 20 feet of public rights-of-way. c. All development of two or more stories in height shall be required to provide screening of garbage collection and storage areas from above. d. All roof mounted mechanical equipment other than vents shall be screened from ground level view. The screening shall be as high as the height of the equipment and shall be integrated with the exterior building façade <p>5. Landscaping</p> <ul style="list-style-type: none"> a. At least 75% of required landscaped area shall be planted with any suitable combination of trees, shrubs, or evergreen ground cover. b. A maximum of 25% of required landscaped area may include architectural features or artificial ground covers such as sculptures, benches, masonry or stone walls, rock groupings, bark dust, decorative hard paving and gravel areas interspersed with planting areas. <p>6. Crime Prevention Through Environmental Design (CPTED)</p> <ul style="list-style-type: none"> a. No landscaping, fences, walls or plant materials greater than 5 feet in height shall be installed which obstruct the visibility of a dwelling entry from a public street. b. A minimum of 25 square feet of window area shall be provided within each dwelling unit facing a common open space, pedestrian path or parking lot to allow visual surveillance. c. A clear line of site between all front building entries and the public sidewalk or parking areas shall be maintained. d. Lighting shall be provided at all exterior dwelling unit entrances. e. Guard or a keyed variation of gated residential development is prohibited. f. Motion activated security lighting is prohibited along public rights-of- way. <p>B. Commercial Development. Commercial development should accommodate automobiles, mass transit, bicycles and pedestrians in a safe, functional and visually appealing manner.</p> <p>1. Circulation</p> <ul style="list-style-type: none"> a. Auto/truck access to parking shall be from a local or collector street, an adjacent development, alley, or existing driveways along arterial roads. b. Where buildings are setback from right-of-ways, a continuous and direct sidewalk with a 12-foot minimum width shall be provided between the public sidewalk and the front door to all buildings on site. c. All roadways or drives shall include sidewalks on both sides of travel lanes with the exception of vehicle facilities that provide access to loading and service areas. <p>2. Parking</p>	<p>b. Bark dust is proposed to be deleted as a possible landscaped cover.</p>
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- a. Construct off-street surface parking that does not occupy more than 50% of the street frontage. Where a site has frontage along a side street a surface parking lot may occupy more than 50% of the side street frontage.
- b. Screen parking with solid perimeter wall, decorative welded metal (wrought iron) fencing and columns, earthen berm or evergreen hedge (or a combination of) at a minimum spacing of 3 feet and a maximum of 5 feet in height. The maximum height of screening shall be five (5) feet except along the street frontage where the maximum height shall be three (3) feet six (6) inches.
- c. Provide a minimum of 15% of parking lot as water pervious surface – either paving or landscaped retention areas.
- d. Development sites shall not consist of paved parking areas of more than 50% of the entire site area.

2b. Amendments intended to clarify current code language.

Figure 8-4: Parking for Commercial Development



A new Figure 8-4 has been added to illustrate parking for commercial development.

3. Pedestrian Environment

- a. The sidewalk system shall connect all customer entrances to the public sidewalk.
- b. Minimum sidewalk width 10 feet. Sidewalks shall meet American with Disabilities Act (ADA) requirements and may include landscaped areas, benches and other pedestrian amenities.
- c. Plazas, squares or courts shall be fronted by commercial building facades with 75% or more transparency/window opening of first 15 feet of building.
- d. All development shall provide a sheltered entry area at the entrance to each commercial establishment with a minimum area of fifty (50) square feet and a minimum dimension of six (6) feet.

4. Screening of Service Areas

- a. All loading areas shall be physically separated from customer pedestrian and residential areas with a solid 5-foot tall fence, evergreen hedge, arbor, berm or a combination of these elements.
- b. All ground level and roof-top mechanical equipment shall be screened from view with landscaping, fencing and or walls. The height of the screen shall equal or exceed the height of the equipment, and include screening from views from above

when visible from adjacent inhabited buildings.

5. Landscaping

- a. Landscaping standards vary depending on zoning, use, adjacent zoning, and existing site conditions.
- b. A landscaped strip at least ten (10) feet in width shall be provided abutting any property line facing a street. The landscape strip shall be appropriately landscaped with ground cover, planted berm, shrubbery and/or trees.
- c. At least 75% of required landscaped area shall be planted with any suitable combination of trees, shrubs, or evergreen ground cover.
- d. A maximum of 25% of required landscaped area may include architectural features or artificial ground covers such as sculptures, benches, masonry or stone walls, rock groupings, ~~bark dust~~, decorative hard paving and gravel areas interspersed with planting areas.

d. Bark dust is proposed to be deleted as a possible landscaped cover.

C. Town Center Districts. The intent of the following standards is to provide a functional and safe environment for vehicles and pedestrians, where the needs of pedestrian are emphasized. The Town Center should have a clear separation between pedestrian and vehicular areas and should be comfortable and safe in all seasons and hours of the day.

1. Building Orientation

- a. TCC: Building facades shall be built along at least 80% of the primary building frontage line.
- b. TCT: Building facades shall be built along at least 75% of the primary building frontage line.
- ~~e. TCS: Building facades shall be built along at least 50% of the primary building frontage line.~~
- d. One (1) entrance shall be provided for each building façade. When building is located on a corner, one entrance at the corner is permitted.
- e. If a building is setback from right-of-way, it shall be a minimum of four feet (4') in depth and include plazas, forecourts or other paved areas for public seating, artwork and landscaped planting beds.
- f. If a building is setback from the right-of-way, mechanical units, meters or other above grade or wall mounted utilities shall not be located in the front setback area.

Deleted TCS building façade frontage requirement in 10.8.140.C(1)(c)

2. Pedestrian Connections

- a. When provided on-site, parking shall not be located between the front façade and the front property line.
- b. Where walkways are parallel and adjacent to a driveway or street (public or private), they shall be raised 6 inches and curbed or separated from the driveway/street with bollards and/or a 3-foot minimum landscape barrier.
- c. Where pathways cross a parking area, driveway or street ("crosswalk"), they shall be clearly marked with contrasting paving materials, humps/raised crossings, or painted striping. If painted striping is used, it shall consist of thermo-plastic striping or similar type of durable application.
- d. All mechanical equipment, outdoor storage, and manufacturing and service and delivery areas shall be screened from view from pedestrian sidewalks. Screening

shall be provided by one or more of the following: decorative wall (masonry or similar quality material, evergreen hedge, wood fence - non-see through).

3. Site Amenities. Every development shall provide at least one of the “pedestrian amenities” listed below:
 - a. A plaza, courtyard next to the building entrance a minimum of 4 feet in depth for residential food, beverage or entertainment establishments only or, for other uses, an area with public art which incorporates seating (e.g. a fountain).
 - b. Public seating areas (chairs, tables, benches)
 - c. Building canopy, awning or similar weather protection across 75% of façade projecting a minimum of four feet over the sidewalk.

CLEAR VISION AREA

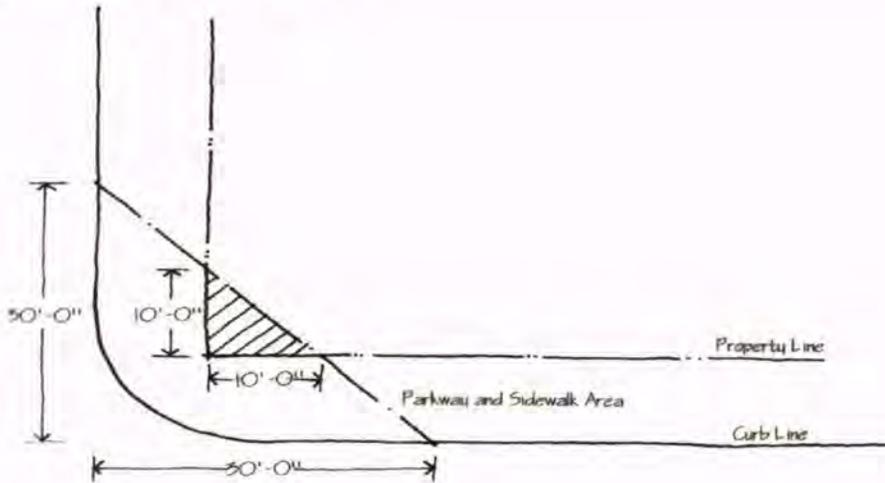
10.8.155 STANDARDS

Except in the Town Center zones ~~Core zone~~, a clear vision area shall be maintained on the corners of all property adjacent to the intersection of two streets, a street and a railroad, or a driveway providing vehicular access to a public street, excluding alleys.

- A. On Corner Lots, no vehicle, fence, wall, hedge or other planting or structure shall be parked, erected, planted, placed, located or maintained, except for occasional tree trunks or poles, so as to impede visibility within the greater of the two clear vision areas defined below (see Figure 8-3):
 1. The triangular area formed by the curb lines along such lots and a straight line joining said curb lines at points which are 30 feet distant from the intersection of the curb lines and measured along such lines; or
 2. The triangular area formed by the property lines of such lots and a straight line joining said property lines at points which are 10 feet distant from the intersection of the property lines and measured along such lines.
- B. A Private Access shall be treated as a public street for the purpose of this section. The edge of the paved surface area of the private access, be it roadway, curb or sidewalk, shall be treated as the curb line in determining the vision clearance area.
- C. The vision clearance area for all commercial, industrial, institutional, and multi-family uses shall be determined in the manner set forth in subsection (A)(1) above.
- D. The vision clearance area for single-family homes and duplexes shall be determined in the manner set forth in subsection (A)(2) above.
- E. Any obstruction maintained in violation of this section shall be deemed in violation of the Development Code and shall be abated pursuant to Section 10.1.400 enforcement procedures.

The TCT zone requires building facades be built along at least 75% of the primary building frontage line. This may be difficult to accomplish with the clear vision triangle. This section was amended so that the exemption applies to all TC zones.

Figure 8-3 5: Clear Vision Area



Updated Figure numbering. No graphic change.

OPEN SPACE, RECREATIONAL FACILITIES AND COMMON AREAS

10.8.205 STANDARDS

- A. Open space areas may be required to avoid placing structures or other improvements either in identified hazard or resource areas as required by Section 10.5.005 et. seq. and Section 10.8.300 et. seq.
- B. Areas retained to comply with Clean Water Services (CWS) water quality sensitive area and vegetative corridor requirements of the Design and Construction Standards or surface water quality or quantity facilities requiring over 100 square feet of area shall be considered open space but not be considered a recreational area unless so designed as to be integrated with a development's site design and available for access for residents in the development.
- C. For land divisions:
 - 1. Each open space and recreational facility shall be placed in separate tracts.
 - 2. Having a net density of at least 9.60 units per acre, 20% of the entire site in open space with at least one recreational tract having minimum dimension of sufficient size to accommodate play equipment targeted for preschool and elementary aged children plus table(s) and bench(es) for passive recreation.
- D. Except for developments in TCC or TCT zones, any For development not involved in a division of land, open space and recreation areas shall be held in common for residential condominiums or by the primary land owner for apartment complexes or non-residential development. The area shall be placed within an easement unless waived by the Director for minor recreation facilities or recreation facilities that are integrated with the developed portion of the project. Developments within the NMU zone may modify the standards below through an approved MUPD.
 - 1. Residential projects in the Residential Multifamily Low (RML) and Residential Multifamily High (RMH) districts shall provide the following: All condominium, two-

C2 may affect developments within NMU zone; however, it is consistent with the 20% open space required for MUPDs.

D1, which requires 20% OS, is specific to RML and RMH, so it does not affect TC or NMU zones.

D2 – D7 are not limited to RML and RMH. The amendments would exempt the TC zones. For NMU, applicants could

<p>family dwellings, multi-family dwellings, residential care facilities, residential care institutions, and nursing or convalescent homes shall provide 20% of the lot area in open space, excluding that area designated in the site plan and improved for off-street parking and driveways. All dwelling units shall be immediately accessible to a minimum of 600 square feet of open space.</p> <ol style="list-style-type: none"> 2. All condominium, two-family dwellings and multi-family dwellings of 20 units or more, residential care facilities, residential care institutions, and nursing or convalescent homes shall improve 1/2 of the required open space as recreation space. Recreation space shall be planted in grass and/or improved for recreational use, and have a minimum area of 870 square feet and a minimum dimension of 20 feet. For two-family and multi-family dwellings, the recreational area shall include a children’s play area. A fence shall be installed that is a minimum of thirty (30) inches in height to separate a parking lot, street, or driveway from any children’s play area. 3. For the development of condominiums, two-family and multi-family dwellings of 20 units or more, residential care facilities and residential care institutions, individual private open space (patio or balcony) shall be provided for each dwelling unit. All private open space shall be directly accessible from the dwelling unit through a doorway. Patios and balconies shall be at least 48 square feet in size with a minimum width dimension of four (4) feet. 4. Floor area of ground floor patios and all balconies for individual units can be used to meet no more than 1/2 of the recreational land area requirement. Private open space shall be separated from common open space through the use of perimeter landscaping, fencing or a change in vertical grade (e.g. second floor balconies). 5. Where a proposed condominium, multi-family development, residential care facility, residential care institution, or nursing or convalescent home will abut an existing or proposed public park or open space, the development shall integrate into its design the adjoining park or open space to maximize its benefits to development residents. 6. No less than 75% of all open space areas excluding recreational space shall be landscaped with living ground cover, shrubs or trees. 7. State law and administrative codes pertaining to residential care facilities, assisted care and nursing facilities shall supersede the provisions of this section where there is a conflict. <p>E. Areas and tracts of land to be held in common, shall meet the following requirements:</p> <ol style="list-style-type: none"> 1. Common areas shall be held in ownership by either a homeowners association or operator of a multi-family development, residential care facility, residential care institution, or nursing or convalescent home. 2. To ensure adequate maintenance of common areas within the land division, the applicant shall provide maintenance provisions in Covenants, Conditions and Requirements (CC&Rs) when to be maintained by a homeowners association or another method an operator of a multi-family development, residential care facility, residential care institution, or nursing or convalescent home. <p>F. A development shall include proposed dedication of open space or recreational facilities to the City or reserve such areas for acquisition by the City based on the following requirements:</p>	<p>modify standards with MUPD.</p>
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<p>1. The dedication or reservation shall be approved by the Parks and Recreation Commission prior to any acceptance of the reservation or dedication by the City. The Parks and Recreation Commission shall only consider any dedication or reservation if found:</p> <ul style="list-style-type: none"> a. Consistent with the Parks, Recreation and Open Space Master Plan in terms of location, size, shape and purpose of the facility or area; b. Adequate recreational elements for active and/or passive recreational needs (e.g. trails, benches, play equipment, ball fields, etc.) are provided; c. Open space areas provide connectivity with other open space or recreational areas as shown on the Parks, Recreation and Open Space Master Plan; d. Adequate services and facilities are available to maintain the dedication or reservation; and e. For reservations, that acquisition of the property has been programmed in the most recently adopted capital improvement program or otherwise authorized by the City Council. <p>2. Where the City will accept a dedication pursuant to paragraph (1) above, no more than 50% of the System Development Charges (SDC) for a project can be waived by the Community Development Director upon consultation with the Parks and Recreation Director. The amount of the waiver shall be based on the completeness of the recreational elements and maintenance facilities provided, and the importance of the property to the completion of the Park, Recreation and Open Space Master Plan. There shall be no waiver of park SDC for any reservation of an open space area or recreational facility.</p> <p>3. If the developer is required to reserve land area for a park, playground, or other public use, such land shall be acquired by the appropriate public agency within eighteen (18) months following final land use approval including but not limited to final plat, site plan, conditional use or design review approvals. The price shall be agreed upon prior to final approval or such reservation shall be released to the subdivider.</p> <p>G. Where it is determined by the Director that to achieve a greater sense of open space of an area, open space that is proposed for two adjoining developments shall be located adjacent to each other.</p>	
<p style="text-align: center;">Figure 8-4 <u>6</u>: Parking Lot Landscaping</p>	<p>Updated Figure numbering. No graphic change</p>
<p>HAZARDS AND RESOURCES</p>	<p>No change to this section</p>
<p>LANDSCAPING, SCREENING & BUFFERING</p> <p>10.8.420 LANDSCAPING REQUIREMENTS BY ZONE</p> <p>A. <u>Landscaping Required In The Neighborhood Commercial Zone.</u> All required yards (exclusive of accessways and other permitted intrusions) adjacent to a public or private street shall be landscaped.</p> <p>B. <u>Landscaping Required In The Community Commercial Zones.</u> A landscaped strip at least ten (10) feet in width shall be provided abutting any property line facing a street. The landscape strip shall be appropriately landscaped with ground cover, planted berm, shrubbery and/or trees.</p>	<p>C. Proposed amendments would eliminate</p>

<p>C. <u>Landscaping Required In The Town Center Zones.</u> All new commercial and residential developments in the Town Center <u>Transition</u> Zones shall landscape no less than 5% of the lot area using appropriate native plant materials and/or architectural features such as benches, planters, and water fountains which are suitable and supportive of the downtown commercial environment. Jointly improved landscaped areas are encouraged to facilitate continuity of landscape design within the Town Center. <u>The City may choose to waive this requirement for developments that provide streetscape amenities within the public right-of-way. New development in the Town Center Core zone is exempt from the minimum landscaping requirement.</u></p> <p>D. <u>Landscaping Required in the Institutional Zone.</u> Landscaping shall be in context with the use and surrounding zones. For institutional uses that consist of primarily open space, such as a park or playground, the landscaped area shall be defined by the site plan and include a landscaped strip at least ten (10) feet in width abutting any property line facing a street. For institutional uses in which the site is primarily devoted to non-open space, the landscaping requirements shall match the zone district immediately adjacent to the use. Where there are no landscaping requirements for the adjoining zone, the landscaping requirements shall meet the requirements of subsection (B) <i>Community Commercial</i> above.</p> <p>E. <u>75% Coverage. Except in the Town Center Core Zone,</u> At least 75% of the required landscaped area shall be planted with any suitable combination of trees, shrubs, or evergreen ground cover. The required 75% coverage shall be based on the size of the plant material within a specified time as follows:</p> <ol style="list-style-type: none"> 1. Trees – within five (5) years from the date of final inspection by the Building Official. 2. Shrubs – within two (2) years from the date of final inspection by the building Official. 3. Ground covers – at the time of final inspection by the Building Official. <p>F. <u>25% Architectural Features. Except in the Town Center Core Zone,</u> Landscaped areas as required by this article may include architectural features or artificial ground covers such as sculptures, benches, masonry or stone walls, fences, rock groupings, bark dust, decorative hard paving and gravel areas, interspersed with planting areas. The exposed area developed with such features shall not exceed 25 percent of the required landscaped area. Artificial plants are prohibited in any required landscaped area.</p>	<p>the 5% landscape requirement in TCC and allow City the option of eliminating it in TCT for developments that provide streetscape improvements in the public ROW.</p> <p>F. Bark dust is proposed to be deleted as a possible landscaped cover.</p>
<p>10.8.425 BUFFERING AND SCREENING STANDARDS</p>	
<p>A. <u>General Provisions</u></p> <ol style="list-style-type: none"> 1. It is the intent that these requirements shall provide for privacy and protection and reduce or eliminate the adverse impacts of visual or noise pollution at a development site, without unduly interfering with the view from neighboring properties or jeopardizing the safety of pedestrians and vehicles; 2. Buffering and screening is required to reduce the impacts on adjacent uses which are of a different type in accordance with the matrices in this chapter (Tables 8-2, 8-3 and 8-4). The owner of each proposed development is responsible for the installation and effective maintenance of buffering and screening. 3. In lieu of these standards, a detailed buffer area landscaping and screening plan may be submitted for the Director's approval as an alternative to the standards, provided it 	

affords the same degree of buffering and screening as required by this code.

B. Buffering and Screening Requirements

1. A buffer consists of an area within a required setback adjacent to a property line and having a depth equal to the amount specified in the buffering and screening matrix and containing a length equal to the length of the property line of the abutting use or uses.
2. Utilities, screening, sidewalks and bikeways, and landscaping may only occupy a buffer area. No buildings, accessways or parking areas shall be allowed in a buffer area except where an accessway has been approved by the City.
3. The minimum improvements within a buffer area shall consist of combinations for landscaping and screening as specified in Tables 8-2, 8-3 and 8-4. In addition, improvements shall meet the following specifications:
 - a. At least one (1) row of trees shall be planted. They shall have a minimum caliper of two (2) inches at four (4) feet in height above grade for deciduous trees and a minimum height of five (5) feet high for evergreen trees at the time of planting. Spacing for trees shall be as follows:
 1. Small or narrow-stature trees, less than twenty-five (25) feet tall or less than sixteen (16) feet wide at maturity shall be spaced no further than twenty (20) feet apart;
 2. Medium-sized trees between twenty-five to forty (25-40) feet tall and with sixteen to thirty-five (16-35) feet wide branching at maturity shall be spaced no greater than thirty (30) feet apart;
 3. Large trees, over forty (40) feet tall and with more than thirty-five (35) feet wide branching at maturity, shall be spaced no greater than forty (40) feet apart.
 - b. In addition, at least ten (10) five-gallon shrubs or twenty (20) one-gallon shrubs shall be planted for each 1,000 square feet of required buffer area;
 - c. The remaining area shall be planted in lawn or other living ground cover.
4. Where screening is required the following standards shall apply in addition to those required for buffering:
 - a. A hedge of narrow or broad leaf evergreen shrubs shall be planted which will form a four (4)-foot continuous screen of the height specified in Table 8-3 within two (2) years of planting; or
 - b. An earthen berm planted with evergreen plant materials shall be provided which will form a continuous screen of the height specified in Table 8-3 within two (2) years; or
 - c. A fence or wall of the height specified in Table 8-3 shall be constructed to provide a continuous sight-obscuring screen.
5. Buffering and screening provisions shall be superseded by the clear vision area requirements as set forth in Section 10.8.150.

6. When the use to be screened is downhill from the adjoining zone or use, the prescribed heights of required fences, walls, or landscape screening shall be measured from the actual grade of the adjoining property. In this case, fences and walls may exceed the permitted six (6)-foot height at the discretion of the Director as a condition of approval. When the grades are so steep so as to make the installation of walls, fences or landscaping to the required height impractical, a detailed landscape/screening plan shall be submitted for approval.
7. Fences and Walls:
 - a. Fences and walls shall be constructed of any materials commonly used in the construction of fences and walls such as wood, stone, rock or brick, or otherwise acceptable by the Director;
 - b. Such fence or wall construction shall be in compliance with other City regulations;
 - c. Walls shall be a minimum of six (6) inches thick; and
 - d. Chain link fences with slats shall qualify for screening. However, chain link fences without slats shall require the planting of a continuous evergreen hedge to be considered screening.

C. Screening: Special Provisions

1. Screening and Landscaping Of Parking and Loading Areas. Screening and landscaping of parking and loading areas is required as provided in this Article.
2. Screening of Service Facilities. Except for one-family and two-family dwellings, any service facilities such as gas meters and air conditioners which would otherwise be visible from a public street, customer or resident parking area, any public facility or any residential area shall be screened from view by placement of a solid wood fence or masonry wall between five and eight (5-8) feet in height.
3. Screening of Refuse Containers. Except for one- and two-family dwellings, any refuse container or refuse collection area which would be visible from a public street, parking lot, residential or commercial area, or any public facility such as a school or park shall be screened or enclosed from view by placement of a solid wood fence, masonry wall or evergreen hedge. All refuse shall be contained within the screened area.
4. Screening of Swimming Pools. All swimming pools shall be enclosed as required by City of Forest Grove Building Code.

D. Buffer Matrix

1. The Buffer Matrices contained in Tables 8-2, 8-3 and 8-4 shall be used in calculating widths of buffering/screening and required improvements to be installed between proposed uses and abutting uses or zoning districts.
2. An application for an adjustment or variance to the standards required in Tables 8-2, 8-3 and 8-4 shall be processed as a Type I, II or III procedure, as regulated by Article 2 provisions for Adjustments and Variances.

TABLE 8-2: BUFFER MATRIX PROPOSED USE

DEVELOPMENT SITE → ABUTTING USE	Single Units, Detached; Manufactured Units	Attached Single Units and Multifamily, 1-5 Units; Duplexes	Attached Single Units and Multifamily, 5+ Units	Commercial and Institutional Zones (NC, CC, INST)	Town Center Zones (TCC, TCS, TCT)	Indus- trial Zones (LI, GI)
Detached Single Units; Manufactured Units	--	A	C	D	C	E
Attached Single Units and Multifamily, 1-5 Units, Duplexes	A	--	B	D	C	E
Attached Single Units and Multifamily, 5+ Units	A	A	--	D	C	E
Commercial Zones (NC, CC)	C	C	C	--	A==	D
Town Center Zones (TCC, TCS, TCT)	C	C	C	A==	--	D
Industrial Zones (LI, GI)	D	D	D	B	A==	--

Table 8-2:
Eliminated
buffer
requirements
between Town
Center zones
and CC zone.

Eliminated TCS
from Table 8-2

Note: See Table 8-3 for alternative combinations for meeting these screening requirements.

**TABLE 8-3
BUFFER COMBINATIONS FOR LANDSCAPING AND SCREENING[1]**

	Options	Minimum Width	Trees (per linear feet of buffer)	Shrubs or Groundcover	Screening
A	--	10	--	Lawn / living	--
B	--	10	20' min / 30' max	Lawn / living	--
C	1	10	15' min / 30' max spacing	Shrubs	4' hedges
	2	8		Shrubs	5' fence
	3	6		Shrubs	6' wall
D	1	20	10' min / 20' max spacing	Shrubs	6' hedge
	2	15		Shrubs	6' fence
	3	10		Shrubs	6' wall
E	1	30	10' min / 20' max spacing	Shrubs	6' hedge or fence
	2	25		Shrubs	5' earthen berm or wall

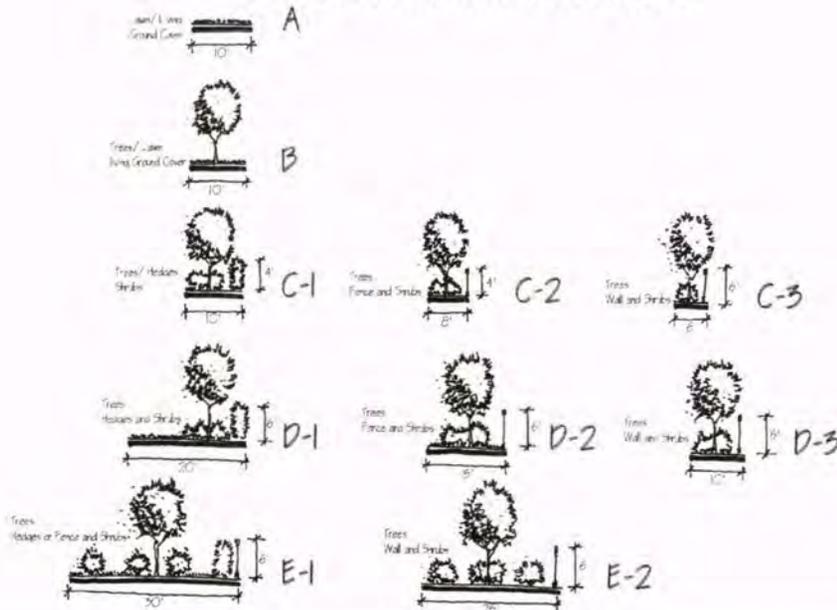
[1] Buffers are not required between abutting uses that are not of a different type when the uses are separated by a street. Adjustments from these requirements can be obtained; see Article 2.

Figure 8-5 7
Buffer Example – Between Single-Family and Multi-Family



Updated Figure numbering. No graphic change

TABLE 8-4
BUFFER COMBINATIONS FOR
LANDSCAPING AND SCREENING



OFF-STREET PARKING AND LOADING
10.8.505 APPLICABILITY

- A. New Construction. At the time of the erection of a new structure within any residential, commercial, institutional, and industrial zoning district, off-street vehicle parking shall be provided in accordance with this section. Except as specified in subsection D, below, uses ~~Uses~~ in the Town Center zones are exempt from the requirement to provide off-street parking.

- B. Expansion of Existing Use. At the time of an enlargement of a structure, which increases the on-site vehicle parking requirement, off-street vehicle parking shall be provided in accordance with this section, subject to the following:
 - 1. On the date of adoption of this code, the number of vehicle parking and loading spaces required shall be based only on floor area or capacity of such enlargement.

- C. Change of Use. When an existing structure is changed from one use to another as listed in this section, the following provisions shall apply:
 - 1. If the parking requirements for each use are the same, no additional vehicle parking shall be required;
 - 2. Where a change results in an intensification of use in terms of the number of vehicle parking spaces required, additional vehicle parking spaces shall be provided in an amount equal to the difference between the number of spaces required for the existing use and the number of spaces required for the more intensive use;
 - 3. Where the change results in a decrease in intensity of use, the applicant may eliminate excess vehicle parking spaces in an amount equal to the difference between the number of spaces required for the existing use and the number of spaces required for the less intensive use.

- D. Parking Exemption for the Town Center Zones.
 - 1. Except for new multifamily dwelling units, all AH development in the Town Center zones shall be exempt from off-street parking requirements for new construction, expansion of existing use and change of use. New multifamily or mixed use developments (residential portion only) shall provide the minimum required spaces in accordance with Table 8-5 except that:
 - a. In order to encourage the preservation and reuse of historic buildings, no parking shall be required for new or existing multifamily units above the ground floor in buildings built before 1950.
 - c. A 25% reduction in the minimum number of parking spaces required is allowed for age-restricted senior housing.
 - d. A reduction of three (3) parking spaces is allowed for every one (1) dedicated car-share space provided.

Change exemption to allow for new parking requirement for MF.

D. Amendments address new parking requirement for new MF units in the TC zones.

TABLE 8-5: Parking Requirements

LAND USE	MINIMUM PARKING REQUIRED	MAXIMUM PARKING ALLOWED	
		Parking Zone	Parking Zone B
RESIDENTIAL			
Household Living - Single Units, Attached	See Multifamily	None	None
- Single Units, Detached	1.0 / DU	None	None
- Accessory Units	1.0 / DU	None	None
- Duplexes	1.0 / DU	None	None
- Multifamily Units (outside of Town Center)	DU<500 sq ft: 1.0 / DU 1 bedroom: 1.25 / DU 2 bedroom: 1.50 / DU 3 bedroom: 1.75 / DU	None	None
- Multifamily Units (within in the Town Center)	DU<500 sq ft: 0.5 / DU 1 bedroom: 0.75 / DU 2 bedroom: 1.25 / DU 3 bedroom: 1.75 / DU	None	None
- Manufactured Units	1.0 / DU	None	None
- Mobile Home Parks	1.0 / DU	None	None
Group Living	1.0 / room 1.0 / 2.5 beds	None 2.7 / 1000[2]	None
Transitional Housing	1.0 / 2.5 beds	None	None
Home Occupation	None	None	None
CIVIC			
Basic Utilities	None	None	None
Colleges	1.0 / 5 students / staff	1.0 / 3.3 students / staff	1.0 / 3.3 students / staff
Community Recreation	2.0 / 1,000[2]	2.5 / 1,000[2]	4.0 / 1,000[2]
Cultural Institutions	2.5 / 1,000[2]	3.5 / 1,000[2]	4.5 / 1000[2]
Day Care - Home	None	None	None
- Commercial	2.0 / classroom	2.7 / 1,000[2]	3.2 / 1,000[2]
Emergency Services	3.0 / 1,000[2]	3.5 / 1,000[2]	4.5 / 1,000[2]
Postal Services	2.5 / 1,000[2]	3.0 / 1,000[2]	4.5 / 1,000[2]
Religious Institutions	1.0 / 4 seats in main assembly area	1.0 / 1.7 seats in main assembly	1.0 / 1.3 seats in main assembly area
Schools			
- Preschool	5.0 + 1 / classroom	7.0 + 1 / classroom	10.0 + 1 / classroom
- K-8	2.0 / classroom	2.5 / classroom	3.5 / classroom
- 9-12	1.0 / 5 students / staff	1.0 / 3.3 students / staff	1.0 / 3.3 students / staff
Social / Fraternal Clubs / Lodges	10.0 / 1,000[2] main assembly area	12.0 / 1,000[2] main	14.0 / 1,000[2] main assembly area

Proposed amendment to the parking table would require parking for MF in the TC, but at somewhat reduced rate relative to the rest of the city. |

<p style="text-align: center;">Figure 8-6 <u>8</u>: Parking Stall Dimensions +Figure 8-7 <u>9</u>: Parking Stall and Aisle Dimensions</p>	<p>Updated Figure numbering. No graphic change</p>
<p>PUBLIC IMPROVEMENTS</p>	<p>No change to this section.</p>
<p>BUILDING DESIGN AND DEVELOPMENT STANDARDS</p> <p>10.8.710 STANDARDS</p> <p>A. Building Design Standards for Multi-Unit Development <u>except within Town Center Zones.</u></p> <p>1. Massing and Form</p> <p>a. On lots with an average cross slope of 15% or more, no more than 60% of the site shall be re-graded.</p> <p>b. No building shall have a dimension greater than one-hundred and fifty (150) feet without a minimum three (3) feet off-set of the exterior wall.</p> <p>c. At a minimum, every two dwelling units shall be offset from the next dwelling unit by at least four (4) feet in depth.</p> <p>d. All habitable rooms shall incorporate at least one window when facing parking lots and common areas.</p> <p>2. Compatibility</p> <p>a. All buildings shall incorporate a porch or architecturally defined entry space for each ground level dwelling unit with a minimum area of 16 square feet per dwelling unit, with no dimension less than 4 feet.</p> <p>b. Shared porches or entry spaces are permitted. All grade level porches shall include hand-railing, half-walls, or shrubs to define their outside perimeter.</p> <p>e. Common entrances shall not serve more than four (4) dwelling units.</p> <p>3. Privacy</p> <p>a. Front entryways into dwelling units shall be separated vertically or horizontally a minimum of three feet from sidewalks used by more than one dwelling unit.</p> <p>b. Bedroom and bathroom windows shall be offset a minimum of four (4) feet from windows on adjacent buildings (unless window glazing is frosted, diffused or glass block)</p>	<p>Some of these standards (e.g. subsection 2a, 2c, 4b) appear to be contrary to the goals of the Town Center zones to have higher density buildings. The TC zones already have their own standards. Proposed amendments make it clear that TC zones are exempt from these zones.</p> <p>Deleted common entrance requirement due to not being practical with buildings served by elevators.</p>

Figure 8-8 10: Examples of Private Multi-Family Open Space

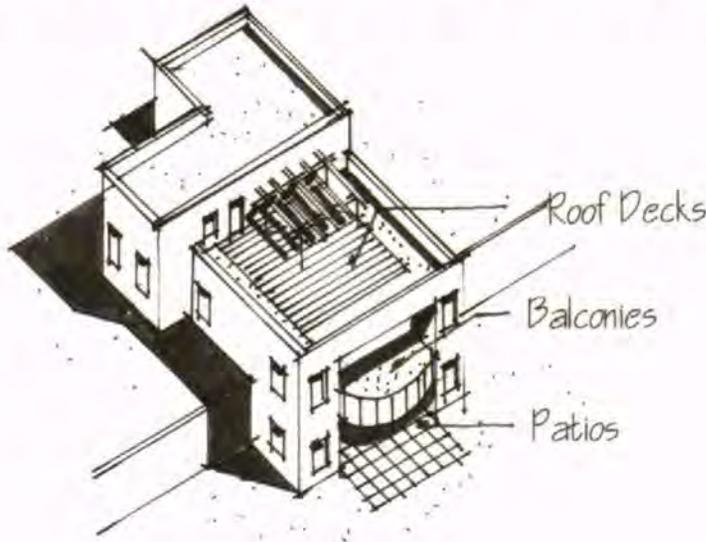


Figure 8-8:
Updated Figure
numbering. No
change to
graphic.

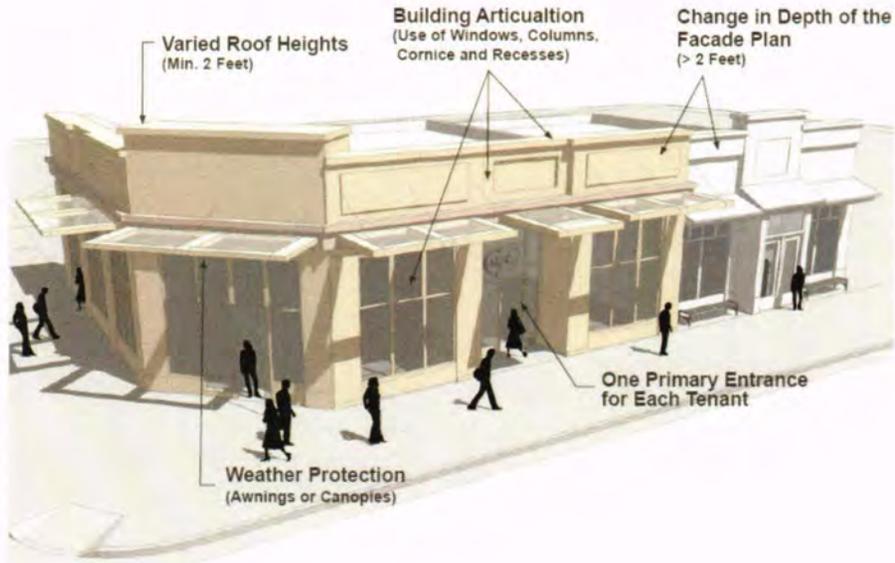
4. Building Relationship with Adjacent Arterials
 - a. Buildings shall be setback at least 10 feet from arterials.
 - b. Buildings shall be oriented away from arterials and no front or main entries shall be facing the arterials.
 - c. The setback area shall be landscaped consistent with the buffering requirements of Section 10.8.425 except that chain link fence shall not be allowed.

B. Building Design Standards for Commercial Development except within Town Center or Mixed Use zones.

1. Building Orientation
 - a. Design and construct buildings so that at least one façade is within sixty (60) feet of a dedicated public street right-of-way line.
 - b. Design and construct a minimum of one primary building entrance for each building ground floor use or tenant along a public right-of-way or internal roadway built to City public street engineering standards.
2. Massing and Form
 - a. Provide changes in the depth of the façade plane in excess of two (2) feet for all building walls more than 75 feet in length.
 - b. Maximum length of building facades shall be 200 feet.
 - c. Roofline heights must be varied a minimum of 2 feet for building facades greater than 75 feet in length.
 - d. Provide vertical partitioning of facades by using columns, pilasters and other vertical elements a minimum of every twenty-five (25) feet.
 - e. Articulation of building fronts through changes in depth or building relief (e.g. windows, doors, cornices, columns, change in materials) shall occur a minimum of every fifteen (15) feet in the horizontal or vertical dimensions.

The TC zones already have their own standards and NMU will be subject to a MUPD. Proposed amendments make it clear that these zones are exempt from this section.

Figure 8-11: Commercial Development Massing and Form

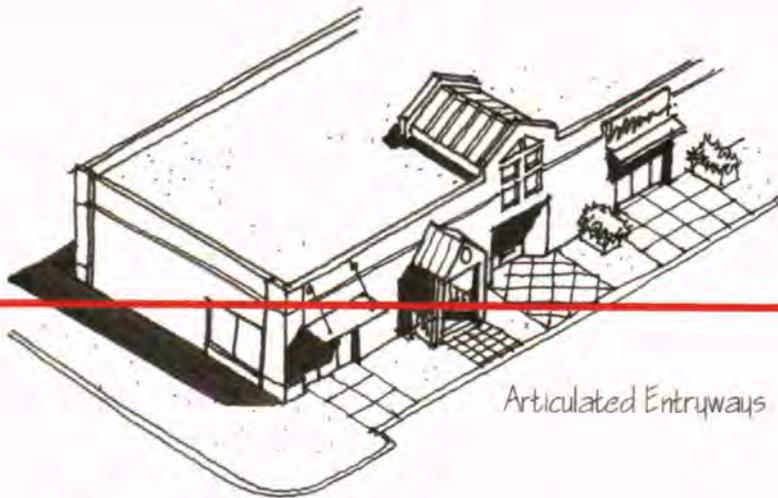


New figure added Figure 8-11: Massing and Form

Figure 8-9 12: Example of Building Entry Design Elements



Replaced Figure 8-9 and renumbered to Figure 8-12



Delete existing Figure 8-9.

3. Design Elements

- a. Every building elevation adjacent to a street with a horizontal dimension of more than 100 feet shall have a building entrance.
- b. Building facades adjacent to sidewalks or pedestrian connections must include weather protection canopies or awnings along at least 75% of the length of the ground floor façade.
- c. Incorporate a minimum of 50% transparency/window openings on the ground floor facades of buildings that are 5,000 square feet or less, or 20% transparency/window openings for buildings greater than 5,000 square feet facing the public right-of-way. For a building that faces two streets, the requirement shall apply to the predominant street or as determined by the Director where both streets are considered similar.
- d. Windows shall not be reflective, tinted or treated in such a way as to block views into or out of windows unless for energy or security purposes.
- e. Windows must be recessed a minimum of 4 inches from building facades.
- f. Materials such as synthetic stucco (EIFS, Dryvit, etc.) are prohibited at ground floor.
- g. Vinyl cladding of building façade is prohibited at ground floor.

4. Compatibility

- a. Any building or portion of a building that is less than 50 feet from existing residential dwellings must not be higher than 15 feet above the top of the roofline of the nearest existing dwelling.
- b. When adjacent to existing residential development, all commercial structures with floor areas less than 20,000 square feet of total building area must provide a gabled element to the parapet or roof.

5. Safety

- a. Security gates or bars on windows or doors are prohibited along ground floors of commercial buildings, excluding loading and storage areas.
- b. Motion activated security lighting is prohibited along building facades that front onto public rights-of-way.
- c. Public access shall be provided to all commercial uses that provide plazas, squares, or courts intended for food, beverage or entertainment uses.
- d. Parking lots or roadways shall not be gated or secured excluding loading and storage areas or residential parking areas.

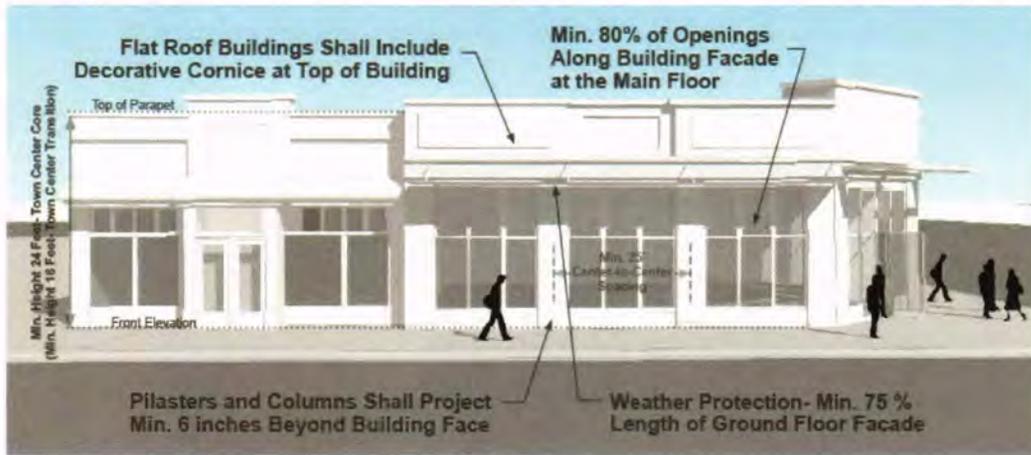
C. Building Design Standards within Town Center Districts

1. Building Form

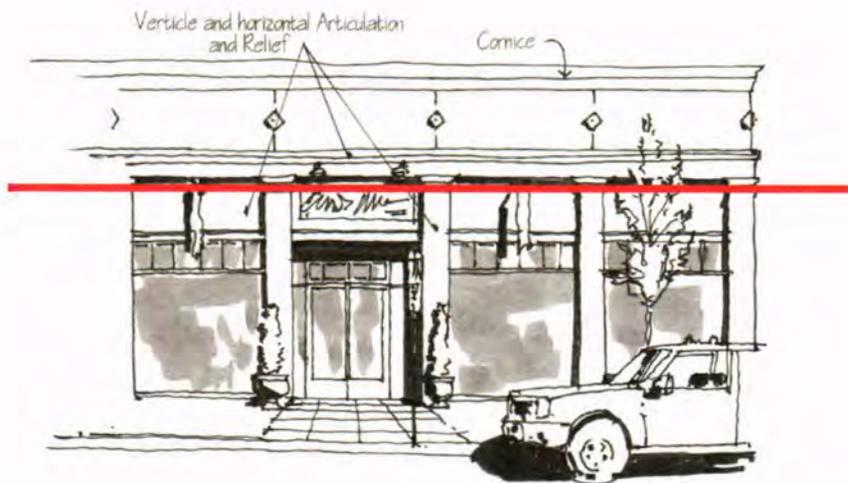
- a. All new structures shall be a minimum of two-stories in height in the TCC zone or a minimum height of ~~24 feet~~ 16 feet in the TCT zone measured at the front elevation to top of parapet or eave line of lowest point of facade.
- b. All flat-roofed buildings shall have a decorative cornice at top of building (parapet)
- c. Exterior pilasters and columns shall project a minimum of 6 inches beyond building face.
- d. All rooftop mechanical equipment shall be screened by a solid wall from view of the public right-of-way and pedestrian routes.

Section C Amended for consistency with development standards table in zone.

Figure 8-10 13: Retail Storefront Details-Town Center Districts Building Form



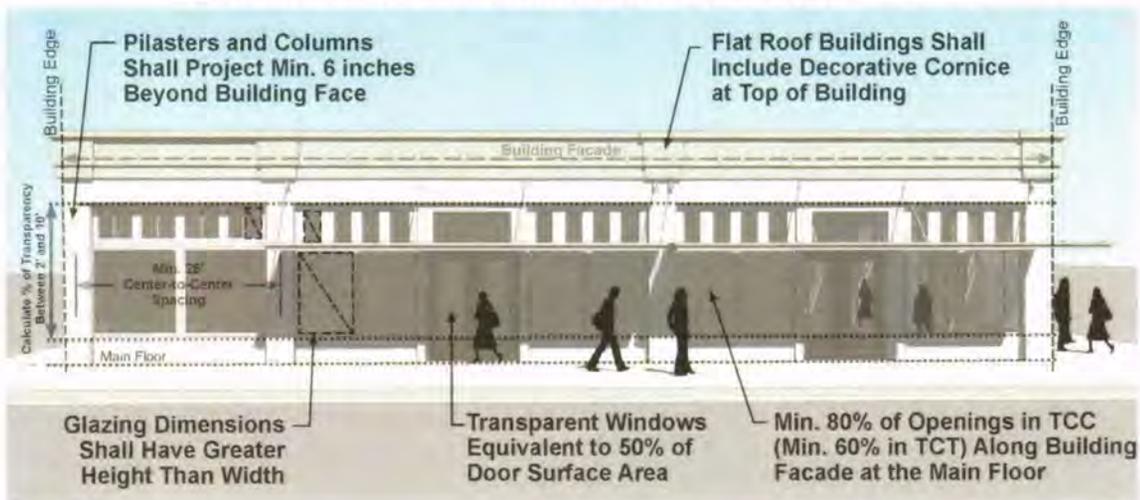
Updated Figure 8-10 with new graphic and renumbered to 8-13



2. Retail Storefronts

- a. ~~Ground floor storefront glazing along the primary public façade shall comprise a minimum percentage of the main floor's exterior wall area, as follows: TCC—80%; TCT & TCS—50%.~~
- ~~a.~~ ~~b.~~ First floor vertical elements such as columns or pilasters shall be provided and spaced center-to-center at a maximum of 25 feet apart.
 - ~~b.~~ ~~e.~~ Doors on the main floor façade facing a street shall contain windows equivalent in size to 50% of door surface area.
 - ~~c.~~ ~~d.~~ Storefront glazing must be transparent. Reflective, tinted, glazed or techniques that obscure more than 20% of glazed surfaces shall be prohibited.
 - ~~d.~~ ~~e.~~ Glazing dimensions shall have a greater height than width.
 - ~~e.~~ ~~f.~~ Storefront glazing with divided lights shall be limited to transom windows only.
 - ~~f.~~ ~~g.~~ All window frames shall be painted.
 - ~~g.~~ ~~h.~~ Awnings shall be constructed of metal, glass or natural canvas fabrics. Vinyl, synthetic fabric, plastic or backlit awnings are prohibited. ~~Signage or lettering on awnings is prohibited.~~

Figure 8-14: Town Center Districts Retail and Storefronts Details



Subsection 2a is redundant to Subsection 5 below. Proposed amendments delete 2a in favor of subsection 5.

New Figure 8-14: Town Center Districts Retail and Storefronts Details graphic

3. Commercial Entries

- a. The entry enclosure shall be offset a minimum of 2 feet from the building façade.
- b. Windows and door in exterior wall shall be surrounded with trim of 2-1/2 inches minimum width.
- c. At least 25% of all primary entry doors shall contain transparent glazing.
- d. Unpainted metal frames are prohibited.
- e. Reflective, opaque or tinted glazing is prohibited.

4. Residential Entries

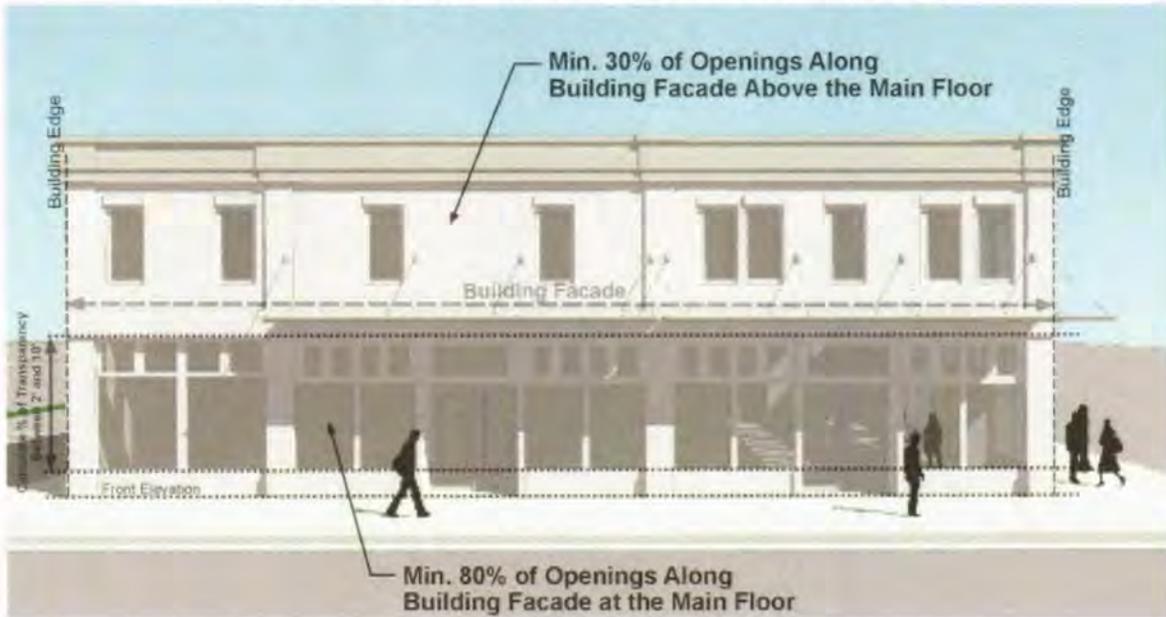
- a. Residential doors which face a public right-of-way shall be setback a minimum of 2 feet 3 feet from a public sidewalk plus any additional setback needed to meet the minimum sidewalk requirements (where applicable).

4a. Require that residential entries be setback at least 3 feet. In some cases additional setback may be needed to accommodate sidewalk

<ul style="list-style-type: none"> b. The following types of doors shall not provide the primary entrance into a dwelling unit: sliding glass or solid metal doors without glazing. c. Residential stoops, porches or terraces shall be raised no higher than 34 feet above grade at the adjacent right of way. d. Residences with entry porches or terraces shall have a minimum area of 60 square feet. e. Residential entries shall be no lower than 4 feet below grade measured at adjacent right-of-way. 	<p>requirements downtown.</p> <p>4c increase the permissible height of stoops, etc.</p>
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<p>5. <u>Windows and doors</u></p> <ul style="list-style-type: none"> a. Window <u>and door</u> openings shall comprise the following minimum portions of the front building facades at the main floor <u>measured between 2 feet above sidewalk to 10 feet above sidewalk</u>: TCC - 80%; TCS - 30%; TCT - 60% 50%. b. Window <u>and door</u> openings shall comprise the following minimum portions of the front building façade above the main floor <u>measured between 2 feet above sidewalk to 10 feet above sidewalk</u>: TCC - 30%; TCS - 20%; TCT - 30%. c. Clear or transparent glazing is required for windows fronting the public rights-of-way. d. Glass shall be recessed a minimum of 1-1/2 inches from the surrounding exterior wall surface. e. Spandrel, glass curtain-wall or any window wall glazing that creates an opaque, flat or featureless, or reflective surface shall not be used at ground floor. 	<p>5a/5b Removed TCS reference. Allow doors to be included. Change from 60% to 50% for consistency with Subsection 2.</p>
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Figure 8-15: Town Center Districts Window and Door Openings



New Figure 8-15: Town Center Districts Window and Door Openings graphic

<p>6. <u>Exterior Walls</u></p> <ul style="list-style-type: none"> f. Vinyl, plastic or metal siding are prohibited the all Town Center zones. g. Synthetic Stucco (EIFS, Dryvit, etc.) insulating cladding materials along the first floor of facades that front public rights-of-way are prohibited. h. Flagstone or other stone veneer along the first floor of facades that front public 	
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<p>rights-of-way are prohibited.</p> <p>i. Simulated or cultured stonework are prohibited for commercial uses.</p> <p>j. Wood, asphalt or cement shingles are prohibited at first floor for commercial uses.</p> <p>7. Walls and Fences</p> <p>k. Plastic and/or chain-link fences are prohibited in all Town Center zones.</p> <p>l. All wood fences shall be painted.</p>	
<p>ARTICLE 12 USE CATEGORIES & DEFINITIONS</p>	
<p>10.12.100 CLASSIFICATION OF USES</p> <p>Uses are assigned to the category whose description most closely describes the nature of the primary uses. When all the primary uses of a development fall within one use category, then the development is assigned to that category. When the primary uses of a development fall within different use categories, each primary use is classified in the applicable category and is subject to the regulations for that category.</p>	
<p>10.12.105 LISTING OF USE CATEGORIES</p> <p>All uses are classified into one or more of the following use categories. In order of listing in this section, they are: Residential, Civic, Commercial, Industrial and Other. Use types listed within each category are presented in alphabetical order.</p> <p>In addition to use categories, the Code defines residential building types. Residential building types are listed in each zoning district that permits residential use from least intensive to most intensive (for example, “Single Detached” is listed above “Single Attached”, and both of these building types are listed above “Multi-Family”).</p>	
<p>10.12.130 COMMERCIAL USE CATEGORY</p> <p>...</p> <p><u>F. Motor Vehicle Related</u></p> <p>1. <u>Motor Vehicle Sales/Rental: Includes car, light and heavy truck, mobile home, boat and recreation vehicle sales, rental and services. Parking facilities for car sharing vehicles by a car sharing organization are regulated as accessory parking to the use where the parking is located and are not classified as motor vehicle rental.</u></p>	<p>Modified Motor Vehicle Sales/Rental use category to clarify relationship to car-sharing.</p>
<p>10.12.210 MEANING OF SPECIFIC WORDS AND TERMS</p> <p>...</p> <p>C1. Caliper. The diameter of a tree trunk measured at a prescribed height.</p> <p>C2. Carpool/Vanpool. A group of two (2) or more commuters who share the ride to and from work, school, and other destinations.</p> <p><u>C3. Car-Share Organization. A car-share organization” is any public or private entity that provides a membership-based car-share service to the public and manages, maintains, and insures motor vehicles for shared use by individuals and group members.</u></p> <p>C4-€3. Change of Use. Any use that differs from the previous use as defined in the Section 10.12.005, Use Categories.</p> <p>C5-€4. City. The area within the territorial limits of the City of Forest Grove.</p> <p>C6-€5. City Engineer. The person assigned the title of City Engineer for the City of Forest Grove, Oregon, or designee.</p>	<p>Added new definition of Car-Share Organization</p>

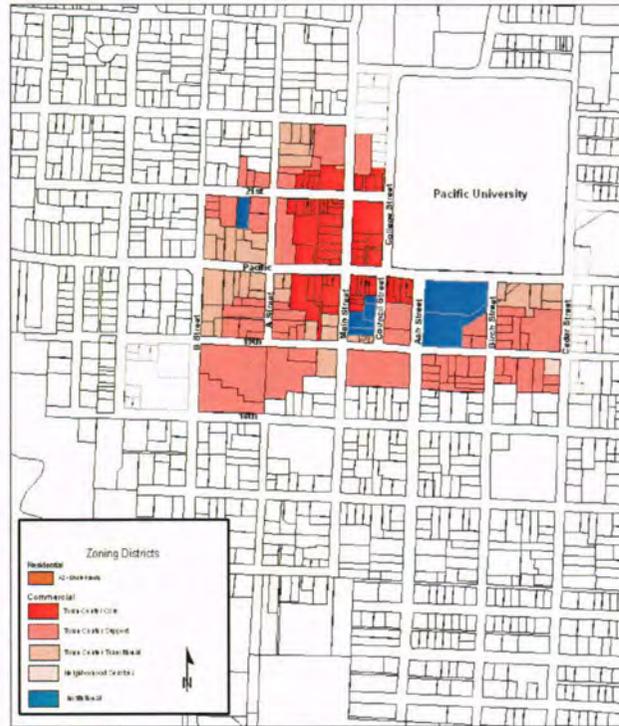
<p>C7-C6: City of Forest Grove. The governing structure for the municipality of Forest Grove, Oregon.</p> <p>C8-C7: Clear Vision Area. A triangular area located at the intersection of two (2) streets, a street and a railroad, or a street and driveway; defined by a line across the corners, the ends of which are on the street or alley lines, an equal and specified distance from the corner.</p> <p>C9-C8: Complex. A structure or group of structures developed on one (1) or more contiguous lots of record and developed as part of an overall development plan.</p> <p>C10-C9: Comprehensive Plan. The generalized, coordinated land use map and policy statement of the governing body of the City of Forest Grove that interrelates all functional and natural systems and activities relating to the use of land, including, but not limited to: sewer and water systems, transportation systems, educational facilities, natural resources, and air and water quality management systems.</p> <p>C11-C10: Conditional Use. A use which may be permitted by the approval authority following a public hearing, upon findings by the authority that the approval criteria have been met or will be met upon satisfaction of conditions of approval.</p> <p>C12-C11: Contiguous. See "Abutting."</p>	
<p>D4. <u>Density</u>. The intensity of residential land uses, usually stated as the number of housing units per net acre.</p> <ul style="list-style-type: none"> a. <u>Gross Density</u>. The number of residential dwelling units per acre based on the area of the site found inside the parcel boundary. This includes the building lots, parking and driving areas, sidewalks, public right-of-ways, public and private streets and common driveways, public and private open space areas, and other tracts intended for public use. b. <u>Net Density</u>. The number of dwelling units per acre based on the net site acreage, which does not include sidewalks, public right of ways, public and private streets, common driveways, public and private open space areas, and other tracts intended for common use rights-of-way through or on the edge of the site, environmentally constrained areas, or land intended for public ownership such park and open spaces uses. c. <u>Bonus Density</u>. Density bonuses <u>either</u> are granted by the Planning Commission and/or City Council to the developer, to have an overall ratio of dwelling units to the planned development site area greater than would be allowed for a conventional development in the same location; <u>or are applied pursuant to 10.7.400.</u> 	<p>Updated definition is more consistent with regional standards.</p>
<p style="text-align: center;">Figure 8-11 <u>16</u>: Signs in the Town Center</p> <p style="text-align: center;">Figure 8-12 <u>17</u>: Visual Surveillance Standard</p>	<p>Updated Figure numbering. No graphic change</p>

Design Guideline Handbook Amendments	Commentary
FOCUS AREA 1 – TOWN CENTER GUIDELINES	
<p>Overview Forest Grove began as a small farming community with a small downtown and a quality educational institution (now Pacific University) at its center. Since the community’s inception, the town center has been important and still maintains much of its turn of the century charm with numerous intact historic buildings.</p> <p>This section, Focus Area I, establishes specific design review guidelines for new development in the Town Center. The primary intent of the Town Center Focus Area is to reinforce the existing positive qualities of the architecture and streetscape. While the City is not seeking new development to replicate the architecture of existing buildings, new projects should use design elements that reinforce or enhance the character of the Town Center.</p> <p>Applicability The City has three <u>two</u> town center zones, listed below and illustrated in the map on the following page (Unless otherwise noted, Design Guidelines apply uniformly to <u>both</u> the three zones.)</p> <ul style="list-style-type: none"> • <u>Town Center Core (TCC)</u> provides for a concentration of retail and office needs in the core commercial area of the city. Retail or office uses are required on the ground floor, with an emphasis on those uses that serve a walk-in clientele. A combination of office and residential uses are allowed on upper floors. Design components should work together to form a unified district. Architectural elements should form a continuous and interesting storefront façade featuring streetscape amenities for the pedestrian shopper. • <u>Town Center Transition (TCT)</u> is similar in form and specific street front characteristics to the TCC with the primary difference being that retail uses are not required on the ground floor, and therefore the vibrant streetscape may also include residential or office activities. • Town Center Support (TCS) creates an area with a mix of office, light industrial and residential and some retail uses as a continuation of the Town Center compact urban form concept. Ground floor retail uses are allowed on the ground floor, but not required. A varied and attractive streetscape is desired in this zone. <p>In cases where a development falls into both the Town Center Focus Area and another Focus Area, both sets of guidelines may apply, but where they conflict the guidelines for the Town Center take precedence.</p> <p>The following section is divided into four general categories:</p> <ul style="list-style-type: none"> • Site • Building • Lighting • Signs <p>Each category has a set of corresponding design objectives, guidelines and standards, and is further divided into sub-categories as necessary.</p>	<p>Consolidated TC zones, delete TCS.</p>

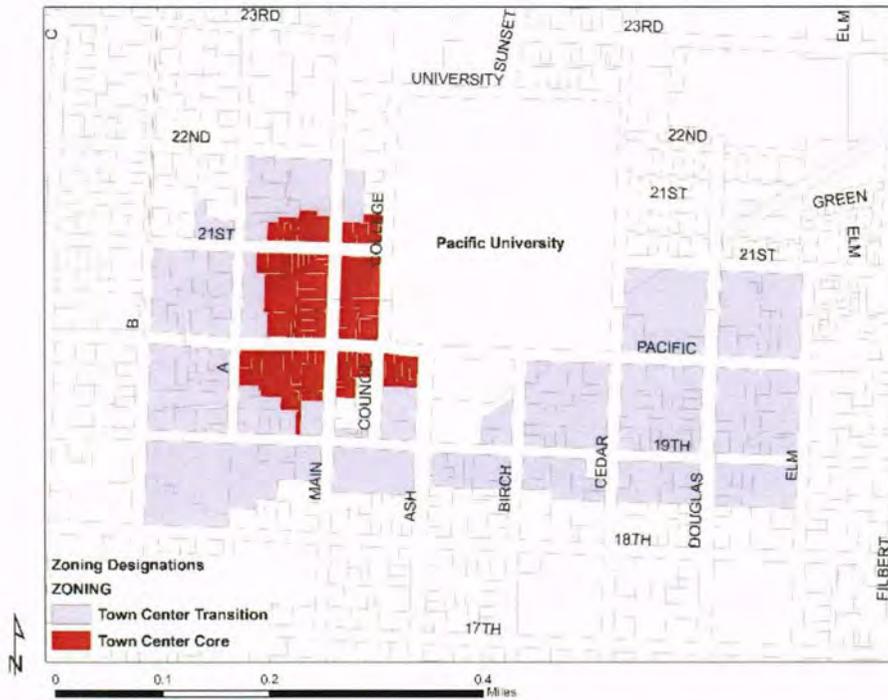
Design Guideline Handbook Amendments

Commentary

**Town Center Map
City of Forest Grove
Town Center Zoning Districts**



Map updated to reflect consolidation of Town Center zones and expansion of the Town Center boundary east to Elm St, south 1/2 from block from 19th and north to 21st



SITE – BUILDING ORIENTATION

Guideline – Design and construct buildings oriented to a public street right-of-way to create safe, pleasant and active pedestrian environments.

Description – Development in the town center area should create a well-defined, safe, attractive and active pedestrian environment. Zero lot line development should be encouraged in all town center zones, particularly the Town Center Core. Parking should be behind or to the side of buildings, so as to not obstruct pedestrian pathways to the front door of buildings. If buildings are setback from the sidewalk, the area in front of the building should be well landscaped and or used as an expanded sidewalk area or a pedestrian plaza.

Suggest modifying last sentence in description by changing “and” to “or”.

As a result of consolidating TC zones, delete TCS.

Track 2 – Design Guidelines

Recommended

- Minimize building setbacks from any public street right-of-way. Zero lot line buildings along the public street right-of-way are encouraged to maintain an inviting and continuous storefront presentation.
- Design and construct a primary building entrance for each building façade. If a building has frontage on more than one public street, a single building entrance on the corner is acceptable.
- Use the area between the right-of-way and building to create a plaza court, planter area, bicycle parking or another amenity.
- Main entrance should be oriented to the street.

Not Recommended

- Parking located between the building façade and the street.
- Excessive front setbacks (greater than 10 feet).
- Storage in front of building façade.

Track 1 – Development Standards

Related Code Standards

- TCC: Building facades shall be built along at least 80% of the primary building frontage line.
- TCT: Building facades shall be built along at least 75% of the primary building frontage line.
- ~~TCS: Building facades shall be built along at least 50% of the primary building frontage line.~~
- One (1) entrance shall be provided for each building façade. When a building is located on a corner, one entrance at the corner is permitted.
- If a building is setback from right-of-way, it shall be a minimum of four (4) feet in depth and include plazas, forecourts or other paved areas for public seating, artwork and landscaped planting beds.
- If a building is setback from the right-of-way, mechanical units, meters or other above grade or wall mounted utilities shall not be located in the front setback area.



Recommended: zero setbacks at sidewalk

SITE – AMENITIES

Guideline – Protect pedestrians from wind, sun and rain and provide courtyards or other outdoor spaces to create a comfortable environment for meeting and sitting.

Description – Pedestrians should be protected from the elements, to encourage use of the Town Center during inclement weather. The design of awnings and canopies should be integral and well proportioned to the facades of buildings. Street furniture, such as benches, lamps and landscape planters should be provided.

Track 2 – Design Guidelines

Recommended

- Provide weather protection above sidewalks in the form of awnings or other building elements appropriate to the design of the structure.
- Canvas fixed or retractable awnings or horizontal metal canopies.
- Seating areas near retail establishments.
- Courtyards, squares, forecourts, and plazas with active adjacent ground-floor uses.

Not Recommended

- Awnings with vinyl or synthetic fabrics, plastic, backlit, or with oversized lettering.
- Seating areas adjacent to loading, service bays or storage areas.
- Seating areas that are hidden, secluded, dark or unsecured spaces behind or on the side of buildings.

Track 1 – Development Standards

Related Code Standards

- Every development shall provide 2 or more of the “pedestrian amenities” listed below:
 - a. A plaza, courtyard next to the building entrance a minimum of four (4) feet in depth for residential food, beverage or entertainment establishments only.
 - b. Seating areas (chairs, tables, benches)
 - c. Building canopy, awning or similar weather protection across 75% of façade projecting a minimum of four feet over the sidewalk.
 - d. Public art ~~which that~~ incorporates seating (e.g., fountain).
- Awnings shall be constructed of metal, glass or natural canvas fabrics. Vinyl, synthetic fabric, plastic or backlit awnings are prohibited. ~~Signage or lettering on awnings is prohibited.~~

Proposed amendment allows signage on awnings. The current graphic illustrating this code requirement includes signage.

Example: Desirable Weather Protection-Canvas Fabric Awning



Existing graphic with new caption.

Example: Desirable Weather Protection – Metal and Glass Awning



New graphic showing metal and glass awning

BUILDING – FACADES

Guideline – All buildings shall contribute to the storefront character and visual relatedness of town center buildings

Description – Facades should define a continuous street edge, while adding visual interest and variety for the pedestrian. Building frontages should provide a sense of continuity and enclosure to the street, creating a human-scale “street wall.”

Track 2 – Design Guidelines

Recommended

- Walls that have a comfortable rhythm of bays, columns, pilasters or other articulations.
- Facades should be taller than one -story to create a sense of enclosure along the sidewalk.
- Architectural elements such as towers, roof parapets.
- Well-detailed cornices of significant proportions to create visual interest and shadow lines.
- Vertical elements ~~which~~ that break up long, monolithic building facades along the street.
- Regularly spaced and similar-shaped windows with window trim on all building stories.
- Bay windows on second story or higher floor levels.
- Screen mechanical equipment.

Not Recommended

- Blank walls at street level.
- Unarticulated rooflines.
- Exposed rooftop mechanical equipment, electrical units or telecommunication equipment.

Track 1 – Development Standards

Related Code Standards

- All new ~~structures~~ buildings in the TCC zone shall be a minimum of two -stories in height or a minimum height of 24 feet. All new buildings in the TCT zone shall be a minimum of 16 feet in height. Building height shall be measured at the front elevation to top of parapet or eave line of lowest point of facade.
- All flat-roofed buildings shall have a decorative cornice at top of building (parapet)
- Exterior pilasters and columns shall project a minimum of 6 inches beyond building face.
- All rooftop mechanical equipment shall be screened by a solid wall from view of the public right-of-way and pedestrian routes.

The minimum height in the TCC zone is two stories. The minimum height in TCT and TCS is 16’. The minimum height requirement should apply to buildings only, not all structures (e.g., fences, walls, poles, etc. should not be subject to minimum height).

Examples: Desirable Building Façade and Design Elements



New graphic showing newer building with desired façade elements.

Updated caption.



Delete existing graphic.

Recommended: Façade articulation

BUILDING – RETAIL STOREFRONTS

Guideline – Storefronts should appear open, inviting and engaging to the passerby.

Description – Retail and other commercial establishments should provide windows and doors offering views in and out of the building so passersby may see activity within buildings.

Track 2 – Design Guidelines
Recommended

- Storefronts should be designed to encourage a lively streetscape with clear windows, window displays.
- Clerestory or transom windows above storefronts are recommended.
- Entryways with multiple doors, windows, architecture details and ornate hardware.
- Sliding, overhead or other operable windows for restaurants or other active uses.

Not Recommended

- Blank walls.
- Windows which that are too small to provide views in and out.
- Tinted, opaque or reflective glazing on windows or doors.
- Solid metal or wood doors with small or no windows.
- Featureless entryways (doors flush with building facades, no architectural details, etc.)
- Windows with residential-type multiple lites or divisions.
- Windows that have a greater horizontal than vertical dimension.

Track 1 – Development Standards
Related Code Standards

- Ground floor storefront glazing along the primary public façade shall comprise a minimum percentage of the main floor’s exterior wall area (measured between 2 feet above sidewalk to 10 feet above sidewalk) as follows:
 - TCC: 80%
 - TCT & TCS: 50%
- First floor vertical elements such as columns or pilasters shall be provided and spaced center-to-center at a maximum of 25 feet apart.
- Doors on the main floor façade facing a street shall contain windows equivalent in size to 50% of door surface area.
- Storefront glazing must be transparent. Reflective, tinted, glazed or techniques that obscure more than 20% of glazed surfaces shall be prohibited.
- Glazing dimensions shall have a greater height than width
- Storefront glazing with divided lites shall be limited to transom windows only.
- All window frames shall be painted.

Proposed amendments delete TCS zone. As noted in Development Code the methodology has been clarified.

Examples: Retail Storefront Windows and Active Streetscape



Added new graphics with caption

BUILDING – WINDOWS

Guideline – Use windows to create an open and inviting atmosphere.

Description - Retail uses should provide windows that create visual interest and encourage people to visit restaurants and shops. Blank walls should be avoided.

Track 2 – Design Guidelines

Recommended

- The majority of the ground floor building façade should contain windows.
- Multiple windows should be provided on the front façade above the main floor in a uniform pattern.
- Window should be oriented vertically with rectangular shapes.
- Frame openings with trim around windows and doors.
- If used, door and window shutters should be sized to cover the entire window.
- “Punched” window openings recessed rather than flush with the building façade.

Not Recommended

- Tinted or reflective glass and glass block.
- Spandrel or other contemporary glazing (i.e., glass surface smoothly and uniformly covering structural elements).

Track 1 – Development Standards

Related Code Standards

- Window openings shall comprise the following portions of the front building facades at the main floor:
TCC: Minimum 80%
TCS: ~~Minimum 30%~~
TCT: Minimum ~~60%~~ 50%
- Window openings shall comprise the following portions of the front building façade above the main floor:
TCC: Minimum 30%, maximum 80%
TCS: ~~Minimum 20%, maximum 80%~~
TCT: Minimum 30%, maximum 80%
- Clear or transparent glazing is required for windows fronting the public rights-of-way.
- Glass shall be recessed a minimum of 1-1/2 inches from the surrounding exterior wall surface.
- Spandrel, glass curtain-wall or any window wall glazing that creates an opaque, flat or featureless, or reflective surface shall not be used at ground floor.

Amended to delete TCS zone.

TCT amended to be consistent with 50% required by “Building – Retail Storefronts”)

Example: Inviting Ground Floor Retail and Upper Floor Windows

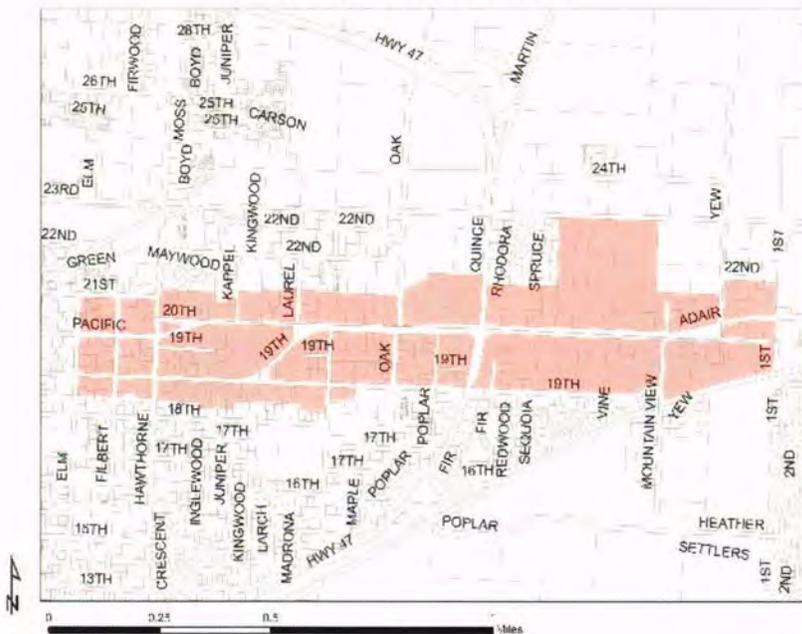


New graphic and caption added.

<p>BUILDING – EXTERIOR WALLS</p> <p>Guideline – Use materials which <u>that</u> are compatible with the character of Forest Grove and create a sense of permanence.</p> <p>Description – Materials used should be consistent with the existing buildings in the Town Center. Quality wall materials provide a sense of permanence and should be applied to create a sense of substance and mass.</p>		
<p>Track 2 – Design Guidelines</p> <p><u>Recommended</u></p> <ul style="list-style-type: none"> Quality materials such as brick, stone and natural wood. Applications which <u>that</u> create depth, such as recessed windows and doors. Establish a single clearly dominant exterior wall material and finish. Belt courses and medallions. Consider the following materials: TCC: Brick or stone masonry. TCT & TCS: Brick or stone masonry; cement-based stucco; lap siding; board and batten siding; shingles and shakes. <p><u>Not Recommended</u></p> <ul style="list-style-type: none"> Brick or stone veneer Vinyl or metal siding. Flagstone, simulated river rock or other similar veneer cladding. Simulated stucco cladding (such as EIFS/Dryvit) at ground level. Concrete masonry block at ground level. 	<p>Track 1 – Development Standards</p> <p><u>Related Code Standards</u></p> <ul style="list-style-type: none"> Vinyl, plastic or metal siding are prohibited the all Town Center zones. Synthetic Stucco (EIFS, Dryvit, etc.) insulating cladding materials along the first floor of facades that front public rights-of-way are prohibited. Flagstone or other stone veneer along the first floor of facades that front public rights-of-way are prohibited. Simulated or cultured stonework are prohibited for commercial uses. Wood, asphalt or cement shingles are prohibited at first floor for commercial uses. 	<p>Amended to delete TCS zone.</p>

Section II – Commercial Corridor Focus Area

Commercial Zoning Map



Commercial Corridor map added. Map shows proposed western boundary at Elm Street, which reflects the rezoning of the Town Center Expansion Area from CC to TCT.

No other changes to the Commercial Corridor Guidelines proposed.

Section III Multi-Unit Residential Focus Area	
<p>Applicability The City has a variety of areas which <u>that</u> are designated for multi-unit housing. Primarily, M-multi-unit housing is focused on land zoned either:</p> <ul style="list-style-type: none"> • Residential Multi-unit Low (RML). • Residential Multi-unit High (RMH). <p>Land zoned Community Commercial (CC) and <u>Neighborhood Mixed-Use (NMU)</u> also allows multi-unit development. The design standards and guidelines in this section apply to multi-unit development in these three <u>four</u> districts, with multi-unit being defined as buildings which <u>that</u> include at least three dwelling units. <u>However, within the NMU zone, these standards may be modified through an approved Mixed-Use Plan Development (MUPD).</u></p> <p>While multi-unit residential development is also allowed in Town Center zones, its character in those areas can be significantly different from that of the Commercial Corridor and residential zoning districts, but is not subject to the standards in this section. Therefore, residential development in the Town Center is subject to the Town Center guidelines found in Section I.</p> <p>In the RML, RMH, and <u>CC, and NMU</u> zones, these multi-unit residential design guidelines apply to the multi-unit types as follows:</p> <ul style="list-style-type: none"> • Single-family attached developments with three or more units (i.e. townhouse, courtyard). • Duplex developments with at least two duplexes (i.e. courtyard/loft/urban housing). • Apartment buildings with three or more units (i.e. courtyard/loft/urban housing). <p>The following guidelines section is divided into four categories, with sub-categories provided as necessary within each. Each category has a set of corresponding design objectives, guidelines and standards:</p> <ul style="list-style-type: none"> • Site • Building • Lighting • Signs 	<p>The proposed amendments assume that MF within NMU zone will occur as part of a MUPD. The standards and guidelines in this section are the “default” if MUPD applicants do not want to develop their own Guidebook.</p>
<p>Map of Zones which Allow Multi-Unit Residential Development</p>	<p>Map reference deleted, previous section outlines which zones are subject to the guidelines.</p>

SITE – PEDESTRIAN ENVIRONMENT

Guideline – Create safe, pleasant and active streets and public spaces.

Description – Pedestrian areas should be functional and inviting, providing the multi-unit residential development with amenities. These areas should enhance the feeling of community and increase the ability for neighbors to meet. Development of public courts, plazas and squares is encouraged.

Track 2 – Design Guidelines

Recommended

- Provide pedestrian amenities such as benches for a pleasant and inviting place to sit, rest, linger and meet.
- Safely separate pedestrian areas from vehicle traffic by providing street trees along sidewalks as buffers.
- Provide an active ground floor environment along all pedestrian streets and public courts, plazas and squares. That is, design buildings with transparent windows and doors.
- Develop public spaces – plazas, squares or other open spaces that are “activated” by building entries and locate uses that encourage foottraffic.

Not Recommended

- Pedestrian-only streets or corridors.
- Indirect pedestrian routes.
- Pedestrian routes blocked by obstructions or barriers including mechanical units or utility, sign or light poles.
- Public spaces that are not “outdoor rooms” –i.e., that are not enclosed by building edges and landscaping on all sides.
- Public plazas or squares in parking lots.
- Pedestrian-inhospitable areas such as public plazas or squares at intersections of arterial roadways.
- Use of public spaces primarily as

Track 1 – Development Standards

Related Code Standards (Section 10.8.140(A)(3))

- Residential doors ~~which that~~ face a public right-of- way shall be setback a minimum of ~~2 feet~~ 3 feet from a public sidewalk plus any additional width needed to meet the minimum sidewalk requirements (where applicable).
- The following types of doors shall not provide the primary entrance into a dwelling unit: sliding glass or solid metal doors without glazing.
- Residential stoops, porches or terraces shall be no higher than raised a maximum of 34 feet above grade measured at the adjacent right-of-way.
- Residences with entry porches or terraces shall have a minimum area of ~~60~~ 40 square feet.
- Residential entries shall be no lower than lowered a maximum of 4 feet below grade measured at adjacent right- of-way.

Proposed amendments for consistency with amendments to Dev Code.

Amendments allow greater flexibility by allowing a higher maximum for the porch or stoop and a smaller minimum for a smaller entry porch.

Example: Residential Entries Oriented to Sidewalk and Transitions



New graphic with caption.

SITE – LANDSCAPING

Guideline – Use landscaping to enhance character and visual quality of residential developments.

Description – Landscaping can help provide a quality environment for residents and help achieve compatibility with surrounding land uses. A mix of landscaping treatments and techniques can provide appropriate separation between adjacent properties, screen objectionable views, provide visual relief, and create weather protection.

Track 2- Design Guidelines

Recommended

- Plant landscaping to define and accentuate entry-ways.
- Provide landscape elements along all exterior walls to soften the visual impact of the building and promote residential character of the site.
- Use landscaping and/or fencing to buffer multi-unit development from abutting properties and enhance privacy of dwelling units, especially by landscaping setback areas.
- Distribute canopy trees along roadways; provide landscape buffers along edges of developed areas and throughout parking areas.
- Large species street trees should be planted to provide shade and emphasize the residential character of the site.

Not Recommended

- Removing significant trees.
- Landscaping that creates a pedestrian physical barrier between walkways, gathering areas and building entries.
- Use of non-native-incompatible, noxious or invasive plant materials.
- Parking lot landscaping that creates pedestrian physical or visual barriers between walkways and building entries
- ~~Canopy trees that drop excessive amounts of seeds, fruit or leaves onto cars and walkways.~~

Track 1- Development Standards

Related Code Standards (Section ~~10.7.055~~)

- Landscaping standards vary depending on zoning, use, adjacent zoning, and existing site conditions.
- At least 75% of required landscaped area shall be planted with any suitable combination of trees, shrubs, or evergreen ground cover.
- A maximum of 25 % of required landscaped area may include architectural features or artificial ground covers such as sculptures, benches, masonry or stone walls, rock groupings, ~~bark dust~~, decorative hard paving and gravel areas interspersed with planting areas.
- Buffering and screening is required as specified in Section ~~10.7.075~~ 10.8.425.

Delete bark dust as a permissible landscape coverage material.

Recommend deleting last bullet. Unless the tree is within the public right-of-way, staff recommends it be left to the property owner to decide type of tree to plant.

BUILDING – COMPATIBILITY

Guideline - Encourage design which that provides visual linkages between proposed development and adjoining existing buildings.

Description - Much of the land zoned for multi-unit development is currently occupied by single-family homes. New multi-unit development should “fit” by relating to surrounding architecture through use of common elements of scale, color, rhythm and proportion in similar ways as existing buildings.

Track 2- Design Guidelines

Recommended

- Design buildings to provide an appropriate transition between new buildings and adjacent buildings.
- Use architectural elements and façade materials consistently throughout the site.
- Locate and treat entries similar to that of existing adjacent or neighborhood buildings (for example, providing roof, awning, or portico)
- Use similar landscaping, setback, spacing and orientation of buildings that strengthens and contributes to character and identity of the neighborhood.
- Use similar façade materials, colors and treatments of existing buildings.
- Repeat, strengthen and expand upon building elements that create a unified street edge – i.e., window placement, belt coursings, etc.

Not Recommended

- Literal interpretation of existing buildings.
- Surface materials, building textures or color that vary greatly with nearby development.
- Proportions of windows, doorways, bays and other features that vary greatly with nearby development.

Task 1 – Development Standards

Related Code Standards

- Setback requirements vary by building height (see Section 10.3.035 et al)
- All buildings shall incorporate a porch or architecturally defined entry space for each ground level dwelling unit with a minimum area of 16 square feet per dwelling unit, with no dimension less than 4 feet. Shared porches or entry spaces are permitted. All grade level porches shall include hand-railing, half-walls, or shrubs to define their outside perimeter.
- ~~Common entrances shall not serve more than four (4) dwelling units.~~

Deleted common entrance requirement due to not being practical with buildings served by elevators.

Date: December 16, 2015
To: Project Management Team - Forest Grove Mixed-use and Town Center Zoning
From: Cathy Corliss, Principal; CJ Doxsee, Planner
cc: Anne Sylvester, SCJ Alliance
Re: Code and Map Amendments and Findings (Task 6.2)¹

PROJECT DESCRIPTION

The City of Forest Grove has recently adopted a new comprehensive plan. This code assistance project is intended to implement some of its key elements including provisions for mixed use and Town Center zoning and increasing residential densities through a series of development code and zoning map amendments which will affect different areas within the City of Forest Grove.

TEXT AMENDMENTS

Text amendments are shown in:

- Exhibit A – Amendments to the City of Forest Grove Development Code
- Exhibit B - Amendments to the City of Forest Grove Design Guideline Handbook

The amendments are presented in two column format. The first column shows potential amendments to the code. Additions to the code are shown in double-underline and deletions are shown in ~~strike-through~~. The second column provides commentary and questions for the Planning Commissions' consideration.

In summary, these amendments are intended to accomplish the following:

¹ *This project is partially funded by a grant from the Transportation and Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Moving Ahead for Progress in the 21st Century (MAP-21), local government, and the State of Oregon funds.*

The contents of this document do not necessarily reflect views or policies of the State of Oregon.

EXHIBIT B

- Establish a new Neighborhood Mixed Use (NMU) zone and Mixed Use Planned Development (MUPD) standards to implement the Mixed Use designation of the Comprehensive Plan.
- Consolidate the Town Center Support (TCS) zone into the Town Center Transition (TCT) zone and make minor adjustments to the TCT zone.
- Increase the maximum residential density from 20.28 to 40 dwelling units per acre in both the TCC and TCT zones and provide a residential density bonus up to 100 dwelling units per acre through building amenity and affordable housing incentives. The proposed increase in the base zone density is consistent with the City's Residential Capacity Analysis completed as a part of the Comprehensive Plan update (January 2014). In that analysis, the City of Forest Grove committed to adopting the necessary Zoning Map and Development Code text to achieve residential capacities planned for the Town Center within one year from DLCD approval of the Forest Grove Comprehensive Plan.
- Increase the maximum residential density in the Community Commercial (CC) zone from 20.28 to 30 dwelling units per acre.
- Amend the definition of "Net Acres" to reduce the types of land that must be subtracted from the gross area of the site when calculating net acres. Net acres is used to calculate density; thus, increasing net acres may potentially increase the resulting number of dwelling units that can be permitted. Rights-of-way through or on the edge of the site, environmentally constrained areas, and land intended for public ownership (such park and open spaces uses) will still be subtracted from gross acres to calculate net acres in the proposed new definition.

MAP AMENDMENTS

MIXED USE AREAS

The three mixed use areas are proposed to have the new Neighborhood Mixed Use (NMU) zoning applied with the purpose of encouraging the placement of complementary land uses in close proximity to promote complete neighborhoods. The NMU zone will allow a variety of housing types to be developed. The NMU zones will also allow for a wide range of non-residential uses within a designated "Village Center" and as part of a Mixed Use Planned Development (MUPD) review.

MIXED USE AREA 1 (DAVID HILL)

The David Hill site is currently zoned as Single-Family Residential (R-10). The 8.5 gross acre area proposed to be rezoned at this time has approximately 2.9 net acres of developable land, due in part to the presence of environmental feature constraints. It is at the northwest corner of David Hill Road and Thatcher Road. The remaining Mixed Use area north of David Hill Road (approximately 20.6 gross acres) will be addressed as part of the Westside Planning Project. This area will likely be recommended for rezoning to Single Family Residential (R-10).



MIXED USE AREA 2 (CPD AREA)

The CPD Area is currently zoned Commercial Planned Development, a now defunct zone since the adoption of the Comprehensive Plan. The CPD area is parcelized and has existing non-conforming development. The total net developable acres on all the sites is approximately 5.4 acres.

MIXED USE AREA 3 (DAVIDSON SITE)

The Davidson Site is currently zoned Industrial (IND). It is a large and level site under one ownership with visibility from Highway 47. The Davidson Site has approximately 19.9 net acres of developable land.

The residential density and minimum and maximum amount of commercial and institutional uses permitted vary by area as shown in the tables below.

Residential Density

NMU Zoned Area	Minimum Density	Target Density	Maximum Density
Area 1 - David Hill	9.6 units/net acre	12 units/net acre	13.8 units/net acre
Area 2 - Gales Creek	6.97 units/net acre	8.71 units/net acre	10.02 units/net acre
Area 3 - Davidson	9.6 units/net acre	12 units/net acre	13.8 units/net acre

Commercial/Institutional Uses within the Village Center

NMU Zoned Area	Minimum Square Footage	Maximum Square Footage
Area 1 - David Hill	None	15,000 SF Gross Floor Area
Area 2 - Gales Creek	None	25,000 SF Gross Floor Area
Area 3 - Davidson	25,000 SF Gross Floor Area	130,000 SF Gross Floor Area

TOWN CENTER

TOWN CENTER CONSOLIDATION – TCS TO TCT

As described above, the proposed amendments consolidate the Town Center Support (TCS) zone into the Town Center Transition (TCT) zone and make minor adjustments to the TCT zone. This change requires that all land zoned TCS be rezoned to TCT.

TOWN CENTER EXPANSION AREA

The Community Commercial (CC) area directly east of the Town Center is proposed to be rezoned to TCT. The transition from CC to TCT will encourage more urbanized development to occur by restricting new auto-oriented development, reducing landscape requirements, applying “urban” design standards, and allowing residential uses in mixed use or stand-alone development.

Summary of Proposed Map Amendments

Location	Comp Plan Designation	Current Zoning	Proposed Zoning	Gross Acreage Proposed to be Rezoned
Mixed Use Area 1 (David Hill)	Mixed Use	R-10	NMU	8.5 acres
Mixed Use Area 2 (CPD Area)	Mixed Use	CPD	NMU	6.4 acres
Mixed Use Area 3 (Davidson Site)	Mixed Use	LI	NMU	23.5 acres
Town Center Consolidation	Town Center	TCS	TCT	22.6 acres
Town Center Expansion Area	Town Center Expansion	CC	TCT	24.2 acres

PUBLIC INVOLVEMENT

A series of community events, public hearings, and stakeholder interviews, allowing for citizen involvement, have been provided throughout the course of developing the proposed amendments. Summarized below are the dates and events allowing for citizen involvement.

Event	Date
Stakeholder Interviews	January 2015
Community Meeting #1	March 2015
Stakeholder Meetings	June 2015
Planning Commission Worksession #1	June 2015
Planning Commission Worksession #2	August 2015
Community Meeting #2	September 2015
Planning Commission Worksession #3	September 2015
Planning Commission/City Council Worksession	November 9, 2015
Planning Commission Hearing	January 4, 2016
City Council Hearing	February 8, 2016 (First Reading of Ordinance) February 22, 2016 (Second Reading of Ordinance)

APPROVAL CRITERIA

All of the proposed actions are legislative subject to a Type IV review. The approval criteria and procedures applicable to text and map amendments are shown below. Findings in response to these criteria are provided in the subsequent sections.

DEVELOPMENT CODE TEXT AMENDMENT

10.2.630 REVIEW CRITERIA

- A. *The text amendment is consistent with relevant goals and policies of the Forest Grove Comprehensive Plan;*
- B. *The text amendment is consistent with relevant statewide and regional planning goals, programs and rules.*

MAP AMENDMENT (ZONE CHANGE)

10.2.770 REVIEW CRITERIA

- A. *The zone change is consistent with the Comprehensive Plan Map. When the Comprehensive Plan has more than one implementing zone as shown on the Correspondence Table in Article 3, it must be shown that the proposed zone is the most appropriate, taking into consideration the purposes of each zone and the zoning pattern of surrounding land.*
- B. *The zone change is consistent with relevant goals and policies of the Comprehensive Plan, as identified by the Director.*
- C. *The site is suitable for the proposed zone and there is a lack of appropriately designated alternative sites within the vicinity. The size of the vicinity will be determined on a case-by-case basis since the impacts of a proposed zone and its potential uses vary. The factors to be considered in determining suitability are parcel size and location.*
- D. *The zone change is consistent with the adopted Transportation System Plan. Development allowed by the zone change will not substantially impact the functional classification or operation of transportation facilities, or reduce the level of service of transportation facilities below the minimum acceptable level identified in the Transportation System Plan. To ensure proper review and mitigation, a traffic impact study may be required for the proposed zone change if it may impact transportation facilities.*

- E. Public facilities and services for water supply, sanitary waste disposal, stormwater disposal, and police and fire protection are capable of supporting the uses allowed by the zone. Adequacy of services is based on the projected service demands of the site and the ability of the public services to accommodate those demands.*
- F. The establishment of a zone district is not subject to the meeting of conditions.*

FINDINGS - TEXT AMENDMENTS (SUBJECT TO 10.2.630)

The City of Forest Grove adopted a new Comprehensive Plan in 2014. This TGM Code Assistance project and the proposed text amendments are intended to implement some of its key elements including provisions for mixed use and Town Center zoning and increasing residential densities. Comprehensive Plan housing, economic development, urbanization, and transportation goals and policies are relevant to the proposed development code text amendments. Relevant goals and policies related to the proposed changes are addressed.

MIXED USE AREAS

Review Criteria 10.2.630.A: The text amendment is consistent with the relevant goals and policies of the Forest Grove Comprehensive Plan.

Finding: The relevant goals and policies are addressed below.

Community Sustainability Goals

Goal 7: Promote interconnected land uses that encourage diverse, accessible, and proximate land uses that promote active living and access to vital services including employment, education, and healthy food.

Goal 8: Create complete neighborhoods, through land use regulations, with housing, recreational opportunities, retail, services and employment nearby.

Goal 11: Encourage the clustering of residential development in the David Hill area to reduce impacts to the environments and minimize degradation of views from lower elevations especially the Town Center.

Finding: A new "Neighborhood Mixed Use (NMU)" zone is proposed to be added to Article 3, Commercial Zones, the purpose of which is to ensure sites are developed into pedestrian-friendly mixed use neighborhoods. The zone allows for a mix of housing types and commercial uses. A new Mixed Use Planned Development (MUPD) is proposed to be added to Article 4, Overlay Districts, which will work in conjunction with the NMU zone. The MUPD standards

ensure that land uses are interconnected and pedestrian-oriented by establishing standards for densities, desired building mix, connectivity, building orientation, and open space requirements. A portion of the David Hill area is proposed to be rezoned to NMU. Clustering is allowed in a MUPD and the standards require that water features such as streams or ponds be left in a natural state unless altered to improve the natural values of the water feature.

Housing Goals and Policies

Goal 3: Promote mixed-use development opportunities throughout the community

Policy 3.1 Identify locations on the Comprehensive Plan and Zoning maps for mixed-use development opportunities. Establish standards for residential and commercial densities, desired building mixed, and building design for mixed-use areas.

Finding: A new “Neighborhood Mixed Use (NMU)” zone is proposed to be added to Article 3, Commercial Zones, and would allow for a mix of housing types. The City is required to provide clear and objective standards for “needed housing”. However, mixed use areas, while potentially providing opportunities for additional housing within the City of Forest Grove, were not identified in the City’s 2009 Economic Opportunity Analysis and Buildable Lands Inventory as necessary to provide “needed housing”. The NMU zone provides development standards for densities, desired building mix, and building design through a “Mixed Use Planned Development” overlay, proposed to be added to Article 4.

Goal 6: Promote neighborhoods complete with residences, open space, schools, parks, and shopping opportunities within close proximity to each other. Avoid stand-alone residential development lacking support activities.

Policy 6.1 Designate small-scale neighborhood-oriented commercial areas within walking distance of residential areas.

Finding: The purpose of Neighborhood Mixed Use zones would be to develop as a pedestrian friendly neighborhoods, with a diversity of housing types, be pedestrian oriented, include a minimum amount of open space, and provide opportunities for neighborhood-scale retail. In addition, the three mixed use areas are distributed throughout the City of Forest Grove and generally adjacent to areas zoned Single Family (R-5 or R-7), which would increase the opportunities for residents in these neighborhoods to access neighborhood amenities through non-automotive means of transportation. The amount of commercial development is capped within each of the mixed use areas in order to help ensure they remain neighborhood-oriented in scale.

The NMU zone with associated MUPD overlay will allow a variety of additional housing to be built to provide options for Forest Grove residents. All the areas with the proposed NMU designations will have the flexibility to develop a mix of housing types, including single-family housing, apartments, row houses, senior housing, etc. as long as the minimum residential density targets are met. In addition, developments in areas with NMU designations will be required to provide basic amenities such as open space, pedestrian facilities, and parking, to support housing development.

Land Use Goals and Policies

Commercial Land Use Objective 1: Distribute commercial activity throughout the city to serve existing and planned residential or employment areas.

Finding: The NMU zone will allow for commercial activity in areas of the community where no commercial land is currently designated. As such, the NMU zone provides the opportunity to distribute commercial activity throughout the City to serve existing and planned residential or employment areas as identified on the Comprehensive Plan Map.

Commercial Land Use Objective 12: The Comprehensive Plan Map and corresponding zoning standards shall provide for commercial development opportunities serving newly developing areas in the City.

Finding: The NMU zone and associated MUPD overlay will promote commercial uses in areas of the City where none currently exists. This includes the Sunset Drive/Hwy. 47 and David Hill Road/Thatcher Road areas.

Economic Development Goals & Policies

Goal 5: Promote Retail Activities

Policy 5.3 Promote opportunities for mixed-use development, including retail, near major transportation intersections (nodes) within the City including the Forest Grove Town Center.

Policy 5.4 Adopt development standards to encourage the creation of commercial areas at a scale proportionate for meeting the daily needs of nearby residents.

Finding: The MUPD Overlay, applied to NMU zones, would include review procedures and development standards for allowing commercial areas (defined as Village Centers) to be developed. The review procedures, Type III for preliminary plan and Type I for final plan, would provide greater flexibility in the development of commercial areas to meet the needs of nearby

residents. In addition, development standards for Village Centers would regulate the maximum square footage and building footprint. The text amendments for the NMU zone addresses a need identified in the City’s 2009 Economic Opportunity Analysis related to retail sales leakage. The EOA States “The City of Forest Grove’s estimated retail sales are significantly below originating sales by a sizable margin, reflecting the City’s position as a bedroom community, failing to capture a typical share of general retail spending, particularly Food Services. In other words, residents in Forest Grove spend a sizable share of their retail dollars outside Forest Grove.” The NMU areas are located in areas of the community not located near shops or services. As such the NMU zone is intended to provide opportunities to capture retail dollars currently being spent outside the community.

Urbanization Goals & Policies

Goal 4: Implement policies to create complete neighborhoods in areas undergoing urbanization.

Finding: The NMU zone is proposed to encourage the creation of complete neighborhoods with a variety of housing choices and the opportunity to obtain goods and services nearby. The NMU zone text amendment establishes the framework necessary to implement the NMU comprehensive plan and zoning designation. Without the ability to implement the NMU zoning designation the opportunity to encourage the creation of complete neighborhoods is lost.

Transportation Goals & Policies

Goal 6: Establish and maintain a context sensitive set of transportation design and development regulations

Policy 6.3 Require developers to include pedestrian, bicycle, and transit-supportive improvements within proposed developments and to adjacent right-of-way in accordance with adopted policies and standards.

Finding: The MUPD Overlay’s purpose, applied to NMU zones, is to create pedestrian-friendly mixed use neighborhoods which provide pedestrian and bicycle access to, and through, the site and provide connectivity to adjacent areas for motorized and non-motorized modes of transportation. Development plans for pedestrian and bicycle access are required to include a transportation system that emphasizes pedestrian mobility and accessibility that provides connections within the property and to adjacent properties. Proposed commercial and institutional uses that are located in the designated “Village Center” are required to be compact and pedestrian friendly and are encouraged to use Forest Grove’s Town Center Design Guidelines as a basis for development. As described in Exhibit C (Transportation Analysis), the

proposed intensities and densities allowed by the proposed NMU zone and associated MUPD overlay are consistent with the City's adopted Transportation System Plan.

Review Criteria 10.2.630.B: The text amendment is consistent with relevant statewide and regional planning goals, programs and rules.

Regional Planning Goals

Metro Regional Framework Plan

Introduction: It is the policy of the Metro Council to exercise its powers to achieve the following six outcomes, characteristics of a successful region:

1. People live, work and play in vibrant communities where their everyday needs are easily accessible....

Finding: The proposed new NMU zone and associated MUPD overlay are intended to facilitate the development of complete neighborhoods that offer a mix of uses in a pedestrian friendly environment where residents can walk to the village center.

Policy 1.1 Compact Urban Form

1.1.1. Ensure and maintain a compact urban form within the UGB.

1.1.7. Promote excellence in community design.

Finding: The proposed new NMU zone and associated MUPD overlay establish minimum densities which are consistent with development within the UGB. The efficient use of land and building styles that establish a cohesive sense of place are stated purposes of the MUPD and required to be addressed as approval criteria.

Policy 1.3 Housing Choices and Opportunities

Finding: The proposed new NMU zone will increase housing choices and opportunities in Forest Grove through regulations that allow new housing to be developed. The associated MUPD overlay will ensure new development provides a range of diverse housing, including single family house, apartments, row houses, cottages, senior housing, etc. in a compact urban form. Standards require a mix of housing types for MUPDs over 3 acres.

1.10 Urban Design

Policy 1.10.1 Support the identity and functioning of communities in the region through:

c. Ensuring that incentives and regulations guiding the development and redevelopment of the urban area promote a settlement pattern that:

iv) Reinforces nodal, mixed-use, neighborhood-oriented community designs to provide walkable access to a mix of destinations to support meeting daily needs, such as jobs, education, shopping, services, transit and recreation, social and cultural activities.

Finding: The proposed new NMU zone and associated MUPD overlay are intended to facilitate the development of complete neighborhoods that offer a mix of uses in a pedestrian friendly environment where residents can walk to the village center. Standards emphasize compact urban forms with access to, and through the area.

Metro Urban Growth Management Functional Plan

Title 1: Housing Capacity

Finding: The proposed new NMU zone and associated MUPD overlay will allow the development of mixed use areas at urban densities and intensities, helping ensure that the City meets its regional housing target.

Statewide Planning Goals

Goal 1, Citizen Involvement – To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

Finding: As described above, an extensive public outreach program was conducted as a part of this project including: community events, public hearings, and stakeholder interviews. These allowed for citizen involvement, throughout the course of developing the proposed amendments.

Goal 9, Economic Development – To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon’s citizens.

Finding: The proposed amendments to add the NMU zone with associated MUPD overlay will help promote economic development by allowing a mix of neighborhood-scale and pedestrian-oriented quality residential and commercial development to occur. All of the areas with the proposed NMU designations will have the flexibility to develop a mix of housing types as long as the minimum residential density targets are met. In addition, the two smaller areas with the proposed NMU designation will have the available option to develop a mix of commercial and institutional uses within designated “Village Centers” up to a maximum square footage. The third and largest of the areas is proposed to have a minimum square footage requirement, ensuring a

minimum amount of commercial or institutional uses are developed to support the surrounding areas.

Goal 10, Housing – To provide for the housing needs of citizens of the state.

Finding: NMU zoned lands, while potentially providing opportunities for additional housing within the City of Forest Grove, were not identified in the City’s 2009 Economic Opportunity Analysis and Buildable Lands Inventory as necessary to provide “needed housing”. While areas designated as Mixed Use in the Comprehensive Plan are not required to provide “needed housing”, the proposed amendments to add the NMU zone with associated MUPD overlay will allow a variety of additional housing to be built. All the areas with the proposed NMU designations will have the flexibility to develop a mix of housing types, including single-family housing, apartments, row houses, senior housing, etc. as long as the minimum residential density targets are met. In addition, developments in areas with NMU designations will be required to provide basic amenities such as open space, pedestrian facilities, and parking, to support housing development.

Goal 12, Transportation – To provide and encourage a safe, convenient and economic transportation system.

Finding: As described in Exhibit C (Transportation Analysis), the proposed intensities and densities allowed by the proposed NMU zone and associated MUPD overlay are consistent with the City’s adopted Transportation System Plan.

Goal 14, Urbanization – To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities.

Finding: As noted above, the proposed NMU designations will allow for the development of new livable neighborhoods to be built within Forest Grove’s Urban Growth Boundary (UGB). The additional opportunities are being provided within the UGB, where City services and amenities are best provided, will help decrease the demand for housing outside of the UGB in more rural areas, and help preserve valuable forest and agricultural land.

TOWN CENTER

Review Criteria 10.2.630.A: The text amendment is consistent with the relevant goals and policies of the Forest Grove Comprehensive Plan.

Community Sustainability Goals

Goal 6: Foster excellence in the design of public and private development projects to minimize environmental impacts, maximize financial efficiency, optimize social equity and benefits, and improve public health.

Goal 7: Promote interconnected land uses that encourage diverse, accessible, and proximate land uses that promote active living and access to vital services including employment, education, and healthy food.

Goal 8: Create complete neighborhoods, through land use regulations, with housing, recreational opportunities, retail, services and employment nearby.

Finding: Town Center zones in the Development Code allow for residential development to occur in an area generally considered to be an existing complete neighborhood, with access to retail, services, transit, etc. The proposed amendments to the Town Center zones would increase the maximum residential density from 20.23 to 40 dwelling units per acre allowed outright, and to 100 dwelling units per acre with density bonuses. The increase in residential capacity will allow for more residents to live in the Town Center where they will be able to access existing neighborhood amenities and transit. The proposed density incentives promote and reward excellence in design and maximize social equity by encouraging affordable housing.

Housing Goals and Policies

Goal 2: Provide incentives for increased residential development densities within the Forest Grove Town Center

Policy 2.1 Establish incentive programs to leverage local resources with private investments. Incentive may take the form of direct financial participation (grants or loans), or indirect participation such as land write-downs.

Policy 2.3 Amend Development Code standards to increase maximum development densities within the Forest Grove Town Center, identified high capacity transit station areas, and mixed-use target areas along the Pacific Avenue commercial corridor.

Finding: Town Center zones in the Development Code currently allow for residential development to occur as part of mixed use developments. Proposed amendments to the Development Code would increase the allowed maximum floor area ratio (FAR) and residential density in Town Center zones. FAR would increase from 3:1 to 4:1, while the outright allowed residential density would increase from 20.28 to 40 dwellings units per acre. In addition, the maximum residential density can be increased above the proposed outright allowed amount, up to 100 dwelling units per acre, through density incentives. The proposed residential density incentive is intended to allow significantly higher densities, while ensuring that livability is preserved and sustainability is encouraged, by allowing developments to include affordable housing and building amenities that exceed minimum design standards.

Goal 5: Develop and implement standards for sustainable neighborhood development.

Policy 5.1 Encourage the use of Leadership in Energy and Environmental Design (LEED) development practices in subdivisions and residential structures.

Policy 5.2 Encourage the use of energy efficient building materials and practices in the design, construction, and remodeling of housing.

Finding: As noted above, the proposed amendment to the Forest Grove Development Code includes incentives to increase the residential density beyond the maximum allowed outright amount. Density incentives are organized by amenity categories and are heavily focused on sustainable building designs including LEED certification. Notable amenities oriented toward sustainable building designs include: Energy Efficiency, “Green” Materials, Low Impact Design, Residential Gardening, Rooftop Garden or Eco-Roof, Public Plaza/Outdoor Patio/Seating Area, and LEED Certification.

Economic Development Goals & Policies

Goal 5: Promote Retail Activities

Policy 5.3 Promote opportunities for mixed-use development, including retail, near major transportation intersections (nodes) within the City including the Forest Grove Town Center.

Policy 5.4 Adopt development standards to encourage the creation of commercial areas at a scale proportionate for meeting the daily needs of nearby residents.

Finding: The Town Center designations currently allow mixed use developments to occur. The proposed amendments to the Town Center designations will continue to allow mixed use

development to occur and encourage it by increasing the allowed outright minimum residential density and by adding residential density bonus incentives. All development within the Town Center will continue to be subject to the design review process provided in Article 2 and standards provided in Article 8 to ensure commercial and mixed use areas develop a scale proportionate to the residents' needs. Proposed amendments to Article 8 are primarily focused on minor edits for consistency with proposed Town Center and NMU zone amendments. In addition, the City will have the capacity to limit residential density incentives based on the availability of public service, traffic impacts, and compatibility with adjacent single-family residential zoning districts.

Goal 7: Promote the Forest Grove Town Center as the Focal Point of the Community

Policy 7.10 Amend development standards to increase minimum development densities in the Town Center to improve the economic investment climate for residential construction and encourage a variety of housing types.

Finding: The proposed amendments will decrease the minimum floor area ratio (FAR) standard from 0.75:1 to 0.5:1 in limited areas of the Town Center by consolidating TCT and TCS designations. Minimum requirements for building height, residential densities, and setbacks will remain the same. However, the economic investment climate will be improved with the proposed amendments by increasing the maximum allowed residential density and allowing residential density to increase further through density bonus incentives in all Town Center zones.

Urbanization Goals & Policies

Goal 4: Implement policies to create complete neighborhoods in areas undergoing urbanization

Policy 10 The City of Forest Grove will continue to promote the efficient use of land within the Forest Grove Town Center and any areas designated as transit station communities on the Forest Grove Comprehensive Plan land use map.

Finding: The proposed amendments will increase the maximum allowed FAR and residential densities in the Town Center zones and the maximum allowed residential densities in the Commercial Corridor. This will enable development to occur at higher densities, which can take advantage of existing public services and promote the use of transit.

Transportation Goals & Policies

Goal 6: Establish and maintain a context sensitive set of transportation design and development regulations

Policy 6.3 Require developers to include pedestrian, bicycle, and transit-supportive improvements within proposed developments and to adjacent right-of-way in accordance with adopted policies and standards.

Finding: As described in Exhibit C (Transportation Analysis), the proposed intensities and densities allowed by the proposed Town Center text amendments are consistent with the City's adopted Transportation System Plan. The proposed increase in density is intended to allow for transit-supportive densities within the Town Center. In addition, bicycle parking and public plazas are proposed as potential amenities that would qualify for a density incentive.

Review Criteria 10.2.630.B: *The text amendment is consistent with relevant statewide and regional planning goals, programs and rules.*

Regional Planning Goals

Metro Regional Framework Plan

Introduction: It is the policy of the Metro Council to exercise its powers to achieve the following six outcomes, characteristics of a successful region:

1. People live, work and play in vibrant communities where their everyday needs are easily accessible....

Finding: The proposed amendments to expand the Town Center and increase the residential densities in the Town Center and Commercial Corridor will encourage higher-density, mixed use redevelopment to occur. Redevelopment in the Town Center and Commercial Corridor will provide walkable access to a mix of existing complete neighborhood amenities and reduce the need for single-occupancy automotive trips.

Policy 1.1 Compact Urban Form

1.1.3 Facilitate infill and re-development, particularly within Centers, Corridors, Station Communities, Main Streets and Employment Areas, to use land and urban services efficiently, to support public transit, to promote successful, walkable communities and to create equitable and vibrant communities.

1.1.7. Promote excellence in community design.

Finding: The proposed amendments to expand the Town Center and increase the residential densities in the Town Center and Commercial Corridor will encourage higher-density, mixed use redevelopment to occur within a center and a corridor. Redevelopment in the Town Center and

Commercial Corridor will support access to public transit and locate additional residents within walking distance to a wide range of commercial, civic uses, and transit.

Policy 1.3 Housing Choices and Opportunities

Finding: The proposed amendments to expand the Town Center and increase the residential densities in the Town Center and Commercial Corridor will allow for a variety of multi-family development to occur. The residential density incentives will also encourage multi-family development to incorporate affordable housing and/or sustainable building design.

1.10 Urban Design

1.10.1 Support the identity and functioning of communities in the region through:

c. Ensuring that incentives and regulations guiding the development and redevelopment of the urban area promote a settlement pattern that:

v) Includes concentrated, high-density, mixed-use urban centers developed in relation to the region's transit system.

Finding: The proposed amendments to expand the Town Center and increase the residential densities in the Town Center and Commercial Corridor will encourage higher density development within the mixed use urban center in proximity to transit service.

Metro Urban Growth Management Functional Plan

Title 1: Housing Capacity

Title 6: Centers, Corridors, Station Communities, and Main Streets

Finding: The proposed increases in residential density will allow the development within the Town Center at appropriate transit-supportive densities, helping ensure that the City meets its regional housing target.

Statewide Planning Goals

Goal 1, Citizen Involvement – To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

Finding: As described above, an extensive public outreach program was conducted as a part of this project including: community events, public hearings, and stakeholder interviews. These allowed for citizen involvement, throughout the course of developing the proposed amendments.

Goal 9, Economic Development – To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon’s citizens.

Finding: The proposed amendments to consolidate the Town Center zones, increase outright residential density, and add residential density bonus incentives will promote economic development by reducing the complexity of Town Center zoning designations and increasing the residential capacity of areas with the most immediate access to existing services.

Current standards and regulations for Town Center Transition and Town Center Support designations are almost identical. Zoning boundaries for these designations are not uniformly distributed across individual blocks increasing the complexity of applying the development code. The proposed amendments to the development code will consolidate the Town Center Support with the Town Center Transition designation, allowing for the development code to be applied to developments more uniformly while still achieving Forest Grove Town Center goals.

The Town Center and Civic Corridor contains the majority of commercial services within Forest Grove. Increasing the maximum allowed residential density and adding residential density bonus incentives will contribute to the creation of complete neighborhoods in these areas by allowing Forest Grove’s growing population to locate nearby existing commercial services. Allowing Forest Grove’s growing population to locate in these areas will help existing and new businesses to grow.

Goal 10, Housing – To provide for the housing needs of citizens of the state.

Finding: The proposed amendments to increase the maximum allowed residential density in the Town Center and Civic Corridor and the addition of residential density bonus incentives to the Town Center will create additional opportunities and incentives for multi-family housing to be developed.

The increase in the maximum allowed residential density will create additional opportunities for a variety of multi-family housing to be developed at a range of densities.

The residential density bonus provide two tiers of incentives which allow flexibility and incentives for new housing development beyond the base residential density to high quality design and sustainability and/or affordable housing. The first tier of incentives encourage the use project amenities, such as sustainable building design or public plazas. The second tier of incentives encourage the development of affordable housing, where 20% are set aside for qualified residents.

Goal 12, Transportation – To provide and encourage a safe, convenient and economic transportation system.

Finding: As described in Exhibit C (Transportation Analysis), the proposed intensities and densities allowed by the proposed Town Center text amendments are consistent with the City’s adopted Transportation System Plan.

Goal 14, Urbanization – To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities.

Finding: As noted above, the proposed amendments to the Town Center and Civic Corridor designations will allow a variety of additional housing to be built within Forest Grove’s Urban Growth Boundary (UGB) to meet the City’s housing needs as population growth occurs over the next 20 years. The additional capacity to supply housing within the UGB, where City services and amenities are best provided, will help decrease the demand for housing outside of the UGB in more rural areas, and help preserve valuable forest and agricultural land.

FINDINGS - MAP AMENDMENTS (SUBJECT TO 10.2.750 – 10.2.770)

MIXED USE AREAS

Review Criteria 10.2.770.A: The zone change is consistent with the Comprehensive Plan Map. When the Comprehensive Plan has more than one implementing zone as shown on the Correspondence Table in Article 3, it must be shown that the proposed zone is the most appropriate, taking into consideration the purposes of each zone and the zoning pattern of surrounding land.

Finding: The recommended zoning designation for the three mixed use areas is consistent with the Comprehensive Plan Map, as amended in Ordinance No. 2014-02. However, only a portion of the David Hill site is proposed to be rezoned at this time. The area proposed to be rezoned consistent with the Comprehensive Plan Map is at the northwest corner of David Hill Road and Thatcher Road. This area has a gross size of approximately 8.5 acres with approximately one-third (2.9 acres) developable. The remaining area (approximately 20.6 gross acres) currently shown as Mixed Use on the Comprehensive Plan map north of David Hill Road will be addressed further as part of the Westside Planning Project. This area will likely be recommended for rezoning to Single Family Residential (R-10). Parallel amendments to include the NMU designation in the Forest Grove Development Code are currently proposed (see Text Amendments above), which would allow for a mix of housing types and commercial uses.

Review Criteria 10.2.770.B: The zone change is consistent with relevant goals and policies of the Comprehensive Plan, as identified by the Director.

Finding: The Comprehensive Plan describes the opportunity to create three mixed use areas as follows:

A potential opportunity site for commercial land is located west of Sunset Drive and south of Highway 47. This site, under single ownership is approximately 23.5 acres in gross land area. Other potential opportunity sites include the area near Watercrest Road and Thatcher Road and David Hill Road and Thatcher Road.

The proposed zone changes for the mixed use areas are consistent with the Comprehensive Plan since the zoning map amendments will affect only the areas identified above. Furthermore, the proposed zone changes for the mixed use areas are consistent with the location factor contained in the Comprehensive Plan which states:

Areas where a mixture of residential, office and retail uses are appropriate to create complete neighborhoods or provide needed services and housing.

The zoning map amendments are consistent with this location factor since all three mixed use areas are located outside of the Town Center and off the Pacific Avenue/19th Avenue commercial corridor in areas that are predominantly residential in character. For the reasons stated above, this criterion is met. In addition, the commercial development in the NMU zones will be limited to “Village Centers” which cannot comprise more than 50% of the buildable land within a mixed use planned development or three acres, whichever is greater. As a result of this standard, the largest “village center” would be at the Davidson Site (9.4 acres based on overall developable site area of 18.8 acres). This ensures the NMU zoning at Mixed Use Area 3 (Davidson Site) is consistent with the Comprehensive Plan location criteria that mixed use sites tend to be 10 acres in land area or less. The basis for this conclusion is the premise that mixed use commercial/residential area is the “village center”, and the “village center” is capped at 9.4 acres.

The NMU areas are also consistent with the Comprehensive Plan location factor that the NMU designation apply to areas where a mixture of residential, office, and retail uses are appropriate to create complete neighborhoods or provide needed service. Mixed use areas are in locations previously zoned for single family residential development. The Gales Creek mixed use area was previously designated as Commercial Planned Development on the Forest Grove Comprehensive Plan Map which promoted commercial development near residential areas. Zoning these areas

as NMU will promote the creation of complete neighborhoods where residents are able to find goods and services in close proximity.

As provided for in the findings in the Text Amendment section above, the proposed map amendment is consistent with the Forest Grove Comprehensive Plan's goals and policies for housing, economic development, urbanization, and transportation.

Review Criteria 10.2.770.C: The site is suitable for the proposed zone and there is a lack of appropriately designated alternative sites within the vicinity. The size of the vicinity will be determined on a case-by-case basis since the impacts of a proposed zone and its potential uses vary. The factors to be considered in determining suitability are parcel size and location.

Finding: The Forest Grove Comprehensive Plan created a new Mixed Use plan designation to provide for a variety of retail and office uses near residential neighborhoods. The Comprehensive Plan states the Mixed Use plan designation is established to provide for a variety of retail and office uses near residential neighborhoods. Such limited commercial zones should be located on or bounded by arterial and collector streets to create nodes or concentrations of activity. The location of these new plan designations, as seen in the Forest Grove Comprehensive Plan Map, are located on or bounded by arterial and collector streets and adjacent to residential areas, so as to create nodes or concentrations of activity. The proposed zoning map amendments implement the adopted Comprehensive Plan designation and are consistent the requirement that the proposed zone change be considered based on parcel suitability and location. Therefore, this criterion is met.

Review Criteria 10.2.770.D: The zone change is consistent with the adopted Transportation System Plan. Development allowed by the zone change will not substantially impact the functional classification or operation of transportation facilities, or reduce the level of service of transportation facilities below the minimum acceptable level identified in the Transportation System Plan. To ensure proper review and mitigation, a traffic impact study may be required for the proposed zone change if it may impact transportation facilities.

Finding: The proposed amendments to the Forest Grove Zoning Map are consistent with the Forest Grove Comprehensive Plan Map, which informed the Forest Grove Transportation System Plan's assessment of future transportation needs. The future transportation needs assessment was based on the Preferred Land Use Alternative, and reflects changes to the City's existing Comprehensive Land Use Plan to encourage more nodal mixed use development. A Transportation Analysis was prepared and is attached as Exhibit C. As noted in this analysis, the proposed map amendments are not expected to substantially impact transportation facilities below the minimum acceptable levels identified in the City's adopted Transportation System

Plan. Based on the transportation analysis completed for the updated Comprehensive Plan Map which included mixed use areas the proposed zoning amendment this criterion is met.

Review Criteria 10.2.770.E: Public facilities and services for water supply, sanitary waste disposal, stormwater disposal, and police and fire protection are capable of supporting the uses allowed by the zone. Adequacy of services is based on the projected service demands of the site and the ability of the public services to accommodate those demands.

Finding: The three mixed use areas range from vacant to partially developed with a related range of available facilities and services. Availability, need, and necessary improvements to facilities and services will be determined as new development occurs and is evaluated through the Mixed Use Planned Development process. The applicant will be responsible for providing all services necessary for the functionality of the MUPD. Facilities such as streets, water supply facilities, sanitary sewers, and storm water detention facilities must be dedicated to the public if they are to provide service to any property not included in the MUPD. Mixed Use Area 1 (David Hill) is primarily vacant and will require new facilities and services as development occurs. Mixed Use Area 2 (CPD Area) is currently a mix of residential and commercial uses in a developed area and will likely not need new facilities and services. Mixed Use Area 3 (Davidson Site) is developed as a residential use and has been historically utilized as a farm use.

Existing Services (Water): Water lines are present adjacent to all three mixed use areas. Development of the mixed use areas is subject to the requirements of Development Code Article 8: Public Improvements. Future improvements to the water system necessary to serve development are identified in the Forest Grove Water Master Plan. The City's Water System Plan shows the City has sufficient capacity to serve future development in the mixed use areas.

Existing Services (Sanitary Sewer):

Mixed Use Area 1 (David Hill): An existing 8" PVC sanitary sewer line is located within the David Hill Right of Way west of Thatcher Road. **David Hill Road/Thatcher Road:** The City's Waste Water Master Plan shows a proposed 12 inch sanitary sewer trunk line within the David Hill Road right-of-way west of Thatcher Road. This line when installed will serve the David Hill Road/Thatcher Road mixed use area.

Mixed Use Area 2 (CPD Area): A 12 inch corrugated steel pipe is located adjacent to the mixed use area within the Thatcher Road right-of-way.

Mixed Use Area 3 (Davidson Site): A 10 inch PVC sanitary sewer line is adjacent to the Davidson Site within the Sunset Drive right-of-way. An 8 inch PVC line also exists across Hwy 47 to a manhole at the Davidson Site from the 36" Clean Water Services trunk line

north/east of Hwy. 47. The City's Waste Water Master Plan shows a future extension of the 8" line into the Davidson site to serve future development.

Existing Services (Storm Sewer):

Mixed Use Area 1 (David Hill): This area is not currently served with storm water piping. The City's Storm Drainage Master Plan shows a capital improvement project for stream restoration within the mixed use area. The Master Plan also shows future piping improvements along Thatcher Road.

Mixed Use Area 2 (CPD Area): An existing 12 inch storm line is present along the north side of Gales Creek Road adjacent to the mixed use area. A storm line is also present along the east side of Thatcher Road This line runs for a distance of approximately 150 feet from the Gales Creek Road/Thatcher Road intersection.

Mixed use Area 3 (Davidson Site): A 30 inch corrugated steel pipe exists along the west side of Sunset Drive. There are five storm inlets along the west side of Sunset Drive adjacent to the Davidson Site. Storm water is conveyed from the inlet to the storm pipe west of Sunset Drive. In addition to the storm pipe, a storm water swale and storm pond exist south of the Davidson Site approximately 730 feet east of Sunset Drive providing additional opportunity to accommodate drainage.

With future improvements shown in the Storm Drainage Master Plan and improvements required for development approval the City will have the ability to serve the mixed use areas.

The three mixed use areas are within the Forest Grove city limits. As such, the City will provide police and fire protection necessary to serve future development. Service needs are assessed though the annual budget process.

Review Criteria 10.2.770.A: The establishment of a zone district is not subject to the meeting of conditions.

Finding: No conditions are proposed.

TOWN CENTER/CIVIC CORRIDOR

Review Criteria 10.2.770. A: The zone change is consistent with the Comprehensive Plan Map. When the Comprehensive Plan has more than one implementing zone as shown on the Correspondence Table in Article 3, it must be shown that the proposed zone is the most appropriate, taking into consideration the purposes of each zone and the zoning pattern of surrounding land.

Finding: As described above, the proposed amendments consolidate the Town Center Support (TCS) zone into the Town Center Transition (TCT) zone. This change requires that all land currently zoned TCS be rezoned to TCT. In addition, the Town Center Expansion area, which is currently zoned Community Commercial (CC), is proposed to be rezoned to TCT. The transition from CC to TCT will encourage more urbanized development to occur by restricting new auto-oriented development, reducing landscape requirements, applying “urban” design standards, and allowing residential uses in mixed use or stand-alone development. **Of the Town Center zones, TCT is the most appropriate for this area given its location adjacent to the Town Center Core and its function as a transition area between Core and adjacent residential areas.**

Review Criteria 10.2.770. B: The zone change is consistent with relevant goals and policies of the Comprehensive Plan, as identified by the Director.

Finding: As provided for in the findings in the Text Amendment section above, the proposed map amendments are consistent with the Forest Grove Comprehensive Plan’s goals and policies for housing, economic development, urbanization, and transportation. This is reinforced by the fact that the proposed zone changes implement the 2014 Forest Grove Comprehensive Plan.

Review Criteria 10.2.770. C: The site is suitable for the proposed zone and there is a lack of appropriately designated alternative sites within the vicinity. The size of the vicinity will be determined on a case-by-case basis since the impacts of a proposed zone and its potential uses vary. The factors to be considered in determining suitability are parcel size and location.

Finding: The update to the Forest Grove Comprehensive Plan, completed in 2014, included consideration of site suitability for the proposed zoning classifications necessary to implement the Comprehensive Plan. Since the proposed zone changes simply implement the Comprehensive Plan the sites are determined to be suitable and consistent with this review criterion. Given that both TCS and TCT are Town Center zones and allow a very similar mix of uses, land which is currently suitable for TCS should be suitable for TCT. With the Town Center Expansion area, the transition from CC to TCT will encourage more urbanized development to occur by restricting new auto-oriented development, reducing landscape requirements, applying “urban” design standards, and allowing residential uses in mixed use or stand-alone development. In order to avoid making some existing uses non-conforming, exceptions are proposed within the TCT zone to allow for the continuation of those uses.

Review Criteria 10.2.770. D: The zone change is consistent with the adopted Transportation System Plan. Development allowed by the zone change will not substantially impact the functional classification or operation of transportation facilities, or reduce the level of service of transportation facilities below the minimum acceptable level identified in the Transportation

System Plan. To ensure proper review and mitigation, a traffic impact study may be required for the proposed zone change if it may impact transportation facilities.

Finding: The proposed amendment to the Forest Grove Zoning Map would be consistent with the Forest Grove Comprehensive Plan Map, which informed the Forest Grove Transportation System Plan’s assessment of future transportation needs. The future transportation needs assessment was based on the Preferred Land Use Alternative and reflects changes to the City’s existing Comprehensive Land Use Plan to encourage more nodal mixed use development. A Transportation Analysis was prepared and is attached as Exhibit C. As noted in this analysis, the proposed map amendments are not expected to substantially impact transportation facilities below the minimum acceptable levels identified in the City’s adopted Transportation System Plan. The City’s minimum acceptable level of service is LOS D.

Review Criteria 10.2.770. E: Public facilities and services for water supply, sanitary waste disposal, stormwater disposal, and police and fire protection are capable of supporting the uses allowed by the zone. Adequacy of services is based on the projected service demands of the site and the ability of the public services to accommodate those demands.

Finding: The proposed amendments to consolidate the Town Center Support with Town Center Transition zones and to expand the Town Center boundary to the east will occur in a developed area, currently provided with public facilities and services. The distinction between Town Center Support and Town Center transition is minor. The proposed change will not affect demand for public services currently planned for as reflected in the Comprehensive Plan. Service demand related to changing the zoning in the Town Center Expansion area from Community Commercial to Town Center Transition was considered during the Comprehensive Plan update. The Forest Grove Water Master Plan Updated (2010) indicates Forest Grove has a sufficient water supply for meeting service needs for at least the next twenty years. The Forest Grove Sanitary Sewer Master Plan update (2007) identifies a capital improvement project in the Town Center and Town Center Expansion areas. This project will increase the 8-inch and 10-inch diameter lines along 19th Avenue from Birch Street to A Street to the B Street pump station. This improvement will improve the City’s ability to serve development promoted by the proposed zone changes. The Forest Grove Storm Drainage Master Plan (2007) does not indicate a need for general storm drainage improvements in the areas affected by the zone changes. Necessary improvements meeting Clean Water Services and City standards resulting from individual development projects will be identified as part of the development review process. Projected service demands to Police and Fire resulting from the proposed zone changes, if any, will be addressed through the annual City budgeting process.



Review Criteria 10.2.770. F: The establishment of a zone district is not subject to the meeting of conditions.

Finding: No conditions are proposed

ORDINANCE NO. 2016-04**ORDINANCE AMENDING FOREST GROVE COMPREHENSIVE PLAN MAP
FOR PROPERTIES DESIGNATED TOWN CENTER SUPPORT AND COMMUNITY
COMMERCIAL IN THE TOWN CENTER EXPANSION AREA ON THE
COMPREHENSIVE PLAN DATED JANUARY 27, 2014
FILE NO. 311-15-00020-PLNG**

WHEREAS, The City of Forest Grove approved Ordinance 2014-01 and 2014-02 updating the Forest Grove Comprehensive Plan; and

WHEREAS, the Comprehensive Plan establishes three land use designations for the Town Center; and

WHEREAS, the three land use designations include Town Center Core, Town Center Support and Town Center Transition; and

WHEREAS, the City desires to change the designation of property classified as Town Center Support to Town Center Transition consistent with proposed Development Code text amendments; and

WHEREAS, the City prepared the Comprehensive Plan map amendments and forwarded the proposed amendments to the Planning Commission for consideration on January 5, 2016; and

WHEREAS, the Planning Commission held a Public Hearing on the proposed amendments on January 5, 2016; and

WHEREAS, the Planning Commission adopted Planning Commission Findings and Decision Number 16-01 recommending approval of the proposed Development Code amendments; and

WHEREAS, the City mailed by first class mail notice required under Measure 56 to affected property owners on January 19, 2016; and

WHEREAS, the City Council held a duly-noticed Public Hearing on the proposed ordinance on March 14, 2016, and continued the hearing on March 28, 2016.

NOW THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1. The City Council of the City of Forest Grove hereby amends the Comprehensive Plan Map as shown on Exhibit A.

Section 2. The City Council hereby finds that the proposed amendments are consistent with and meet the provisions of the Forest Grove Comprehensive Plan Chapter 2, Decision Considerations Pertaining to Comprehensive Plan Amendments making the following findings in support:

1. Consistency with applicable Comprehensive Plan policies

Finding: City Council adopted Ordinance 2014-02 and 2014-02 updating the Forest Grove Comprehensive Plan. The updated Comprehensive Plan Map identifies the area generally between Cedar and Elm Street and 21st Avenue and south of 19th Avenue as Town Center Expansion. This area is currently designated Community Commercial. Amending the Comprehensive Plan Map, from Community Commercial to Town Center Transition, implements the Town Center Expansion land use concept. In addition, meets the Land Use Location Factors described in the Comprehensive Plan including being located within ¼ mile of existing or planned transit service. This factor is met since the Town Center Expansion area is served by Tri-Met Line 57 and GroveLink.

Finding: Amending the Comprehensive Plan Map, from Town Center Support to Town Center Transition is consistent with applicable Comprehensive Plan policies including the Land Use Location Factors described in the Comprehensive Plan including being located within ¼ mile of existing or planned transit service. This factor is met since the Town Center Expansion area is served by Tri-Met Line 57 and GroveLink. In addition, the Town Center Transition designation, as described in the Comprehensive Plan, is established to increase employment and housing opportunities in close proximity to the Town Center. With the permitted uses allowed by the Town Center Transition designation, the designation supports Comprehensive Plan Housing Policy 3: Promote mixed-use development opportunities throughout the community. The designation also supports Comprehensive Plan policies related to strengthening downtown Forest Grove including promoting the continued viability of the Forest Grove Town Center as the focal point of the community and increasing housing in the Town Center.

2. Consistency with the Metro Regional Framework Plan or Regional Transportation Functional Plan as applicable

Finding: The proposed Comprehensive Plan Map amendments are consistent with Metro Regional Framework Policies related to Town Centers including Policy 1.15.1 which recognizes that the success of the Metro Region 2040 Growth Concept depends upon the enhancement of the Center City, Regional and Town Centers, Station Communities and Main Streets. In addition the amendments are consistent Framework Plan Policy 1.6 (Growth Management) by encouraging development within the urban growth boundary through efficient use of land.

3. Consistency with the Oregon Statewide Land Use Planning Goals

Finding: The proposed amendments are consistent with Oregon Statewide Land Use Planning Goal 9 (Economy) and Goal 10 (Housing). This is achieved by providing opportunities for a variety of housing types and new businesses by identifying land on the Comprehensive Plan map for such activities.

Section 3. All other ordinances or portions thereof inconsistent or conflicting with this ordinance or any portions hereof are hereby repealed to the extent of such inconsistency or conflict.

Section 4. This ordinance shall be effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 14th day of March, 2016.

PASSED the second reading this 28th day of March, 2016.

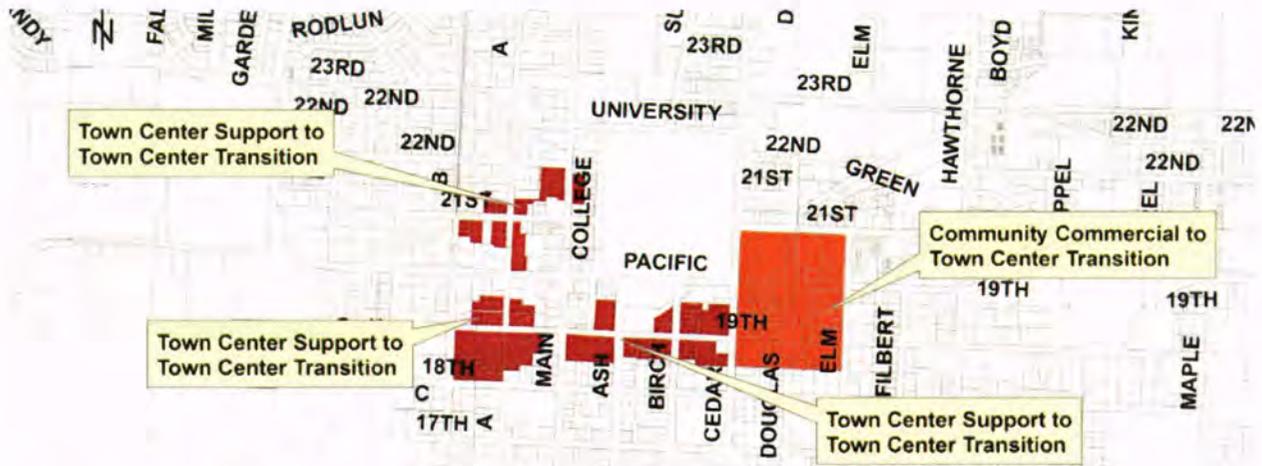
Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

EXHIBIT A

The update to the Comprehensive Plan in 2014 identified an area as “Town Center Expansion” on the Comprehensive Plan Map. This area is located between Cedar and Elm Street and 21st Avenue and just south of 19th Avenue as shown on the map below. This area is intended to be a transition between the more auto-oriented commercial corridor and the Town Center core. The subject area is currently designated Community Commercial. The recommendation is to change the Comprehensive Plan designation to Town Center Transition.



The map above also shows the area proposed for a Comprehensive Plan Map change from Town Center Support to Town Center Transition. This change is necessary since the Town Center Support designation is proposed for elimination as part of the recommended Development Code amendments.

The table below shows the amount of land affected by the proposed Comprehensive Plan map changes. This information is provided for reference.

	Current Comp Plan Designation		Proposed Comp Plan Designation	Gross Acreage Proposed to be Redesignated
Town Center Expansion Area (Cedar St. to Elm St. and south of 19th Avenue to 21st Avenue)	Community	Commercial	Town Center Transition	24.2 acres
Town Consolidation Center	Town Center Support		Town Center Transition	22.6 acres

ORDINANCE NO. 2016-05

**ORDINANCE AMENDING OFFICIAL FOREST GROVE ZONING MAP FOR
PROPERTIES DESIGNATED NEIGHBORHOOD MIXED USE AND TOWN CENTER
EXPANSION ON THE COMPREHENSIVE PLAN MAP DATED JANUARY 27, 2014
FILE NO. 311-15-00020-PLNG**

WHEREAS, The City of Forest Grove approved Ordinance 2014-01 and 2014-02 updating the Forest Grove Comprehensive Plan; and

WHEREAS, the updated Forest Grove Comprehensive Plan requires specific revisions to the Forest Grove Development Code to implement policies contained in the updated Forest Grove Comprehensive Plan; and

WHEREAS, the City prepared the required amendments and forwarded the proposed amendments to the Planning Commission for consideration on January 5, 2016; and

WHEREAS, the City desires to make other amendments to the Development Code to promote the efficient use of land within commercial areas, encourage housing near transit and shopping; and

WHEREAS, the Planning Commission held a Public Hearing on the proposed amendments on January 5, 2016; and

WHEREAS, the Planning Commission adopted Planning Commission Findings and Decision Number 16-01 recommending approval of the proposed Development Code amendments; and

WHEREAS, the City mailed by first class mail notice required under Measure 56 to affected property owners on January 19, 2016; and

WHEREAS, the City Council held a duly-noticed Public Hearing on the proposed ordinance on March 14, 2016, and continued the hearing on March 28, 2016.

NOW THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1. The City Council of the City of Forest Grove hereby amends the Official Zoning Map as shown on Exhibit A.

Section 2. The City Council hereby adopts the Planning Commission's Findings and Decision dated January 12, 2016 as shown on Exhibit B.

Section 3. The City Council hereby finds that the proposed amendments are consistent with and meet the provisions of Development Code Section 10. 2.770 *Review Criteria Pertaining to Zone Change* as shown on Exhibit B.

Section 4. All other ordinances or portions thereof inconsistent or conflicting with this ordinance or any portions hereof are hereby repealed to the extent of such inconsistency or conflict.

Section 5. This ordinance shall be effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 14th day of March, 2016.

PASSED the second reading this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

EXHIBIT A

	Current Zoning	Proposed Zoning	Gross Proposed Rezoned	Acreage to be
Mixed Use Area 1 (David Hill)	R-10	NMU	8.5 acres	
Mixed Use Area 2 (CPD Area)	CPD	NMU	6.4 acres	
Mixed Use Area 3 (Davidson Site)	LI	NMU	23.5 acres	
Town Center Consolidation	TCS	TCT	22.6 acres	
Town Center Expansion Area (Cedar St. to Elm St. and south of 19 th Avenue to 21 st Avenue)	CC	TCT	24.2 acres	

Proposed Zoning Map Amendments

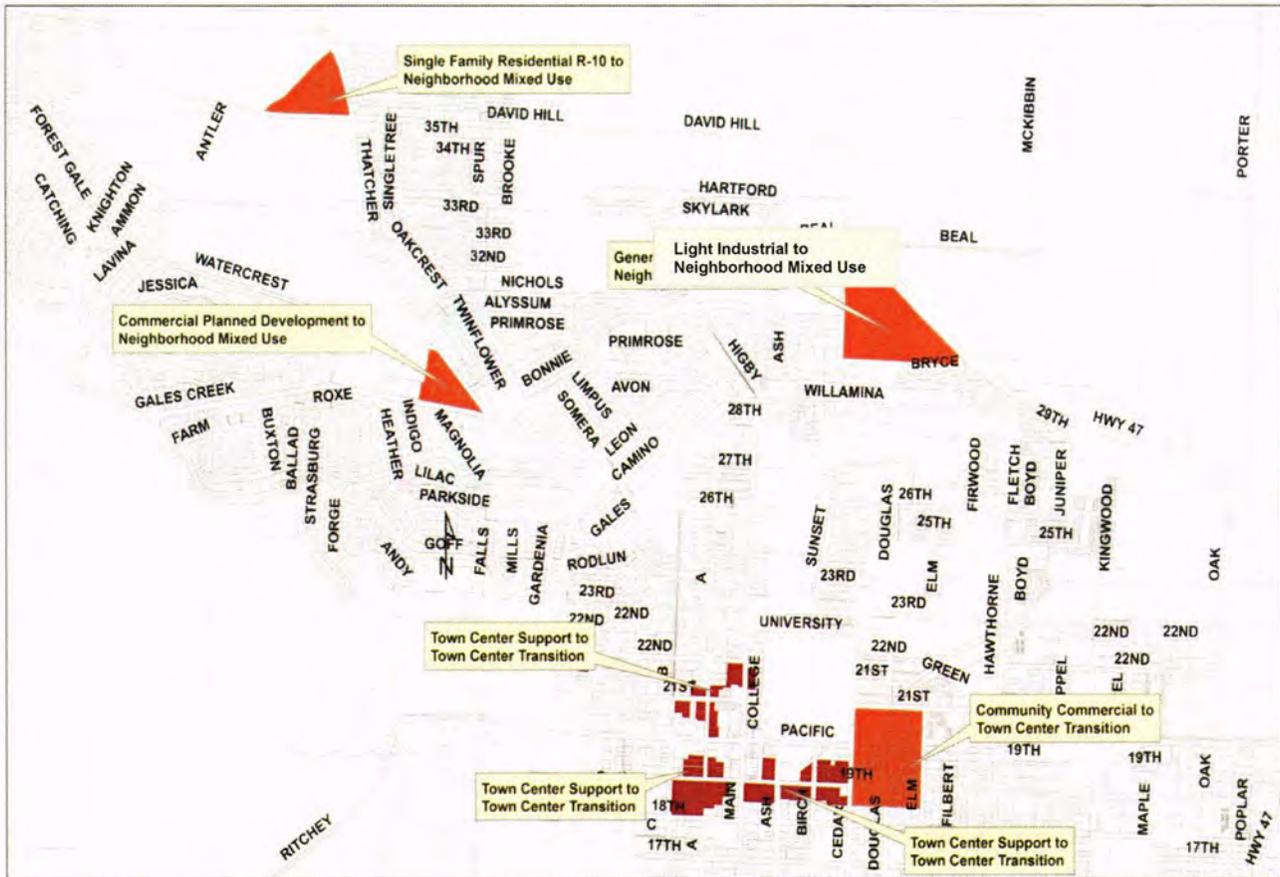


EXHIBIT B

FINDINGS - MAP AMENDMENTS (SUBJECT TO 10.2.750 – 10.2.770)

MIXED USE AREAS

Review Criteria 10.2.770.A: *The zone change is consistent with the Comprehensive Plan Map. When the Comprehensive Plan has more than one implementing zone as shown on the Correspondence Table in Article 3, it must be shown that the proposed zone is the most appropriate, taking into consideration the purposes of each zone and the zoning pattern of surrounding land.*

Finding: The recommended zoning designation for the three mixed use areas is consistent with the Comprehensive Plan Map, as amended in Ordinance No. 2014-02. However, only a portion of the David Hill site is proposed to be rezoned at this time. The area proposed to be rezoned consistent with the Comprehensive Plan Map is at the northwest corner of David Hill Road and Thatcher Road. This area has a gross size of approximately 8.5 acres with approximately one-third (2.9 acres) developable. The remaining area (approximately 20.6 gross acres) currently shown as Mixed Use on the Comprehensive Plan map north of David Hill Road will be addressed further as part of the Westside Planning Project. This area will likely be recommended for rezoning to Single Family Residential (R-10). Parallel amendments to include the NMU designation in the Forest Grove Development Code are currently proposed (see Text Amendments above), which would allow for a mix of housing types and commercial uses.

Review Criteria 10.2.770.B: *The zone change is consistent with relevant goals and policies of the Comprehensive Plan, as identified by the Director.*

Finding: The Comprehensive Plan describes the opportunity to create three mixed use areas as follows:

A potential opportunity site for commercial land is located west of Sunset Drive and south of Highway 47. This site, under single ownership is approximately 23.5 acres in gross land area. Other potential opportunity sites include the area near Watercrest Road and Thatcher Road and David Hill Road and Thatcher Road.

The proposed zone changes for the mixed use areas are consistent with the Comprehensive Plan since the zoning map amendments will affect only the areas identified above. Furthermore, the proposed zone changes for the mixed use areas are consistent with the location factor contained in the Comprehensive Plan which states:

Areas where a mixture of residential, office and retail uses are appropriate to create complete neighborhoods or provide needed services and housing.

The zoning map amendments are consistent with this location factor since all three mixed use areas are located outside of the Town Center and off the Pacific Avenue/19th Avenue commercial corridor in areas that are predominantly residential in character. For the reasons stated above, this criterion is met. In addition, the commercial development in the NMU zones will be limited to "Village Centers" which cannot comprise more than 50% of the buildable land within a mixed use planned development or three acres, whichever is greater. As a result of this standard, the largest "village center" would be at the Davidson Site (9.4 acres based on overall developable site area of 18.8 acres). This ensures the NMU zoning at Mixed Use Area 3 (Davidson Site) is consistent with the Comprehensive Plan location criteria that mixed use sites tend to be 10 acres in land area or less. The basis for this conclusion is the premise that mixed

use commercial/residential area is the “village center”, and the “village center” is capped at 9.4 acres.

The NMU areas are also consistent with the Comprehensive Plan location factor that the NMU designation apply to areas where a mixture of residential, office, and retail uses are appropriate to create complete neighborhoods or provide needed service. Mixed use areas are in locations previously zoned for single family residential development. The Gales Creek mixed use area was previously designated as Commercial Planned Development on the Forest Grove Comprehensive Plan Map which promoted commercial development near residential areas. Zoning these areas as NMU will promote the creation of complete neighborhoods where residents are able to find goods and services in close proximity.

As provided for in the findings in the Text Amendment section above, the proposed map amendment is consistent with the Forest Grove Comprehensive Plan’s goals and policies for housing, economic development, urbanization, and transportation.

Review Criteria 10.2.770.C: The site is suitable for the proposed zone and there is a lack of appropriately designated alternative sites within the vicinity. The size of the vicinity will be determined on a case-by-case basis since the impacts of a proposed zone and its potential uses vary. The factors to be considered in determining suitability are parcel size and location.

Finding: The Forest Grove Comprehensive Plan created a new Mixed Use plan designation to provide for a variety of retail and office uses near residential neighborhoods. The Comprehensive Plan states the Mixed Use plan designation is established to provide for a variety of retail and office uses near residential neighborhoods. Such limited commercial zones should be located on or bounded by arterial and collector streets to create nodes or concentrations of activity. The location of these new plan designations, as seen in the Forest Grove Comprehensive Plan Map, are located on or bounded by arterial and collector streets and adjacent to residential areas, so as to create nodes or concentrations of activity. The proposed zoning map amendments implement the adopted Comprehensive Plan designation and are consistent the requirement that the proposed zone change be considered based on parcel suitability and location. Therefore, this criterion is met.

Review Criteria 10.2.770.D: The zone change is consistent with the adopted Transportation System Plan. Development allowed by the zone change will not substantially impact the functional classification or operation of transportation facilities, or reduce the level of service of transportation facilities below the minimum acceptable level identified in the Transportation System Plan. To ensure proper review and mitigation, a traffic impact study may be required for the proposed zone change if it may impact transportation facilities.

Finding: The proposed amendments to the Forest Grove Zoning Map are consistent with the Forest Grove Comprehensive Plan Map, which informed the Forest Grove Transportation System Plan’s assessment of future transportation needs. The future transportation needs assessment was based on the Preferred Land Use Alternative, and reflects changes to the City’s existing Comprehensive Land Use Plan to encourage more nodal mixed use development. A Transportation Analysis was prepared and is attached as Exhibit C. As noted in this analysis, the proposed map amendments are not expected to substantially impact transportation facilities below the minimum acceptable levels identified in the City’s adopted Transportation System Plan. Based on the transportation analysis completed for the updated

Comprehensive Plan Map which included mixed use areas the proposed zoning amendment this criterion is met.

Review Criteria 10.2.770.E: Public facilities and services for water supply, sanitary waste disposal, stormwater disposal, and police and fire protection are capable of supporting the uses allowed by the zone. Adequacy of services is based on the projected service demands of the site and the ability of the public services to accommodate those demands.

Finding: The three mixed use areas range from vacant to partially developed with a related range of available facilities and services. Availability, need, and necessary improvements to facilities and services will be determined as new development occurs and is evaluated through the Mixed Use Planned Development process. The applicant will be responsible for providing all services necessary for the functionality of the MUPD. Facilities such as streets, water supply facilities, sanitary sewers, and storm water detention facilities must be dedicated to the public if they are to provide service to any property not included in the MUPD. Mixed Use Area 1 (David Hill) is primarily vacant and will require new facilities and services as development occurs. Mixed Use Area 2 (CPD Area) is currently a mix of residential and commercial uses in a developed area and will likely not need new facilities and services. Mixed Use Area 3 (Davidson Site) is developed as a residential use and has been historically utilized as a farm use.

Existing Services (Water): Water lines are present adjacent to all three mixed use areas. Development of the mixed use areas is subject to the requirements of Development Code Article 8: Public Improvements. Future improvements to the water system necessary to serve development are identified in the Forest Grove Water Master Plan. The City's Water System Plan shows the City has sufficient capacity to serve future development in the mixed use areas.

Existing Services (Sanitary Sewer):

Mixed Use Area 1 (David Hill): An existing 8" PVC sanitary sewer line is located within the David Hill Right of Way west of Thatcher Road. David Hill Road/Thatcher Road: The City's Waste Water Master Plan shows a proposed 12 inch sanitary sewer trunk line within the David Hill Road right-of-way west of Thatcher Road. This line when installed will serve the David Hill Road/Thatcher Road mixed use area.

Mixed Use Area 2 (CPD Area): A 12 inch corrugated steel pipe is located adjacent to the mixed use area within the Thatcher Road right-of-way.

Mixed Use Area 3 (Davidson Site): A 10 inch PVC sanitary sewer line is adjacent to the Davidson Site within the Sunset Drive right-of-way. An 8 inch PVC line also exists across Hwy 47 to a manhole at the Davidson Site from the 36" Clean Water Services trunk line north/east of Hwy. 47. The City's Waste Water Master Plan shows a future extension of the 8" line into the Davidson site to serve future development.

Existing Services (Storm Sewer):

Mixed Use Area 1 (David Hill): This area is not currently served with storm water piping. The City's Storm Drainage Master Plan shows a capital improvement project for stream restoration within the mixed use area. The Master Plan also shows future piping improvements along Thatcher Road.

Mixed Use Area 2 (CPD Area): An existing 12 inch storm line is present along the north side of Gales Creek Road adjacent to the mixed use area. A storm line is also present along the east side of Thatcher Road This line runs for a distance of approximately 150 feet from the Gales Creek Road/Thatcher Road intersection.

Mixed use Area 3 (Davidson Site): A 30 inch corrugated steel pipe exists along the west side of Sunset Drive. There are five storm inlets along the west side of Sunset Drive adjacent to the Davidson Site. Storm water is conveyed from the inlet to the storm pipe west of Sunset Drive. In addition to the storm pipe, a storm water swale and storm pond exist south of the Davidson Site approximately 730 feet east of Sunset Drive providing additional opportunity to accommodate drainage.

With future improvements shown in the Storm Drainage Master Plan and improvements required for development approval the City will have the ability to serve the mixed use areas. The three mixed use areas are within the Forest Grove city limits. As such, the City will provide police and fire protection necessary to serve future development. Service needs are assessed through the annual budget process.

Review Criteria 10.2.770.A: The establishment of a zone district is not subject to the meeting of conditions.

Finding: No conditions are proposed.

TOWN CENTER/CIVIC CORRIDOR

Review Criteria 10.2.770. A: The zone change is consistent with the Comprehensive Plan Map. When the Comprehensive Plan has more than one implementing zone as shown on the Correspondence Table in Article 3, it must be shown that the proposed zone is the most appropriate, taking into consideration the purposes of each zone and the zoning pattern of surrounding land.

Finding: As described above, the proposed amendments consolidate the Town Center Support (TCS) zone into the Town Center Transition (TCT) zone. This change requires that all land currently zoned TCS be rezoned to TCT. In addition, the Town Center Expansion area, which is currently zoned Community Commercial (CC), is proposed to be rezoned to TCT. The transition from CC to TCT will encourage more urbanized development to occur by restricting new auto-oriented development, reducing landscape requirements, applying “urban” design standards, and allowing residential uses in mixed use or stand-alone development. Of the Town Center zones, TCT is the most appropriate for this area given its location adjacent to the Town Center Core and its function as a transition area between Core and adjacent residential areas.

Review Criteria 10.2.770. B: The zone change is consistent with relevant goals and policies of the Comprehensive Plan, as identified by the Director.

Finding: As provided for in the findings in the Text Amendment section above, the proposed map amendments are consistent with the Forest Grove Comprehensive Plan’s goals and policies for housing, economic development, urbanization, and transportation. This is reinforced by the fact that the proposed zone changes implement the 2014 Forest Grove Comprehensive Plan.

Review Criteria 10.2.770. C: The site is suitable for the proposed zone and there is a lack of appropriately designated alternative sites within the vicinity. The size of the vicinity will be determined on a case-by-case basis since the impacts of a proposed zone and its potential uses vary. The factors to be considered in determining suitability are parcel size and location.

Finding: The update to the Forest Grove Comprehensive Plan, completed in 2014, included consideration of site suitability for the proposed zoning classifications necessary to implement

the Comprehensive Plan. Since the proposed zone changes simply implement the Comprehensive Plan the sites are determined to be suitable and consistent with this review criterion. Given that both TCS and TCT are Town Center zones and allow a very similar mix of uses, land which is currently suitable for TCS should be suitable for TCT. With the Town Center Expansion area, the transition from CC to TCT will encourage more urbanized development to occur by restricting new auto-oriented development, reducing landscape requirements, applying "urban" design standards, and allowing residential uses in mixed use or stand-alone development. In order to avoid making some existing uses non-conforming, exceptions are proposed within the TCT zone to allow for the continuation of those uses.

Review Criteria 10.2.770. D: The zone change is consistent with the adopted Transportation System Plan. Development allowed by the zone change will not substantially impact the functional classification or operation of transportation facilities, or reduce the level of service of transportation facilities below the minimum acceptable level identified in the Transportation System Plan. To ensure proper review and mitigation, a traffic impact study may be required for the proposed zone change if it may impact transportation facilities.

Finding: The proposed amendment to the Forest Grove Zoning Map would be consistent with the Forest Grove Comprehensive Plan Map, which informed the Forest Grove Transportation System Plan's assessment of future transportation needs. The future transportation needs assessment was based on the Preferred Land Use Alternative and reflects changes to the City's existing Comprehensive Land Use Plan to encourage more nodal mixed use development. A Transportation Analysis was prepared and is attached as Exhibit C. As noted in this analysis, the proposed map amendments are not expected to substantially impact transportation facilities below the minimum acceptable levels identified in the City's adopted Transportation System Plan. The City's minimum acceptable level of service is LOS D.

Review Criteria 10.2.770. E: Public facilities and services for water supply, sanitary waste disposal, stormwater disposal, and police and fire protection are capable of supporting the uses allowed by the zone. Adequacy of services is based on the projected service demands of the site and the ability of the public services to accommodate those demands.

Finding: The proposed amendments to consolidate the Town Center Support with Town Center Transition zones and to expand the Town Center boundary to the east will occur in a developed area, currently provided with public facilities and services. The distinction between Town Center Support and Town Center transition is minor. The proposed change will not affect demand for public services currently planned for as reflected in the Comprehensive Plan. Service demand related to changing the zoning in the Town Center Expansion area from Community Commercial to Town Center Transition was considered during the Comprehensive Plan update. The Forest Grove Water Master Plan Updated (2010) indicates Forest Grove has a sufficient water supply for meeting service needs for at least the next twenty years. The Forest Grove Sanitary Sewer Master Plan update (2007) identifies a capital improvement project in the Town Center and Town Center Expansion areas. This project will increase the 8-inch and 10-inch diameter lines along 19th Avenue from Birch Street to A Street to the B Street pump station. This improvement will improve the City's ability to serve development promoted by the proposed zone changes. The Forest Grove Storm Drainage Master Plan (2007) does not indicate a need for general storm drainage improvements in the areas affected by the zone changes. Necessary improvements meeting Clean Water Services and City standards resulting from individual development projects will be identified as part of the development review process. Projected

service demands to Police and Fire resulting from the proposed zone changes, if any, will be addressed through the annual City budgeting process.

Review Criteria 10.2.770. F: The establishment of a zone district is not subject to the meeting of conditions.

Finding: No conditions are proposed

ORDINANCE NO. 2016-06

**ORDINANCE AMENDING FOREST GROVE DEVELOPMENT CODE ARTICLE 3 TO
ESTABLISH THE BUSINESS INDUSTRIAL PARK ZONE
FILE NO. 311-15-00027-PLNG**

WHEREAS, The City of Forest Grove approved Ordinance 2014-01 and 2014-02 updating the Forest Grove Comprehensive Plan; and

WHEREAS, the updated Forest Grove Comprehensive Plan includes a new plan designation title Campus Employment; and

WHEREAS, the Campus Employment Comprehensive Plan designation is an employment plan designation intended for development of industrial and office parks with a high level of amenity value including landscaping and open space; and

WHEREAS, the Campus Employment designation is intended to allow for industrial and corporate office uses and supporting service activities; and

WHEREAS, a zoning designation is necessary to implement the Campus Employment Plan designation and for zoning property; and

WHEREAS, on January 19, 2016, the Planning Commission held a duly-noticed Public Hearing on proposed amendments to Article 3 of the Development Code to establish a Business Industrial Park zoning designation intended to implement the Campus Employment Plan designation; and

WHEREAS, the Planning Commission adopted Planning Commission Findings and Decision Number 16-02 recommending approval of the proposed Development Code amendments; and

WHEREAS, the City Council held a duly-noticed Public Hearing on the proposed ordinance on March 14, 2016, and continued the hearing on March 28, 2016.

NOW THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1. The City Council of the City of Forest Grove hereby adopts the text amendments to the Development Code Articles 3 as shown on Exhibit A.

Section 2. The City Council hereby adopts the Planning Commission's Findings and Decision dated January 12, 2016 as shown on Exhibit B.

Section 3. The City Council hereby finds that the proposed amendments are consistent with and meet the provisions of Development Code Section 10.2.630 *Review Criteria Pertaining to Zoning Text Amendments* as shown on Exhibit B.

Section 4. All other ordinances or portions thereof inconsistent or conflicting with this ordinance or any portions hereof are hereby repealed to the extent of such inconsistency or conflict.

Section 5. This ordinance shall be effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 14th day of March, 2016.

PASSED the second reading this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

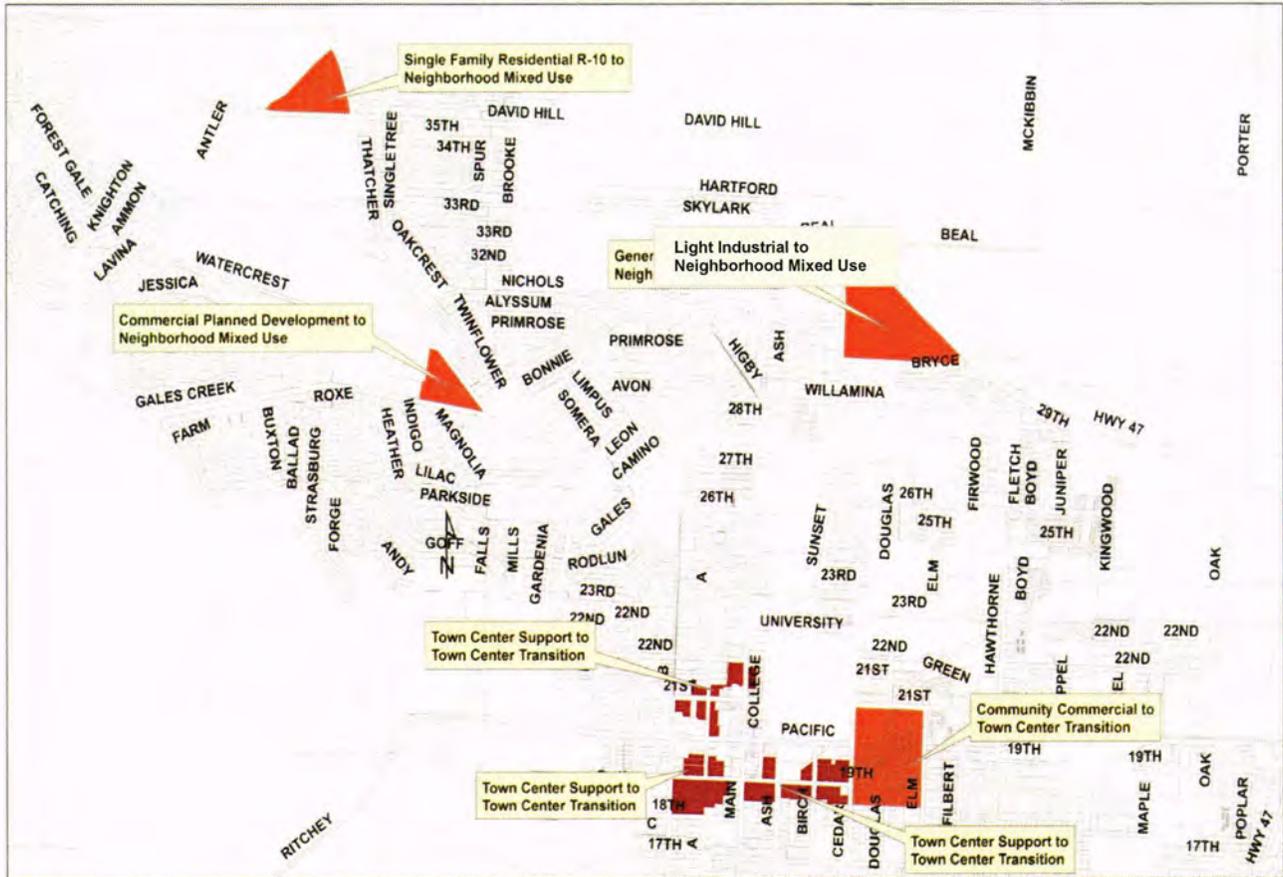
APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

EXHIBIT A

	Current Zoning	Proposed Zoning	Gross Proposed Rezoned	Acreage to be
Mixed Use Area 1 (David Hill)	R-10	NMU	8.5 acres	
Mixed Use Area 2 (CPD Area)	CPD	NMU	6.4 acres	
Mixed Use Area 3 (Davidson Site)	LI	NMU	23.5 acres	
Town Center Consolidation	TCS	TCT	22.6 acres	
Town Center Expansion Area (Cedar St. to Elm St. and south of 19 th Avenue to 21 st Avenue)	CC	TCT	24.2 acres	

Proposed Zoning Map Amendments



Z-7

EXHIBIT B

FINDINGS - MAP AMENDMENTS (SUBJECT TO 10.2.750 – 10.2.770)

MIXED USE AREAS

Review Criteria 10.2.770.A: *The zone change is consistent with the Comprehensive Plan Map. When the Comprehensive Plan has more than one implementing zone as shown on the Correspondence Table in Article 3, it must be shown that the proposed zone is the most appropriate, taking into consideration the purposes of each zone and the zoning pattern of surrounding land.*

Finding: The recommended zoning designation for the three mixed use areas is consistent with the Comprehensive Plan Map, as amended in Ordinance No. 2014-02. However, only a portion of the David Hill site is proposed to be rezoned at this time. The area proposed to be rezoned consistent with the Comprehensive Plan Map is at the northwest corner of David Hill Road and Thatcher Road. This area has a gross size of approximately 8.5 acres with approximately one-third (2.9 acres) developable. The remaining area (approximately 20.6 gross acres) currently shown as Mixed Use on the Comprehensive Plan map north of David Hill Road will be addressed further as part of the Westside Planning Project. This area will likely be recommended for rezoning to Single Family Residential (R-10). Parallel amendments to include the NMU designation in the Forest Grove Development Code are currently proposed (see Text Amendments above), which would allow for a mix of housing types and commercial uses.

Review Criteria 10.2.770.B: *The zone change is consistent with relevant goals and policies of the Comprehensive Plan, as identified by the Director.*

Finding: The Comprehensive Plan describes the opportunity to create three mixed use areas as follows:

A potential opportunity site for commercial land is located west of Sunset Drive and south of Highway 47. This site, under single ownership is approximately 23.5 acres in gross land area. Other potential opportunity sites include the area near Watercrest Road and Thatcher Road and David Hill Road and Thatcher Road.

The proposed zone changes for the mixed use areas are consistent with the Comprehensive Plan since the zoning map amendments will affect only the areas identified above. Furthermore, the proposed zone changes for the mixed use areas are consistent with the location factor contained in the Comprehensive Plan which states:

Areas where a mixture of residential, office and retail uses are appropriate to create complete neighborhoods or provide needed services and housing.

The zoning map amendments are consistent with this location factor since all three mixed use areas are located outside of the Town Center and off the Pacific Avenue/19th Avenue commercial corridor in areas that are predominantly residential in character. For the reasons stated above, this criterion is met. In addition, the commercial development in the NMU zones will be limited to "Village Centers" which cannot comprise more than 50% of the buildable land within a mixed use planned development or three acres, whichever is greater. As a result of this standard, the largest "village center" would be at the Davidson Site (9.4 acres based on overall developable site area of 18.8 acres). This ensures the NMU zoning at Mixed Use Area 3 (Davidson Site) is consistent with the Comprehensive Plan location criteria that mixed use sites tend to be 10 acres in land area or less. The basis for this conclusion is the premise that mixed

use commercial/residential area is the “village center”, and the “village center” is capped at 9.4 acres.

The NMU areas are also consistent with the Comprehensive Plan location factor that the NMU designation apply to areas where a mixture of residential, office, and retail uses are appropriate to create complete neighborhoods or provide needed service. Mixed use areas are in locations previously zoned for single family residential development. The Gales Creek mixed use area was previously designated as Commercial Planned Development on the Forest Grove Comprehensive Plan Map which promoted commercial development near residential areas. Zoning these areas as NMU will promote the creation of complete neighborhoods where residents are able to find goods and services in close proximity.

As provided for in the findings in the Text Amendment section above, the proposed map amendment is consistent with the Forest Grove Comprehensive Plan’s goals and policies for housing, economic development, urbanization, and transportation.

Review Criteria 10.2.770.C: The site is suitable for the proposed zone and there is a lack of appropriately designated alternative sites within the vicinity. The size of the vicinity will be determined on a case-by-case basis since the impacts of a proposed zone and its potential uses vary. The factors to be considered in determining suitability are parcel size and location.

Finding: The Forest Grove Comprehensive Plan created a new Mixed Use plan designation to provide for a variety of retail and office uses near residential neighborhoods. The Comprehensive Plan states the Mixed Use plan designation is established to provide for a variety of retail and office uses near residential neighborhoods. Such limited commercial zones should be located on or bounded by arterial and collector streets to create nodes or concentrations of activity. The location of these new plan designations, as seen in the Forest Grove Comprehensive Plan Map, are located on or bounded by arterial and collector streets and adjacent to residential areas, so as to create nodes or concentrations of activity. The proposed zoning map amendments implement the adopted Comprehensive Plan designation and are consistent the requirement that the proposed zone change be considered based on parcel suitability and location. Therefore, this criterion is met.

Review Criteria 10.2.770.D: The zone change is consistent with the adopted Transportation System Plan. Development allowed by the zone change will not substantially impact the functional classification or operation of transportation facilities, or reduce the level of service of transportation facilities below the minimum acceptable level identified in the Transportation System Plan. To ensure proper review and mitigation, a traffic impact study may be required for the proposed zone change if it may impact transportation facilities.

Finding: The proposed amendments to the Forest Grove Zoning Map are consistent with the Forest Grove Comprehensive Plan Map, which informed the Forest Grove Transportation System Plan’s assessment of future transportation needs. The future transportation needs assessment was based on the Preferred Land Use Alternative, and reflects changes to the City’s existing Comprehensive Land Use Plan to encourage more nodal mixed use development. A Transportation Analysis was prepared and is attached as Exhibit C. As noted in this analysis, the proposed map amendments are not expected to substantially impact transportation facilities below the minimum acceptable levels identified in the City’s adopted Transportation System Plan. Based on the transportation analysis completed for the updated

Comprehensive Plan Map which included mixed use areas the proposed zoning amendment this criterion is met.

Review Criteria 10.2.770.E: Public facilities and services for water supply, sanitary waste disposal, stormwater disposal, and police and fire protection are capable of supporting the uses allowed by the zone. Adequacy of services is based on the projected service demands of the site and the ability of the public services to accommodate those demands.

Finding: The three mixed use areas range from vacant to partially developed with a related range of available facilities and services. Availability, need, and necessary improvements to facilities and services will be determined as new development occurs and is evaluated through the Mixed Use Planned Development process. The applicant will be responsible for providing all services necessary for the functionality of the MUPD. Facilities such as streets, water supply facilities, sanitary sewers, and storm water detention facilities must be dedicated to the public if they are to provide service to any property not included in the MUPD. Mixed Use Area 1 (David Hill) is primarily vacant and will require new facilities and services as development occurs. Mixed Use Area 2 (CPD Area) is currently a mix of residential and commercial uses in a developed area and will likely not need new facilities and services. Mixed Use Area 3 (Davidson Site) is developed as a residential use and has been historically utilized as a farm use.

Existing Services (Water): Water lines are present adjacent to all three mixed use areas. Development of the mixed use areas is subject to the requirements of Development Code Article 8: Public Improvements. Future improvements to the water system necessary to serve development are identified in the Forest Grove Water Master Plan. The City's Water System Plan shows the City has sufficient capacity to serve future development in the mixed use areas. Existing Services (Sanitary Sewer):

Mixed Use Area 1 (David Hill): An existing 8" PVC sanitary sewer line is located within the David Hill Right of Way west of Thatcher Road. David Hill Road/Thatcher Road: The City's Waste Water Master Plan shows a proposed 12 inch sanitary sewer trunk line within the David Hill Road right-of-way west of Thatcher Road. This line when installed will serve the David Hill Road/Thatcher Road mixed use area.

Mixed Use Area 2 (CPD Area): A 12 inch corrugated steel pipe is located adjacent to the mixed use area within the Thatcher Road right-of-way.

Mixed Use Area 3 (Davidson Site): A 10 inch PVC sanitary sewer line is adjacent to the Davidson Site within the Sunset Drive right-of-way. An 8 inch PVC line also exists across Hwy 47 to a manhole at the Davidson Site from the 36" Clean Water Services trunk line north/east of Hwy. 47. The City's Waste Water Master Plan shows a future extension of the 8" line into the Davidson site to serve future development.

Existing Services (Storm Sewer):

Mixed Use Area 1 (David Hill): This area is not currently served with storm water piping. The City's Storm Drainage Master Plan shows a capital improvement project for stream restoration within the mixed use area. The Master Plan also shows future piping improvements along Thatcher Road.

Mixed Use Area 2 (CPD Area): An existing 12 inch storm line is present along the north side of Gales Creek Road adjacent to the mixed use area. A storm line is also present along the east side of Thatcher Road This line runs for a distance of approximately 150 feet from the Gales Creek Road/Thatcher Road intersection.

Mixed use Area 3 (Davidson Site): A 30 inch corrugated steel pipe exists along the west side of Sunset Drive. There are five storm inlets along the west side of Sunset Drive adjacent to the Davidson Site. Storm water is conveyed from the inlet to the storm pipe west of Sunset Drive. In addition to the storm pipe, a storm water swale and storm pond exist south of the Davidson Site approximately 730 feet east of Sunset Drive providing additional opportunity to accommodate drainage.

With future improvements shown in the Storm Drainage Master Plan and improvements required for development approval the City will have the ability to serve the mixed use areas. The three mixed use areas are within the Forest Grove city limits. As such, the City will provide police and fire protection necessary to serve future development. Service needs are assessed through the annual budget process.

Review Criteria 10.2.770.A: The establishment of a zone district is not subject to the meeting of conditions.

Finding: No conditions are proposed.

TOWN CENTER/CIVIC CORRIDOR

Review Criteria 10.2.770. A: The zone change is consistent with the Comprehensive Plan Map. When the Comprehensive Plan has more than one implementing zone as shown on the Correspondence Table in Article 3, it must be shown that the proposed zone is the most appropriate, taking into consideration the purposes of each zone and the zoning pattern of surrounding land.

Finding: As described above, the proposed amendments consolidate the Town Center Support (TCS) zone into the Town Center Transition (TCT) zone. This change requires that all land currently zoned TCS be rezoned to TCT. In addition, the Town Center Expansion area, which is currently zoned Community Commercial (CC), is proposed to be rezoned to TCT. The transition from CC to TCT will encourage more urbanized development to occur by restricting new auto-oriented development, reducing landscape requirements, applying "urban" design standards, and allowing residential uses in mixed use or stand-alone development. Of the Town Center zones, TCT is the most appropriate for this area given its location adjacent to the Town Center Core and its function as a transition area between Core and adjacent residential areas.

Review Criteria 10.2.770. B: The zone change is consistent with relevant goals and policies of the Comprehensive Plan, as identified by the Director.

Finding: As provided for in the findings in the Text Amendment section above, the proposed map amendments are consistent with the Forest Grove Comprehensive Plan's goals and policies for housing, economic development, urbanization, and transportation. This is reinforced by the fact that the proposed zone changes implement the 2014 Forest Grove Comprehensive Plan.

Review Criteria 10.2.770. C: The site is suitable for the proposed zone and there is a lack of appropriately designated alternative sites within the vicinity. The size of the vicinity will be determined on a case-by-case basis since the impacts of a proposed zone and its potential uses vary. The factors to be considered in determining suitability are parcel size and location.

Finding: The update to the Forest Grove Comprehensive Plan, completed in 2014, included consideration of site suitability for the proposed zoning classifications necessary to implement

the Comprehensive Plan. Since the proposed zone changes simply implement the Comprehensive Plan the sites are determined to be suitable and consistent with this review criterion. Given that both TCS and TCT are Town Center zones and allow a very similar mix of uses, land which is currently suitable for TCS should be suitable for TCT. With the Town Center Expansion area, the transition from CC to TCT will encourage more urbanized development to occur by restricting new auto-oriented development, reducing landscape requirements, applying "urban" design standards, and allowing residential uses in mixed use or stand-alone development. In order to avoid making some existing uses non-conforming, exceptions are proposed within the TCT zone to allow for the continuation of those uses.

Review Criteria 10.2.770. D: The zone change is consistent with the adopted Transportation System Plan. Development allowed by the zone change will not substantially impact the functional classification or operation of transportation facilities, or reduce the level of service of transportation facilities below the minimum acceptable level identified in the Transportation System Plan. To ensure proper review and mitigation, a traffic impact study may be required for the proposed zone change if it may impact transportation facilities.

Finding: The proposed amendment to the Forest Grove Zoning Map would be consistent with the Forest Grove Comprehensive Plan Map, which informed the Forest Grove Transportation System Plan's assessment of future transportation needs. The future transportation needs assessment was based on the Preferred Land Use Alternative and reflects changes to the City's existing Comprehensive Land Use Plan to encourage more nodal mixed use development. A Transportation Analysis was prepared and is attached as Exhibit C. As noted in this analysis, the proposed map amendments are not expected to substantially impact transportation facilities below the minimum acceptable levels identified in the City's adopted Transportation System Plan. The City's minimum acceptable level of service is LOS D.

Review Criteria 10.2.770. E: Public facilities and services for water supply, sanitary waste disposal, stormwater disposal, and police and fire protection are capable of supporting the uses allowed by the zone. Adequacy of services is based on the projected service demands of the site and the ability of the public services to accommodate those demands.

Finding: The proposed amendments to consolidate the Town Center Support with Town Center Transition zones and to expand the Town Center boundary to the east will occur in a developed area, currently provided with public facilities and services. The distinction between Town Center Support and Town Center transition is minor. The proposed change will not affect demand for public services currently planned for as reflected in the Comprehensive Plan. Service demand related to changing the zoning in the Town Center Expansion area from Community Commercial to Town Center Transition was considered during the Comprehensive Plan update. The Forest Grove Water Master Plan Updated (2010) indicates Forest Grove has a sufficient water supply for meeting service needs for at least the next twenty years. The Forest Grove Sanitary Sewer Master Plan update (2007) identifies a capital improvement project in the Town Center and Town Center Expansion areas. This project will increase the 8-inch and 10-inch diameter lines along 19th Avenue from Birch Street to A Street to the B Street pump station. This improvement will improve the City's ability to serve development promoted by the proposed zone changes. The Forest Grove Storm Drainage Master Plan (2007) does not indicate a need for general storm drainage improvements in the areas affected by the zone changes. Necessary improvements meeting Clean Water Services and City standards resulting from individual development projects will be identified as part of the development review process. Projected

service demands to Police and Fire resulting from the proposed zone changes, if any, will be addressed through the annual City budgeting process.

Review Criteria 10.2.770. F: The establishment of a zone district is not subject to the meeting of conditions.

Finding: No conditions are proposed

ORDINANCE NO. 2016-07

**ORDINANCE AMENDING FOREST GROVE DEVELOPMENT CODE ARTICLE 3, ARTICLE 7, AND ARTICLE 8 TO ADDRESS MARIJUANA FACILITIES;
FILE NO. 311-15-00028-PLNG**

WHEREAS, the City Council on April 28, 2015, adopted Ordinance Numbers 2015-02 and 2015-03 amending the Development and City codes to establish regulations for the development and operation of Medical Marijuana Dispensaries;

WHEREAS, the Oregon Legislature enacted House Bill 3400 (ORS 475B 2015), which pertains to laws for regulation and control of marijuana related activities;

WHEREAS, House Bill 3400, authorizes local jurisdictions to adopt time, place and manner regulations for the operation of marijuana facilities;

WHEREAS, House Bill 3400 prohibits medical and commercial marijuana processors if processing marijuana extracts and marijuana dispensaries and commercial retail stores in exclusively zoned residential districts;

WHEREAS, staff has prepared a code analysis pertaining to the allowance by zone district, and development and operational requirements for all regulated marijuana activities regulated by the State;

WHEREAS, residential zone districts in Forest Grove may not be considered exclusively zoned residential districts because other uses such as neighborhood stores, parks and other non-residential uses are allowed in the various residential zone districts;

WHEREAS, the City Council during work sessions held on September 14 and November 9, 2015, expressed that marijuana facilities regulated by the State should not be banned in Forest Grove but should be properly placed including avoiding any facilities in residential areas;

WHEREAS, the City has prepared proposed amendments to the Development Code pertaining to the placement and requirements for marijuana activities; and

WHEREAS, the Planning Commission held a duly-noticed Public Hearing on the proposed amendments to the Development Code on January 19, 2016.

WHEREAS, the Planning Commission's Decision No. 16-03 recommends the City Council approve the proposed changes to the Development Code with amendments; and

WHEREAS, the City Council held a duly-noticed Public Hearing on March 14 and

continued the hearing on March 28, 2016, to consider the Planning Commission's recommendation.

NOW, THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1. The City Council adopts the Planning Commission findings dated February 4, 2016.

Section 2. Based on the findings set forth in Section 1, the City Council adopts the Planning Commission recommendation and approves the Development Code text amendment as shown on Exhibit A.

Section 3. All other ordinances or portions thereof inconsistent or conflicting with this ordinance or any portions hereof are hereby repealed to the extent of such inconsistency or conflict.

Section 4. This ordinance shall be effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 14th day of March, 2016.

PASSED the second reading this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

Exhibit A

Marijuana Activities

Development Code Amendments

File Number 311-15-000028 - PLNG

The following are proposed amendments to the Development Code pertaining to marijuana related activities:

1. Prohibit Marijuana Retail Sales in Residential Districts

Amend Footnote 10 from Table 3-2 – Residential Zones. It pertains to General Retail Sales – Oriented use in the Residential Zone Districts including R-10, R-7, R-5, RML and RMH. The amendment would be as follows:

[10] A neighborhood store, limited to a size of 2,000 square feet, is permitted with approval of a conditional use permit. Any No retail outlet regulated by the Oregon Liquor Control Commission other than operating under an Off-Premises Sales License is not allowed in any residential zone district. Stores must be located along a collector street.

Off-Premises Sales License pertains to the sale of sealed malt beverages, wine and cider. This amendment would prohibit any retail outlet for marijuana or hard liquor in residential areas (in the event an initiative is approved to allow hard liquor sales in stores) while allowing typical convenience store items such as beer, wine and cider.

~~2. Amend the definition of "Neighborhood Stores" to explicitly exclude marijuana retail outlets in Residential Districts~~

~~Amend Development Code Section 10.12.210 N1 to revise definition of "neighborhood store" to not allow retailers to locate in residential districts as follows:~~

~~N1 — Neighborhood Store. A commercial structure of 2,000 square feet or less providing goods and services to a neighborhood area. Such store is intended to provide convenience items to reduce the need for longer trips, and is not intended to provide goods and services which would encourage trips from throughout the community. Stores for marijuana retail use is not included in the definition of neighborhood store.~~

3. Prohibit Marijuana Grow Sites within Residential Zone Districts

Underlined – Proposed additional text;

Double Underlined – Proposed additional text by Planning Commission

~~Strikeout~~ – Proposed text to be removed

Amend Footnote 11 on Table 3-2, Residential Zones: Use Tables to prohibit grow sites in Residential Districts as follows:

[11] Agriculture uses such as truck farming and horticulture are permitted. Commercial agriculture uses including but not limited to marijuana grow site for commercial or medical purposes as regulated by the State and buildings and the keeping of livestock and poultry (other than ordinary household pets, and domesticated fowl as identified in footnote (1±2)) are not permitted}.

4. Prohibit Marijuana Retail outlets in the Neighborhood Commercial District and prohibit taverns, lounges or other commercial uses intended for the on-site consumption of marijuana related products

Amend Table 3-10, Commercial Use Zones Use Table as follows:

TABLE 3-10 Commercial Zones Use Table

USE CATEGORY	NC	CC
<u>RESIDENTIAL</u>		
Household Living	L ^[1]	L ^[2]
Group Living	N	P
Transitional Housing	N	C
Home Occupation	L ^[3]	L ^[3]
Bed and Breakfast	L ^[4]	P
CIVIC / INSTITUTIONAL		
Basic Utilities	P	P
Major Utility Transmission Facilities	C	C
Colleges	N	C
Community Recreation	N	P
Cultural Institutions	P	P
Day Care	P	P
Emergency Services	C	C
Postal Services	C	P
Religious Institutions	C	P
Schools	C	C
Social/ Fraternal Clubs / Lodges	C	P
COMMERCIAL		
Commercial Lodging	N	L ^[5]
Eating and Drinking Establishments	L ^[6] [Z]	p ^[Z]

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Entertainment – Oriented: - Major Event Entertainment	N	N
- Outdoor Entertainment	N	N
- Indoor Entertainment	N	p ^[7]
General Retail: - Sales – Oriented	P L ^[8]	P L ^[8]
- Personal Services	P	P
- Repair – Oriented	P	P
- Bulk Sales	N	P
- Outdoor Sales	L ^[79]	L ^[79]
- Animal – Related	N	P
Medical Centers	N	L ^[11 13]
Motor Vehicle Related: - Motor Vehicles Sale / Rental	N	L ^[8 10]
- Motor Vehicle Servicing / Repair	N	P
- Motor Vehicle Fuel Sales	p ^[9]	P
Non-Accessory Parking	N	P
Office	P	P
Self-Service Storage	N	C
INDUSTRIAL		
Industrial Services	N	N
Manufacturing and Production: - Light Industrial	N	C ^[10 12]
- General Industrial	N	N
Railroad Yards	N	N
Research and Development	N	N
Warehouse / Freight Movement	N	N
Waste – Related	N	N
Wholesale Sales	N	N
OTHER		
Agriculture / Horticulture	L ^[13 15]	L ^[13 15]
Cemeteries	N	N
Detention Facilities	N	N
Mining	N	N
Wireless Communication Facilities	L ^[12 14]	L ^[12 14]

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Double Underlined – Proposed additional text by Planning Commission

~~Strikeout~~ – Proposed text to be removed

P=Permitted

L=Limited

C=Conditional Use

N=Not Permitted

Footnotes:

- [1] Residential units are permitted in conjunction with a mixed-use development in the NC zone, at a minimum density of 3.48 and a maximum density of 4.35 dwelling units/net acre.
- [2] Residential units are permitted as a stand-alone use or as part of a mixed-use development in the CC zone, at a minimum density of 16.22 units/net acre and a maximum density of 20.28 units/net acre. Stand-alone residential projects shall have a minimum density of 16.22 units/net acre. There is no minimum density requirement when residential units are constructed over first floor commercial uses.
- [3] Home occupations permitted as an accessory use to residential development, subject to compliance with the home occupation standards in Article 7.
- [4] Bed & breakfast inn limited to three (3) guest rooms in the NC zone, subject to compliance with the bed & breakfast inn standards in Article 7.
- [5] Recreational Vehicle Parks require obtaining a conditional use permit and compliance with the requirements of Section 10.5.500 et. seq. All other commercial lodging uses are permitted.
- [6] Restaurants are permitted in the NC zone (drive-through service is prohibited).
- [7] Establishments are prohibited where a portion or all of the facility is intended for the on-site consumption of marijuana, cannabinoid concentrate or cannabinoid extract.
- [8] Marijuana retailers are prohibited in the Neighborhood Commercial Zone District and permitted within the Community Commercial Zone District consistent with the locational requirements of State laws ORS 475B (2015) and comply with the requirements of Section 10.8.1100 of this code.
- [7 9] Outdoor sales in the NC zone are limited to plants and produce. Outdoor sales areas in the CC zone must be set back at least ten (10) feet from street lot lines and lot lines abutting residential zones and the setback area must be landscaped.
- [8 10] Cleaning, sales and repair of motor vehicles and light equipment is permitted outright in the CC zone; sales and rental or heavy vehicles and farm equipment and/or storage of recreational vehicles and boats permitted with conditional use approval.
- [9 11] Automobile service station in the NC zone is limited to fuel sales and incidental repair service.
- [10 12] As a conditional use pursuant to Section 10.2.200 et. seq., light industrial uses limited within a building no larger than 5,000 square feet in size with no visible emissions or odor outside the building, and with the added criteria that such use does not detract from the commercial viability of the area.

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Double Underlined – Proposed additional text by Planning Commission

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~~[11~~ 13] Medical marijuana dispensaries must be located consistent with the requirements of ORS 475.314 State laws ORS 475B (2015 and comply with the provisions of Section 10.8.1100 of this code.

~~[12~~ 14] Wireless communication facilities are regulated by the standards in Article 7.

~~[13~~ 15] Domesticated fowl are allowed in conjunction with existing single-family uses and primarily for personal use. Domesticated fowl are allowed subject to these conditions.

- a. Up to 4 adult fowl over 6 months of age may be kept on any lot with a minimum area of 5,000 square feet. One additional adult fowl shall be permitted for each 2,000 square feet of additional lot area, up to a maximum of 12 fowl.
- b. No roosters shall be permitted.
- c. Animal waste matter shall not be allowed to accumulate.
- d. All animal food shall be stored in metal or other rodent-proof containers.
- e. Fencing shall be designed and constructed to confine all animals to the owner's property.
- f. All structures that house fowl shall be located at least 20 feet from all residences (except the animal owner's).
- g. All structures that house fowl shall be located at least 5 feet from any side or rear property line.

5. Allow Medical Marijuana Dispensaries and Marijuana Retail activities and prohibit taverns, lounges or other commercial uses intended for the on-site consumption of marijuana related products within the Town Center Zone Districts

Amend Table 3-12, Town Center Zones: Use Table to allow medical marijuana dispensaries and marijuana retailers to locate in Town Center Districts.

**TABLE 3-12
Town Center Zones: Use Table**

USE CATEGORY	TC-Core	TC-Transition	TC-Support
<u>RESIDENTIAL</u> Household Living	L ^[1]	L ^[1]	L ^[1]
Group Living	P ^[1]	P	P
Transitional Housing	N	C	C
Home Occupation	L ^[2]	L ^[2]	L ^[2]
Bed and Breakfast	C ^[2]	P	P

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<u>CIVIC / INSTITUTIONAL</u>	P	P	P
Basic Utilities			
Major Utility Transmission Facilities	C	C	C
Colleges	C	C	C
Community Recreation	N	P	P
Cultural Institutions	P	P	P
Day Care	P	P	P
Emergency Services	C	C	C
Postal Services	C	P	P
Religious Institutions	C	P	P
Schools	C	C	C
Social/ Fraternal Clubs / Lodges	C	P	P

<u>COMMERCIAL</u>	L ^[3]	L ^[3]	L ^[3]
Commercial Lodging			
Eating and Drinking Establishments	p ^[4] ^[5]	p ^[4] ^[5]	p ^[5]
Entertainment – Oriented: - Major Event Entertainment	N	N	C ^[5]
- Outdoor Entertainment	N	N	N
- Indoor Entertainment	p ^[5]	p ^[5]	p ^[5]
General Retail: - Sales – Oriented	p ^[2]	p ^[2]	p ^[2]
- Personal Services	P	P	P
- Repair – Oriented	P	P	P
- Bulk Sales	L ^[5 6]	L ^[5 6]	L ^[5 6]
- Outdoor Sales	N	N	N
- Animal – Related	N	N	N
Medical Centers	N	C ^[6 2]	C ^[6 2]
Motor Vehicle Related:			
- Motor Vehicles Sale / Rental	N	N	N
- Motor Vehicle Servicing / Repair	N	N	N
- Motor Vehicle Fuel Sales	N	N	N
Non-Accessory Parking	N	C	C
Office	L ^[3]	P	P
Self-Service Storage	N	N	N

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<u>INDUSTRIAL</u>			
Industrial Services	N	N	N
Manufacturing and Production:			
- Light Industrial	N	C	C
- General Industrial	N	N	N
Call Centers	L ^{[7][8][9]}	L ^[7 8]	L ^[7 8]
Railroad Yards	N	N	N
Research and Development	N	N	P
Warehouse / Freight Movement	N	N	N
Waste – Related	N	N	N
Wholesale Sales	N	N	N
<u>OTHER</u>		N	N
Agriculture / Horticulture	N		
Cemeteries	N	N	N
Detention Facilities	N	N	N
Mining	N	N	N
Wireless Communication Facilities	L ^[8 10]	L ^[8 10]	L ^[8 10]
Information	L ^{[9][10]} [11][12]	L ^[9 11]	L ^[9 11]

P = Permitted L = Limited C = Conditional Use N = Not Permitted

Footnotes:

- [1] New dwellings in the TCC zone are only permitted on or above the 2nd floor. There are no minimum density requirements when housing is part of a mixed-use building. In the TCT and TCS zones new dwellings are permitted as "stand-alone" developments or as part of mixed-use developments, but must meet density requirements.
- [2] Home occupations are permitted as an accessory use to residential uses, subject to compliance with the home occupation standards in Article 7.
- [3] Offices only permitted as part of a ground-floor retail or personal service use or as a stand-alone use above the first floor in the TC-Core zone. Recreational vehicle parks are prohibited in all districts.
- [4] Drive through service is prohibited from restaurants in the TC-Core and TC-Transition zones.
- [5] Establishments are prohibited where a portion or all of the facility is intended for the on-site consumption of marijuana, cannabinoid concentrate or cannabinoid extract.

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- [5 6] Bulk sales stores with a ground floor building footprint smaller than 10,000 square feet are permitted. All merchandise must be enclosed within a building. All other bulk sales are prohibited.
- [6 7] Medical marijuana dispensaries are prohibited and marijuana retailers are permitted consistent with the locational requirements of State laws ORS 475B (2015) and comply with the requirements of Section 10.8.1100 of this code.
- [7 8] Permitted where there are no off premise impacts and no product is transported from the site. Centers with any offsite impacts or transport products from the site are to be located in either Light or General Industrial districts.
- [8 9] Call Centers shall not be allowed on the ground floor in the TC-Core Zone District.
- [9 10] Wireless communication facilities are regulated by the standards in Article 7.
- [10 11] Permitted where there are no off premise impacts.
- [11 12] Information business is not allowed on the ground floor in the TC-Core Zone District.

6. Prohibit Marijuana related activities as a Home Occupation

Amend Section 10.7.065, Standards to prohibit marijuana related activities as a home occupation as follows:

- 10.7.065 STANDARDS

The home occupation shall not change the residential character of the dwelling and shall meet all of the following standards and limitations:

- A. Any product produced on-site for sale must be hand manufactured or grown using only hand tools or domestic mechanical equipment. Such domestic mechanical equipment shall not exceed horsepower or other measurements of power, which would typically be used by a residential homeowner.
- B. There shall be no outdoor storage of material or products on the premises. Indoor storage of material or products shall not exceed the limitations imposed by the Building, Fire, Health and Housing Codes.
- C. The home occupation shall not generate vehicular traffic measurably in excess of that normally associated with single-family uses.
- D. No more than 20% of the floor area of the dwelling shall be used for the home occupation.
- E. One sign shall be permitted, not exceeding six (6) square feet in area, non-illuminated and professionally prepared.

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Double Underlined – Proposed additional text by Planning Commission

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- F. The home occupation shall not cause the elimination of required off-street parking.
- G. The home occupation shall not cause any external effects such as increased noise, excessive lighting, or excessive odor that is incompatible with the characteristics of the residential zone, or in violation of any applicable government code.
- H. There shall be no more than two (2) outside paid employees.
- I. A business occupancy permit is required for the home occupation.
- J. All marijuana related activities are prohibited as a home occupation.

7. Revise Development Standards to apply to Marijuana retailer activities

Amend Section 10.8.1100 to include marijuana retailers with development requirements:

10.8.1100 MEDICAL MARIJUANA DISPENSARIES AND MARIJUANA RETAILERS

A medical marijuana dispensary and marijuana retailer activities shall comply with the following design standards and operational requirements in addition to all other applicable City requirements:

- A. The application shall demonstrate compliance with the locational requirements of State laws ORS 475B (2015) and must maintain State certification at all times.
- B. ~~A medical marijuana dispensary in~~ May not be open to the public between the hours of ~~10:00~~ 8:00 p.m. and 8:00 a.m.
- C. Entrances and off-street parking areas ~~for the medical marijuana facility~~ shall be well-lit and not visually obscured from public view / right-of-way.
- D. The facility must be located in a permanent building and may not locate in a trailer, cargo container, or motor vehicle. Outdoor storage of merchandise, raw materials, or other material associated with the facility is prohibited.
- E. Any modification to the site or the exterior of the building housing the facility must be consistent with the Design Standards of Section 10.8.700 et. seq. Security bars or grates on windows and doors are prohibited unless integrated into the design.
- F. The dispensary or retail operation may not have facilities for drive-up use.

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Double Underlined – Proposed additional text by Planning Commission

~~Strikeout~~ – Proposed text to be removed

- G. The dispensary must provide for secure disposal or render impotent marijuana remnants or by-products, or items with marijuana residue of any kind.

8. Add new Development Standards to apply to marijuana activities other than dispensaries and retailers

Currently, there are no development standards that would apply to marijuana activities except for dispensaries and subject to approval, marijuana retail outlets. In review of the requirements, staff did not believe the development requirements for the outlets and dispensaries were appropriate for these other activities. As a result, a new Section 10.8.1110 is proposed as follows:

10.8.1110 OTHER MARIJUANA FACILITIES

The following requirements to marijuana related warehouse, processing and grow facilities.

- A. The application shall demonstrate compliance with the locational requirements of State laws ORS 475B (2015) and must maintain State certification at all times.
- B. Entrances and off-street parking areas shall be well-lit and not visually obscured from public view / right-of-way.
- C. With the exception of outdoor grow operations including the use of removable greenhouses, the facility must be located in a permanent building and may not locate in a trailer, cargo container, or motor vehicle. Outdoor storage of merchandise, raw materials, or other material associated with the facility is prohibited.
- D. The facility must provide for secure disposal or render impotent marijuana remnants or by-products, or items with marijuana residue of any kind.
- E. All production, processors, indoor grow operations and outdoor grow operations with greenhouses shall provide a method to control odors. Such facilities shall install and maintain enhanced ventilation systems designed to prevent detection of marijuana odor from adjacent properties or the public right-of-way. The systems shall include the following features:
 - 1. Installation of activated carbon filters on all exhaust outlets to the building exterior;
 - 2. Location of exhaust outlets a minimum of 10 feet from the property line; 3 feet from exterior walls; and 10 feet above finished grade; and
 - 3. Maintenance of negative air pressure within the facility; or

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Double Underlined – Proposed additional text by Planning Commission

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4. An alternative odor control system approved by the Building Official based on a report by a mechanical engineer licensed in the State of Oregon, demonstrating that the alternative system will control odor equally or better than the required activated carbon filtration system.

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ORDINANCE NO. 2016-08

ORDINANCE AMENDING FOREST GROVE CITY CODE CHAPTER 7 BY RENAMING CODE SECTIONS 7.850 THROUGH 7.865 FROM "MEDICAL MARIJUANA DISPENSARIES" TO "MARIJUANA ACTIVITIES"; AMENDING CODE SECTIONS 7.850 THROUGH 7.860 TO ADDRESS MARIJUANA FACILITIES; AND REPEALING PORTIONS OF ORDINANCE NO. 2015-03; FILE NO. 311-15-00028-PLNG

WHEREAS, the City Council on April 28, 2015, adopted Ordinance Numbers 2015-02 and 2015-03 amending the Development and City codes to establish regulations for the development and operation of Medical Marijuana Dispensaries;

WHEREAS, the Oregon Legislature enacted House Bill 3400 (ORS 475B 2015), which pertains to laws for regulation and control of marijuana related activities;

WHEREAS, House Bill 3400, authorizes local jurisdictions to adopt time, place and manner regulations for the operation of marijuana facilities;

WHEREAS, House Bill 3400 prohibits medical and commercial marijuana processors if processing marijuana extracts and marijuana dispensaries and commercial retail stores in exclusively zoned residential districts;

WHEREAS, staff has prepared a code analysis pertaining to the allowance by zone district, and development and operational requirements for all regulated marijuana activities regulated by the State;

WHEREAS, residential zone districts in Forest Grove may not be considered exclusively zoned residential districts because other uses such as neighborhood stores, parks and other non-residential uses are allowed in the various residential zone districts;

WHEREAS, the City Council during work sessions held on September 14 and November 9, 2015, expressed that marijuana facilities regulated by the State should not be banned in Forest Grove but should be properly placed including avoiding any facilities in residential areas;

WHEREAS, the City has prepared proposed amendments to the Development Code pertaining to the placement and requirements for marijuana activities; and

WHEREAS, the City Council held a duly-noticed Public Hearing on March 14 and continued the hearing on March 28, 2016, to consider the Planning Commission's recommendation.

NOW, THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1. Forest Grove City Council hereby amends Forest Grove City Code Chapter 7 by renaming Title of Code Sections 7.850 through 7.865, from "Medical Marijuana Dispensaries" to "Marijuana Activities" and amends Code Sections 7.850 through 7.860 as shown on Exhibit A.

Section 2. The applicable existing Code Sections in Ordinance No. 2015-03 are hereby repealed.

Section 3. This ordinance shall be effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 14th day of March, 2016.

PASSED the second reading this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

Exhibit A

Marijuana Activities

City Code Amendments

File Number 311-15-000028 - PLNG

The following are proposed amendments to the Development Code pertaining to marijuana related activities:

Amend City Code provisions on marijuana activities to address all marijuana related activities

Amend City Code Sections 7.850, 7.855 and 7.860 to establish regulations for all marijuana related activities and prohibit the use of marijuana related products in a commercial establishment.

MEDICAL MARIJUANA DISPENSARIES ACTIVITIES

7.850 Statement of Purpose.

The purpose of sections Forest Grove Municipal Code Sections 7.850 to 7.865 is to promote the public health, safety and general welfare by establishing standards in the City of Forest Grove for the operation of ~~medical marijuana dispensaries~~ related activities as allowed by state law ~~in the City of Forest Grove.~~

7.855 Definitions.

~~Medical Marijuana Dispensary. A facility for selling marijuana and marijuana-related products to medical marijuana card holders and that is registered with the Oregon Health Authority under ORS 475.314.~~

Marijuana Related Activities. An activity involved with the growing, processing, wholesaling or selling of marijuana, cannabinoid product, cannabinoid concentrate, or cannabinoid extract regulated by Oregon Health Authority or the Oregon Liquor Control Commission.

7.860 License Requirements.

- A. ~~A medical marijuana dispensary related activity~~ must comply with all applicable requirements of State laws ORS 475B (2015).

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- B. A ~~medical marijuana dispensary~~ related activity must obtain a City Business License pursuant to Code Sections 7.000 to 7.070 prior to opening.
- C. A medical marijuana dispensary or marijuana retailer may not be open to the public between the hours of 10:00 p.m. and 8:00 a.m.
- D. All products and paraphernalia sold to the public or members of a club or organization must be enclosed in an opaque bag or container upon exiting the a dispensary or retail facility.
- E. A ~~medical marijuana dispensary~~ related activity must provide secure disposal for marijuana remnants or by-products, including any item with marijuana residue.
- F. Commercial establishments are prohibited where a portion or all of the facility is intended for the on-site consumption of marijuana, cannabinoid concentrate or cannabinoid extract.

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Comprehensive Plan Map Amendment Campus Employment (File No. 311-15-0032-PLNG)

City Council
March 28, 2016

A place where families and businesses thrive.

Purpose of Tonight's Hearing

- Council consideration of City-initiated amendment to the Comprehensive Plan Map to:
 - Assign Campus Employment Comprehensive Plan Map Designation to south UGB addition area
- Council review of Planning Commission discussion and recommendation on proposed amendment
- Opportunity for formal public comment before final City Council action on April 11th

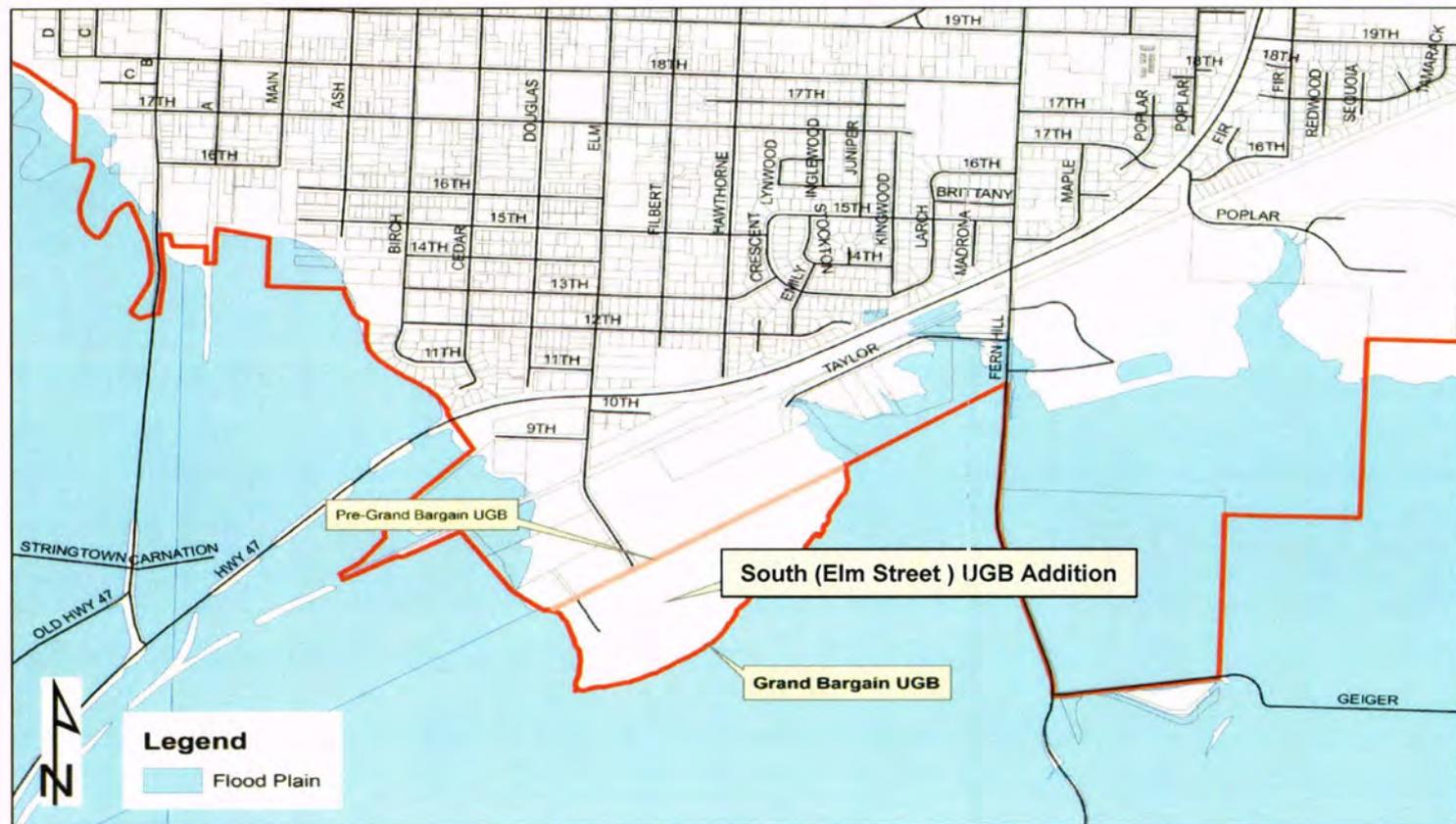
Proposed Amendment Overview

- Subject area added to UGB by Oregon Legislature in 2014 by “Grand Bargain”
- Located at terminus of Elm Street adjacent to General Industrial zoned land
- Currently Zoned Washington County FD-20
- Approximately 38 gross acres
- Site is outside of revised 100-year floodplain
- Campus Employment Designation Allow flexibility for use of property:
 - Manufacturing
 - Office
 - Services
 - Flex-Space

Proposed Amendment Overview

- Vicinity

South (Elm Street) UGB Addition



Alternatives

- Accept Planning Commission recommendation
 - Land use alternatives considered by Planning Commission
 - General Industrial
 - Light Industrial
 - Campus Employment
 - Planning Commission adopted motion supporting Campus Employment (6-0 vote)
- Modify Planning Commission recommendation
 - Consider other Plan Designations
 - General Industrial
 - Light Industrial
- Refer proposal back to Planning Commission for further consideration.

Review Criteria

- Comprehensive Plan
 - Consistency with applicable Comprehensive Plan policies
 - Consistency with Metro Regional Framework Plan
 - Consistency with Metro Urban Growth Management Functional Plan
 - Consistency with Oregon Statewide Land Use Planning Goals
- Findings contained in Planning Commission Decision 2016-04 demonstrates compliance with the review criteria

Staff Recommendation

- Staff recommends that City Council approve an ordinance to designate the 38 acre new south UGB area Campus Employment on the Comprehensive Plan map.

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News Times

Legal Ads/Public Notice

To be published: Wednesday, March 23, 2016

NOTICE OF PUBLIC HEARING FOR THE CITY OF FOREST GROVE

NOTICE IS HEREBY GIVEN that the Forest Grove City Council will hold a Public Hearing on **Monday, March 28, 2016**, at 7:00 PM or thereafter, at the Community Auditorium, 1915 Main Street, to consider the Planning Commission's recommendation to approve the following proposal:

Proposal: Amendment to the Forest Grove Comprehensive Plan Map to designate the south urban growth boundary addition as Campus Employment. The subject property is generally located near 600 Elm Street (Washington County Tax Lots 1S3070000100 & 1S306D000700)

Applicant: City of Forest Grove

File Number: 311-15-00032-PLNG

Criteria: The proposal will be reviewed based on the following considerations:

1. Consistency with Applicable Comprehensive Plan Policies;
2. Consistency with Oregon Statewide Land Use Planning Goals;
3. Consistency with Metro Regional Framework Plan; and
4. Consistency with Metro Urban Growth Management Functional Plan or Regional Transportation Functional Plan, as applicable

All persons will be given reasonable opportunity to give testimony about this proposal responding to the review criteria. If an issue is not raised in the hearing (by person or by letter) or if the issue is not explained in sufficient detail to allow the City Council to respond to the issue, then that issue cannot be used for an appeal to the Land Use Board of Appeals. If additional documents or evidence are provided in support of the application, any party shall be entitled to a continuance of the hearing. Unless there is a continuance, if a participant so requests before the conclusion of the initial evidentiary hearing, the record shall remain open for at least seven days after the hearing. A copy of the report is available for inspection before the hearing at the City Recorder's Office or by visiting the City's website at www.forestgrove-or.gov. Written comments or testimony may be submitted at the hearing or e-mailed to City Recorder's Office, aruggles@forestgrove-or.gov, or sent to P.O. Box 326, 1924 Council Street, Forest Grove, OR 97116, prior to the hearing. For further information, pertaining to this proposal, please contact the Community Development Department, PO Box 326, 1924 Council Street, (503) 992-3226 between 9 a.m. and 5 p.m. Daniel Riordan, Senior Planner, driordan@forestgrove-or.gov.

Anna D. Ruggles, CMC, City Recorder

Published: March 23, 2016

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A place where families and businesses thrive.

CITY COUNCIL STAFF REPORT

TO: *City Council*

FROM: *Jesse VanderZanden, City Manager*

MEETING DATE: *March 14, 2016*

PROJECT TEAM: *Daniel Riordan, Senior Planner; and Jon Holan, Community Development Director*

SUBJECT TITLE: *Public Hearing and First Reading of an Ordinance amending the Comprehensive Plan to designate the south urban growth boundary addition as Campus Employment; File No. 311-15-00032-PLNG*

ACTION REQUESTED: Ordinance Order Resolution Motion Informational

X all that apply

ISSUE STATEMENT:

In 2014, the Oregon Legislature approved House Bill 4078 designating urban reserves throughout the region. This legislation also modified the Portland regional urban growth boundary including a roughly 38 acre area south of the Taylor Way Industrial Park adjacent to Forest Grove (Attachment A). This area is within the Forest Grove planning area. Under Title 11 of the Metro Urban Growth Management Functional Plan the City must amend the Forest Grove Comprehensive Plan Map and assign a City Comprehensive Plan Map designation to the subject property. The City Council has several options for designating the subject property as described in this memorandum.

BACKGROUND:

The subject area was added to the UGB in 2014 and is currently outside the city limits. The area is currently planned and zoned by Washington County as Future Development with a 20 acre minimum lot size (FD-20). FD-20 is a temporary “holding zone” applied to unincorporated areas within the urban growth boundary. Typically, when property is annexed into the City the zoning designation applied to the property is the one that most closely matches the City’s Comprehensive Plan designation. Since this area was recently added to the urban growth boundary and has no City Plan designation one must be assigned. Assigning a City Plan designation will address Title 11 of the Metro Urban Growth Management Functional plan related to planning for new urban areas and will also streamline the annexation and development process.

The Planning Commission considered the proposal on February 15, 2016 and unanimously recommended approval on a 6-0 vote. The staff report is attached for reference (Attachment B). The Planning Commission decision (Decision 2016-04) is also attached demonstrating compliance with the applicable review criteria (Attachment C).

On March 3, 2016, the Economic Development Commission considered and endorsed designating the subject property Campus Employment.

FINANCIAL IMPACT:

Designating the subject property Campus Employment on the Comprehensive Plan map has no financial impact on the City. Subsequent development will require public services, some of which may have fiscal impacts on the City. However, any land use designation for the site will result in similar potential fiscal impacts.

ALTERNATIVES:

1. Accept Planning Commission Recommendation.

The Planning Commission considered several land use alternatives for the subject property including:

- General Industrial;
- Light Industrial; and
- Campus Employment

The Planning Commission adopted a motion recommending the Campus Employment designation. The Campus Employment designation provides flexibility for use of the property including, but not limited to manufacturing, office, services, and flexible use space.

2. Modify Planning Commission Recommendation with Supportive Findings.
3. Refer the recommendation back to the Planning Commission for further consideration.

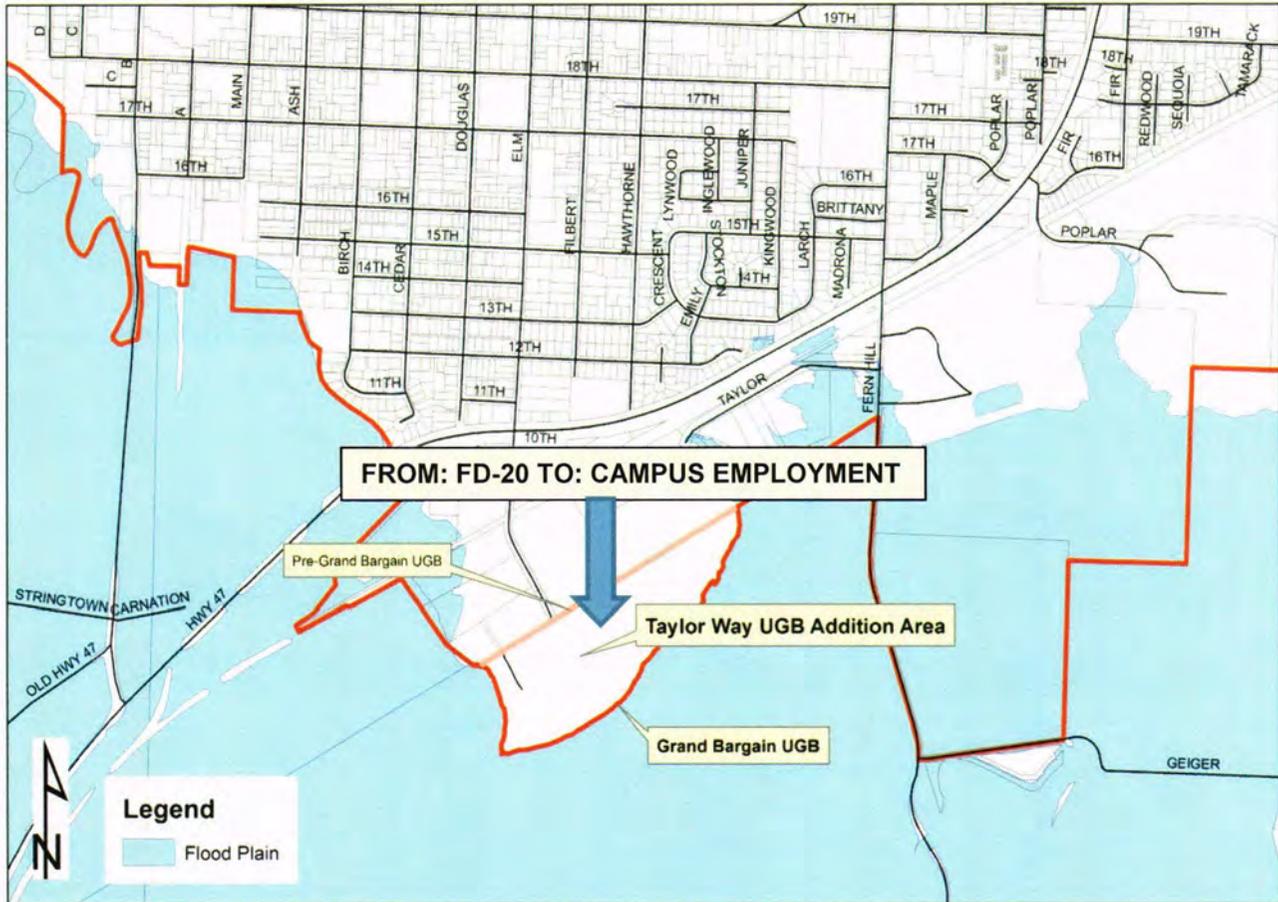
STAFF RECOMMENDATION:

Staff recommends City Council adopt the ordinance for first reading March 28 and second reading April 11, 2016, approving the proposed Comprehensive Plan Map amendment to designate the 38 acre south urban growth boundary addition area Campus Employment as recommended by the Planning Commission.

ATTACHMENTS:

- A. Vicinity Map
- B. Planning Commission Staff Report, February 15, 2016
- C. Planning Commission Decision 2016-04

**COMPREHENSIVE PLAN AMENDMENT
FILE NO. 311-15-00032-PLNG**





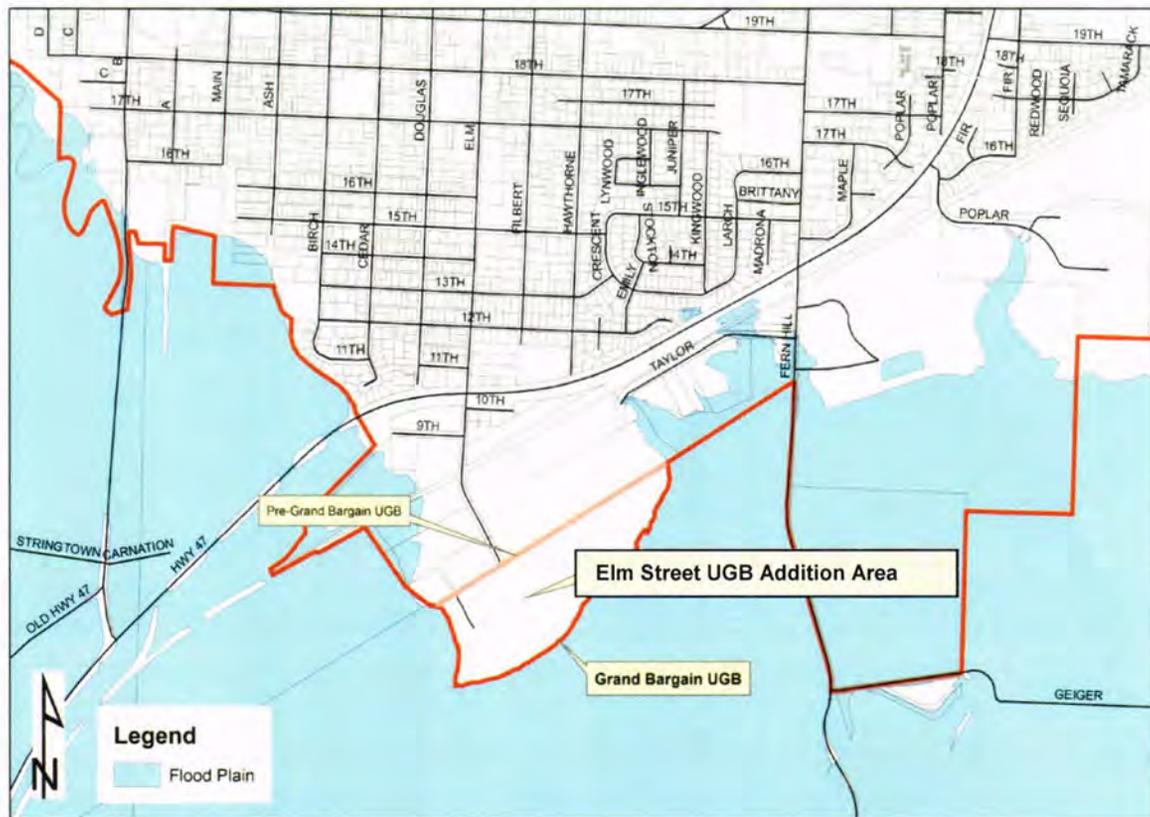
**Comprehensive Plan Map Amendment
Staff Report and Recommendation**
Community Development Department, Planning Division

Report Date:	February 8, 2016
Hearing Date:	February 15, 2016
Request:	Planning Commission recommendation to assign the Campus Employment Comprehensive Plan designation to approximately 38 acres of land located south of the BPA power line transmission easement and south of the Elm Street terminus
File Number	311-15-00032-PLNG
Property Location:	2014 Urban Growth Boundary addition area south of the Taylor Industrial Park
Legal Description:	1S3070000100, 1S306D000700
Owner/Applicants:	Owner: Hally L. and Mary J. Haworth Applicant: City of Forest Grove
Comprehensive Plan Map Designations	Washington County FD-20 to City of Forest Grove Campus Employment
Zoning Map Designations	Washington County FD-20
Review Process	Type III (Quasi-Judicial)
Applicable Standards and Criteria	Statewide Land Use Planning Goals Forest Grove Comprehensive Plan Policies Oregon Transportation Planning Rule (OAR 660-012-0060) Metro Framework Plan Metro Urban Growth Management Functional Plan Metro Regional Transportation Functional Plan
Reviewing Staff	Daniel Riordan, Senior Planner Jon Holan, Community Development Director
Recommendation	Staff recommends the Planning Commission recommend City Council approval of the proposed Comprehensive Plan Map amendment to designate the 38 acre urban growth boundary addition area Campus Employment.
Report Contents	Section I: Background Section II: Existing Conditions Section III: Alternatives Analysis Section IV: Review Criteria and Findings of Fact Section V: Recommendation

I. BACKGROUND

In 2014, the Oregon Legislature approved House Bill which modified and then enacted urban and rural reserves for Washington County. The bill, also known as the “Grand Bargain”, modified the Portland regional urban growth boundary including two locations adjacent to the Forest Grove planning area. One UGB modification resulted in the addition of 235 acres of land north of David Hill Road and west of Highway 47. Land use in the David Hill area is being considered as part of the Westside Planning Project. The other modification to the UGB made by HB 4078 is the addition of approximately 38 gross acres into the urban growth boundary south of the Taylor Industrial Park shown on the map below. It is the second modification that is the subject of this report as explained below.

Vicinity Map



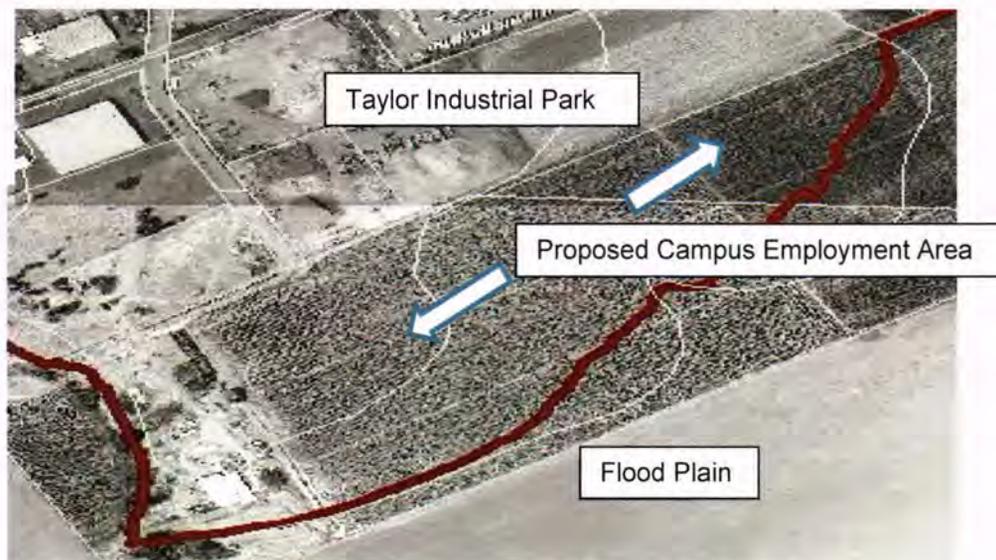
Now that the area is in the UGB, the City is required, under Title 11 of the Metro Urban Growth Regional Functional Plan, to assign a Comprehensive Plan designation to the property. The Comprehensive Plan designation shows how land may be developed in the future. The Comprehensive Plan designation also guides the zoning of property when annexed by the City.

The Planning Commission has several alternatives to consider regarding a Comprehensive Plan designation for the new UGB area. In general, alternatives include applying the City's General Industrial, Light Industrial or the new Campus Employment designation created as part of the Comprehensive Plan update in 2014. These alternatives are described in detail in Section III of this report. For the reasons discussed in this report the staff recommendation is to apply the Campus Employment Comprehensive Plan designation to subject property.

II. EXISTING CONDITIONS

Since the area was added to UGB, and is currently outside the city limits, the subject property has a Washington County Comprehensive Plan designation. The existing Washington County Comprehensive Plan and Zoning designation is FD-20 (Future Development with a 20 acre minimum lot size). This is a "holding zone" applied to unincorporated areas intended for future urban development. The Washington County zoning designation will remain until the area is annexed. Typically, when property is annexed the new zoning designation applied to the property will be the one that most closely matches the City's Comprehensive Plan designation.

As noted above, the subject property is approximately 38 gross acres in area. The property consists of parts of two tax lots under single ownership. As the image below indicates, the site is vacant and is currently being farmed. The entire 38 acre area is located above the pending/revised 100-year floodplain which follows the urban growth boundary (shown by the red line). The subject area is generally unconstrained by natural features. The property is general flat with a slight slope toward the Tualatin River floodplain. Given these site characteristics the area is suitable for future development.



Transportation

The subject property is located at terminus of Elm Street. Elm Street is classified as a collector street in the Forest Grove Transportation System Plan (2014). Elm Street in this area is two lanes. Elm Street provides direct access to Oregon Highway 47.

The image below shows Elm Street looking north toward Highway 47 in the vicinity of the subject property. Elm Street is two lanes with a parkway and sidewalks on both sides.



The image below shows Elm Street near its terminus with subject property. The subject property is in the distance near the vehicle shown in the picture.



Highway 47 is classified as a primary arterial road in the Forest Grove Transportation Plan. Highway 47 functions as a bypass route and is classified as a regional route in the Oregon Highway Plan. The image below shows Highway 47 looking northeast at the Elm Street intersection. Highway 47 is two lanes with a turn lane at the intersection.



The image below shows Highway 47 looking southwest near the Elm Street intersection. This image shows the through lane and left turn lane from Highway 47 onto Elm Street.



Data for 2013 from the Oregon Department of Transportation indicates that average daily traffic on Highway 47 at Elm Street is approximately 11,400 vehicles per day. This amount is lower than shown in the TSP at 14,500 vehicles per day. This lower traffic volume may be due to the 2008/2009 economic downturn. The Forest Grove Transportation System Plan indicates that approximately 10% of the average daily traffic is freight traffic. An assessment of potential traffic impacts is provided below in the alternatives analysis. The traffic assessment is based on a comparison of possible Comprehensive Plan designations that could apply to the subject property.

Water

Municipal water is available near the subject property. The area is served with two water lines. One line is located in the Elm Street right-of-way approximately 600 feet to the north of the site. The other line serves the Kerr and Woodburn properties immediately to the north of the subject property. Water lines must be extended to serve development in the UGB addition area. Possible extension of water lines will be addressed at time of development review.

Storm Sewer

The Forest Grove Stormwater Master Plan Update shows an 18" storm water conveyance line in the Elm Street right-of-way approximately 600 feet to the north of the subject property. Necessary storm water conveyance improvements will be addressed at time of development review.

Sanitary Sewer

The City's Sanitary Sewer Master Plan shows a line approximately 300 feet to the north of the subject property. Required sanitary sewer improvements to serve development will be addressed as part of the development review process. The ability to provide sanitary sewer to the entire UGB addition area may be a limiting factor as to how much of the 38 acre site is developable. The constraint to sanitary sewer conveyance is due to the depth and slope of the existing sanitary sewer line. The reason for this is sewage is typically conveyed for treatment by gravity.

III. ALTERNATIVES ANALYSIS

The Planning Commission has several alternatives to consider for designating the subject property. Alternatives include:

- General Industrial;
- Light Industrial; or
- Campus Employment

The Alternatives above were selected based on the existing development pattern and the site's location adjacent to the Taylor Way Industrial Park. The options are discussed further below.

General Industrial

Article 3 of the Development Code describes the General Industrial zone as intended for industrial uses generally incompatible with residential development due to operational characteristics. Uses within the General Industrial zone typically require extensive outdoor areas for business activities, product storage or display. General Industrial uses include those involved in the processing of raw materials into refined products and/or industrial uses that have external impacts.

Light Industrial

Article 3 of the Development Code describes the Light Industrial zone as intended for a wide variety of manufacturing and other industrial uses with controlled external impacts. Such industries are often involved with secondary processing of materials into components, the assembly of components into finished products, transportation, communications and utilities, wholesaling and warehousing. Activities associated with these uses occur within enclosed buildings. On a limited basis, supporting commercial and offices are permitted in the Light Industrial zone.

Campus Employment

Campus Employment is a new employment designation described in the updated Forest Grove Comprehensive Plan. The Campus Employment designation is intended for development of industrial and office parks with a high level of amenity value including landscaping and open space. Typical uses include high technology companies, call centers, research and development firms, and business incubators. Similar to light industrial activities take place with enclosed structures. The Campus Employment designation includes limited supportive retail development, business services and offices.

The Planning Commission considered amendments to the Development Code to establish a zoning district – the Business Industrial Park zone - to implement the Campus Employment Comprehensive Plan designation. The text of the Business Industrial Park zone is attached for reference. The attached indicates the permitted, limited, and conditional uses allowed by the zone.

Economic Opportunities Analysis

The Forest Grove Economic Opportunity Analysis (EOA) identifies a need for campus employment uses. The UGB addition area provides an opportunity to fulfill this need as explained below.

The EOA shows that Forest Grove has growth potential to create demand for between 56 and 71 acres of business park land some of which could be absorbed by the Campus Employment designation. The EOA goes on to say, although demand is not sufficient for a single typical, 50 acre business park site, business park development is estimated as the largest category of demand among office spaces classes which include business parks, medium-size office sites, small-size office sites).

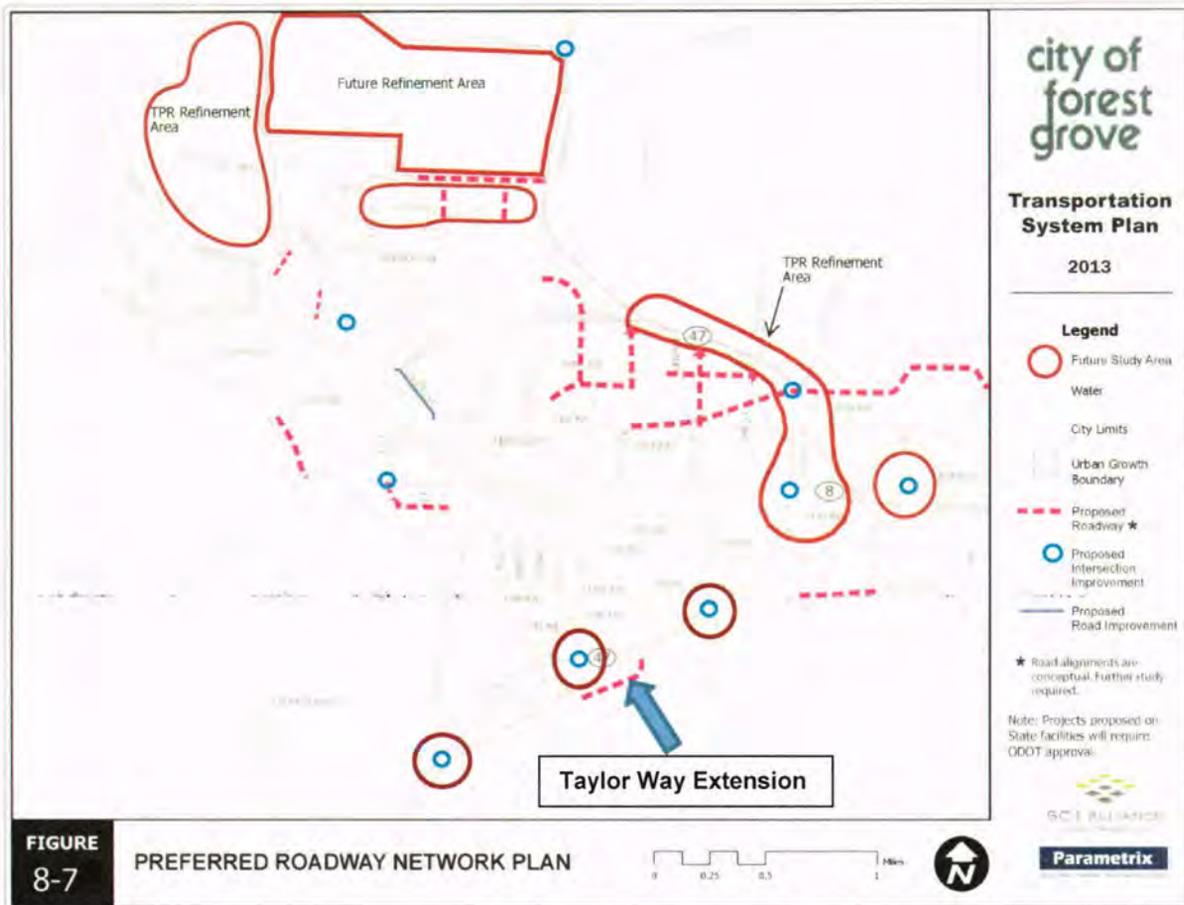
The EOA also states Forest Grove currently has a total supply of 2 vacant, buildable acres, suitable for office development. All of this supply is in the small category (10 acres or less). Furthermore, this supply is located in downtown Forest Grove. The EOA underscores that Forest Grove has no site availability suitable for larger, predominantly office park development and outside downtown Forest Grove.

Finally, the EOA indicates Forest Grove has an oversupply of approximately 127 acres of industrial land for the 20-year planning period covered by the EOA (planning horizon Year 2028). Applying the Campus Employment designation to the subject 38 acres provides an opportunity for the City to address an unmet land use need for business park development. Whereas, applying either the General Industrial or Light Industrial designation exacerbates the documented oversupply of land in the industrial category.

Given the existing development pattern near the property, including the property's location adjacent to the Taylor Way Industrial Park to the north and agricultural land to the south, the recommended plan designation for this site is Campus Employment. The Campus Employment designation will provide a transition from more intensive development near Highway 47 and agricultural activities. The Campus Employment designation also responds to a need for business park land as described in the EOA and elsewhere in this report.

Traffic Analysis

The proposed Comprehensive Plan amendment will allow for development of the subject property when annexed and zoned by the City. Future development will impact the Highway 47/Elm Street intersection. The traffic analysis presented in this section assumes that all traffic flows through the Highway 47/Elm Street intersection. As shown below, the TSP includes a future extension of Taylor Way from its terminus to Elm Street. When completed, some trips could be diverted to Fern Hill Road via Taylor Way from the proposed Campus Employment area.



The table below shows that the Highway 47/Elm Street intersection operates within acceptable standards. The minimum acceptable level of service adopted by the City is LOS D. The table also shows the current volume to capacity ratio is well below the operational standard of 0.99

Current PM Peak Hour Intersection Level of Service

No.	Intersection	Operational Standard	Level of Service (LOS) ¹	Average Delay* (Seconds) ²	Volume / Capacity (V/C) ²
<i>Unsignalized Intersections</i>					
18	Highway 47/Elm Street	V/C=0.99	A/D	31.3	0.45

¹ First value is free movement (Highway 47), second value is worst stopped movement (Elm Street).

² Worst (Elm Street) stopped movement for minor street average delay reported for unsignalized intersections.

Operation of the Highway 47/Elm Street intersection is expected to degrade by the year 2035. The table below expected operational characteristics with new street connections identified in the TSP. The table does not show how the intersection may operate with intersection improvements such as a traffic signal. Therefore, this is a

worst case scenario. It should also be noted the Highway 47 movement operates at a level of service A in 2035. It is the Elm Street movement that operates below standard. To address future operation of the intersection the TSP includes a project for future intersection improvements. The TSP shows that the Highway 47/Elm Street intersection meets preliminary warrants for a traffic signal and is a possible solution. Installation of a traffic signal requires ODOT approval as owner of Highway 47.

2035 PM Peak Hour Traffic Operations with Added Local Street Connectivity

No.	Intersection	Operational Standard	2035 Preferred Alternative		
			Level of Service (LOS) ¹	Average Delay (Seconds) ²	Volume / Capacity (V/C) ²
18	Highway 47/Elm Street	V/C=0.99	A/F	172.2	1.20

3 First value is free movement (Highway 47); second value is worst stopped movement (Elm Street).
 4 Worst (Elm Street) stopped movement for minor street average delay reported for unsignalized intersections.

The proposed Comprehensive Plan amendment will allow for future development after the property is annexed and assigned a City zoning designation. The subject property was added to the UGB after the Transportation System Plan was updated. Therefore, traffic resulting from future development will be above what was assumed in the TSP. Several land uses from the Institute of Traffic Engineers (ITE) Trip General Manual, Seventh Edition, was used to assess potential traffic impacts related to the proposed comprehensive plan amendment. The results from the analysis are provided below. The results are based using three ITE land use classifications. The ITE classifications are:

Industrial Park (ITE 130): Industrial parks contain a number of industrial or related facilities. The area characterized by a mix of manufacturing, service and warehouse facilities with a wide variation in the proportion of each type of use. Industrial parks may contain a number of small businesses or one or two dominant industries. The Campus Employment designation would also allow for limited office and support services such as banks, restaurants, and service stations.

General Heavy Industrial (ITE 120): According to the ITE Manual, 7th Edition, heavy industrial facilities usually have a high number of employees per industrial plant and could also be categorized as manufacturing facilities (ITE Land Use 140). Heavy industrial uses are limited to the manufacture of large items.

Light Industrial (ITE Land Use 110): According to the ITE Manual, 7th Edition, light industrial facilities usually employ fewer than 500 persons. Light industrial facilities have emphasis on activities other than manufacturing and typically have minimal office space. Light industrial uses include processing, assembling, packaging or treatment of finished products from previously prepared materials or components.

The following analysis presents a worst case scenario since the entire 30.4 net acre area likely won't be developed in its entirety due to limited ability to provide sanitary sewer to the entire site.

The following tables show estimated traffic generation for the three ITE classifications during the morning and afternoon peak. The ITE land use classifications for the traffic analysis are consistent with selected uses identified in the Washington County Industrial Site Readiness Project. Selected uses for the subject area identified in the Washington County project include food processing, general/advanced manufacturing business park, general advanced manufacturing (single user) or advanced high technology manufacturing campus. Based on the Washington County analysis it is believed the subject property identified for the Campus Employment Plan designation could accommodate up to 480,000 square feet of industrial space.

The tables provide data for trip generation by acre, employee and building area in square feet. The estimated traffic generation ranges from 195 to 484 additional trips during the morning peak and 128 to 518 additional trips during afternoon peak depending on land use and basis of analysis (acre, employee or building area). The Campus Employment land use falls in the middle of the range.

AM PEAK TRIPS PER ACRE

Land Use (ITE)	Acres (Net)	Average Rate Weekday AM Peak	Trips Weekday AM Peak
Option 1: Campus Employment/Industrial Park (130)	30.4	8.29	252
Option 2: General Heavy Industrial (120)	30.4	6.41	195
Option 3: Light Industrial (110)	30.4	7.96	242

AM PEAK TRIPS PER EMPLOYEE¹

Land Use (ITE)	Employees	Average Rate Weekday AM Peak	Trips Weekday AM Peak
Option 1: Campus Employment/Industrial Park (130)	513	0.43	221
Option 2: General Heavy Industrial (120)	513	0.40	205
Option 3: Light Industrial (110)	513	0.42	215

¹ Employment calculated using 16.9 employees per net acre for industrial uses based on the City's Economic Opportunity Analysis.

AM PEAK PER 1,000 SQUARE FEET OF GROSS FLOOR AREA

Land Use (ITE)	Square Feet	Average Rate per 1,000 Square Feet Weekday AM Peak	Trips Weekday AM Peak
Option 1: Campus Employment/Industrial Park (130)	480,000	0.82	393
Option 2: General Heavy Industrial (120)	480,000	0.51	244
Option 3: Light Industrial (110)	480,000	1.01	484

PM PEAK TRIPS PER ACRE

Land Use (ITE)	Acres (Net)	Average Rate Weekday PM Peak	Trips Weekday PM Peak
Option 1: Campus Employment/Industrial Park (130)	30.4	8.67	264
Option 2: General Heavy Industrial (120)	30.4	4.22	128
Option 3: Light Industrial (110)	30.4	8.77	267

PM PEAK TRIPS PER EMPLOYEE

Land Use (ITE)	Employees	Average Rate Weekday PM Peak	Trips Weekday PM Peak
Option 1: Campus Employment/Industrial Park (130)	513	0.45	230
Option 2: General Heavy Industrial (120)	513	0.40	205
Option 3: Light Industrial (110)	513	0.48	246

PM PEAK PER 1,000 SQUARE FEET OF GROSS FLOOR AREA

Land Use (ITE)	Square Feet	Average Rate per 1,000 Square Feet Weekday PM Peak	Trips Weekday PM Peak
Option 1: Campus Employment/Industrial Park (130)	480,000	0.86	412
Option 2: General Heavy Industrial (120)	480,000	0.68	326
Option 3: Light Industrial (110)	480,000	1.08	518

Since Highway 47 is projected to operate at a level of service (LOS) A in 2035, Highway 47 has the capacity to absorb these trips generated by the UGB addition area. Furthermore, the TSP includes two project benefitting the transportation system near the subject property. First, the TSP identifies a project to improve the operation of Highway 47/Elm Street (e.g. traffic signal). This intersection improvement is intended to reduce future delay on Elm Street. Second, the TSP identifies the extension of Taylor Way from its current terminus northeast of the site to Elm Street. This improvement should distribute some trips from Elm Street to Fern Hill Road. This should reduce potential impacts to the Highway 47/Elm Street intersection.

The Oregon Transportation Planning Rule (TPR) requires an analysis to determine if an amendment to the Comprehensive Plan will require improvements to the transportation system to ensure that roads operate at an acceptable level of service. The Forest Grove Transportation System Plan establishes a minimum acceptable level of service of LOS D. The Oregon Department of Transportation minimum acceptable level of service is a volume to capacity ratio of 0.99. This analysis is discussed further in the next section.

The Transportation System Plan includes an analysis of the operation of the Highway 47/Elm Street intersection. The analysis was done for current and expected future conditions. The results of this analysis are attached for reference (Attachment B). The analysis shows that traffic volumes on Elm Street are heaviest in the southbound movement north of Highway 47. Northbound traffic on Elm Street is about half of the southbound traffic. The 2035 mitigated volume on Highway 47 is 734 vehicles per hour for the westbound through movement and 488 vehicles in the eastbound through movement. The northbound movement from the industrial park on Elm Street is about 45 vehicles per hour for all movements (Attachment C). The southbound movement on Elm Street from the Town Center is about 270 vehicles per hour.

IV. REVIEW CRITERIA AND FINDINGS OF FACT

Amendments to the Comprehensive Plan Map are reviewed based on the following considerations:

1. Consistency with applicable Comprehensive Plan policies
2. Consistency with Metro Regional Framework Plan
3. Consistency with Metro Regional Functional Plan and Regional Transportation Functional Plan
4. Consistency with Oregon Statewide Land Use Planning Goals

Each of the considerations identified above are discussed in detail below.

1. Comprehensive Plan Policies

The Comprehensive Plan contains policies governing where certain Comprehensive Plan designations should apply. The land use location factors for the Campus Employment designation include:

- A. Greater than 25 acres:

Finding: The subject property is 38 gross acres which is greater than 25 acres

- B. Direct access to arterial:

Finding: The subject property has access to an arterial street (Highway 47) via Elm Street. This access is direct as it is the shortest route.

C. Buffered from surrounding residential and agricultural uses:

Finding: The site is not adjacent to residential areas.

Finding: the Floodplain provides a natural buffer with agricultural uses.

Finding: Article 2 of the Development Code requires site Development Review. This review applies to all development on vacant sites. The review criteria require that site plan ensures reasonable compatibility with surrounding uses.

In addition to the findings above, the Campus Employment designation implements Comprehensive Plan policies related to ensuring an adequate supply of land for office campus development. The Comprehensive Plan (Policy 8.2) supports designating between 55 and 70 acres of land for office and office campus development outside the Town Center. Applying the Campus Employment designation to the 38 acre UGB expansion area will help achieve the Comprehensive Plan policy.

The proposed Campus Employment designation complies with the Comprehensive Plan policies as described above.

2. Statewide Land Use Planning Goals

Goal 9: Economic Development (To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens).

- A. Campus Employment designation consistent with EOA findings
- B. Provides for a supply of land for a variety of industrial and office activities consistent with the comprehensive plan
- C. Compared with residential or community commercial the Campus Employment Designation is compatible with adjacent industrial area to north and agricultural areas to south.

Finding: Assigning the Campus Employment Comprehensive Plan designation to the subject property will create direct benefits in terms of traded-sector jobs since the designation limits uses to primarily industrial or traded-sector industries including offices associated with traded-sector industries. Anticipated retail will be incidental to industrial or traded sector development.

Goal 12: Transportation (To provide and encourage a safe, convenient and economic transportation system).

Finding: Goal 12 is implemented through Oregon Administrative Rules (OAR) Chapter 660-0012-0060. This chapter is also called the Oregon Transportation Planning Rule (TPR). Under OAR 660-0012-0060 an analysis must be done to demonstrate whether a proposed comprehensive plan amendment may have a significant effect on a transportation facility. Findings related to the TPR analysis are provided below.

Under the TPR a plan amendment significantly affects a transportation facility if it would:

- a. Change the functional classification of an existing or planned transportation facility;
- b. Change the standards implementing a functional classification system;
- c. Result in any of the following effects:
 - a. Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility
 - b. Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or degrade performance of an existing or planned transportation facility that it is otherwise projected to not meet the performance standards identified in the TSP or comprehensive plan.

The preceding considerations are discussed below.

- a. Finding: The proposed Comprehensive Plan amendment to designate the subject property Campus Employment will not change the functional classification of an existing or planned transportation facility. Elm Street is designated a collector and Highway 47 is designated a primary arterial in the Forest Grove Transportation Plan. The Oregon Highway Plan designates Highway 47 as a regional route. These classifications will not change as a result of the proposed amendment.
- b. Finding: The proposed amendment will not change the standards implementing the functional classification system contained in the TSP. The functional classification system provides a street hierarchy is based on access, mobility, multi-modal transportation and facility design. The subject property is located adjacent to a collector street (Elm Street) which feeds directly into a primary arterial roadway (Highway 47). The proposed amendment does not require changing the functional classification of either Elm Street or Highway 47.
- c. Finding: The type of traffic (auto and freight) and level of traffic described in this report is not inconsistent with the functional classification of existing facilities. This includes Elm Street (collector) and Highway 47 (primary arterial).

The proposed Comprehensive Plan amendment could degrade the performance of the Highway 47/Elm Street intersection such that it would not meet performance standards identified in the TSP and Comprehensive Plan. Both the TSP and Comprehensive Plan establish a minimum acceptable level of service as LOS D. On ODOT owned facilities the minimum level of services is based on a volume to capacity (v/c) ratio. The minimum acceptable v/c ratio is 0.99. Currently, the TSP shows the Highway 47/Elm Street intersection functions with LOS A/D and v/c ratio of 0.45. Currently, the intersection meets or exceeds acceptable standards. As noted above, ADT at the Highway 47/Elm Street intersection has fallen from 14,205 vehicles to 11,400 vehicles. This suggests

the intersection has capacity to absorb additional traffic and maintain current level of service. The 2035 preferred alternative mitigated intersection analysis for the TSP update shows an ideal flow of 1,800 vehicles per hour per lane during the afternoon peak. The 2035 mitigated volume on Highway 47 is 734 vehicles per hour for the westbound through movement and 488 vehicles in the eastbound through movement. The northbound movement from the industrial park on Elm Street is about 45 vehicles per hour for all movements (Attachment C). The southbound movement on Elm Street from the Town Center is about 270 vehicles per hour. This indicates that traffic generation from the industrial park is not a problem. Rather, traffic on Elm Street from the Town Center is more of a concern.

The Forest Grove TSP projects future intersection operations for the Year 2035 based on added local street connectivity. The Highway 47/Elm Street intersection is projected to operate at a level of service of A/F with a v/c ratio of 1.20. The level of service on the ODOT-owned facility remains at a level of service of A. The Elm Street traffic, however, will likely encounter average delay of 172 seconds if current intersection control remains with stops signs on Elm Street.

The Highway 47/Elm Street intersection meets Manual of Uniform Traffic Control Device (MUTCD) preliminary traffic signal warrants (Attachment C). As such, the Highway 47/Elm Street intersection is a possible candidate for full traffic signal control. Such improvement could improve intersection performance to acceptable standards. It should be noted, however, that meeting traffic signal warrants does not guarantee that a signal will be installed but provides data that could be used with engineering judgment. While the Highway 47/Elm Street intersection meets preliminary traffic signal warrants the City recognizes that approval of the proposed Comprehensive Plan map amendment may result in additional motor vehicle traffic congestion and that other facility providers (ODOT) is not expected to provide additional capacity for motor vehicles in response to this congestion.

In addition to intersection optimization to improve traffic transportation demand management (TDM) programs could help alleviate traffic. TDM Programs include increasing transit service. A more complete bicycle network could also help reduce vehicle demand.

Under the TPR, if a local government determines there would be a significant effect from the proposed amendment, then the local government must ensure that the land use allowed by the amendment are consistent with the identified function, capacity and performance standards of the facility measured at the end of the planning period. This can be approached in a number of ways including amendment the TSP to include transportation improvements adequate to support the proposed land uses. The TSP includes a project to improve the operation of the Highway 47/Elm Street intersection. As required by the TPR (OAR 660-012-0060-4(b)) the project is included on the financially constrained project list meaning funding is expected to be available during

the planning period. The estimated project cost is \$520,000 with anticipated completion within the next six to ten years. Actual timing will be largely dependent on development. An amendment to the TSP is not necessary to support the proposed amendment.

The TPR provides the option to mitigate potential traffic impacts including requiring a condition of development approval or through a development agreement or similar technique. The Forest Grove Development Code allows for the use of development agreements to implement goals, policies or programs of the Comprehensive Plan (including the Transportation System Plan) or for the development of land.

The Development Code (Section 10.1.225) describes land use application requirements. As part of the land use application process, the Community Development Director may require a transportation study when a proposed project would have potential traffic circulation or safety impacts, need for off-site improvements or would increase traffic on City streets by at least 50 peak hourly trips, or a transportation study is required by the Oregon Department of Transportation. This provision ensures that potential traffic impacts are mitigated through the development approval process.

Goal 14: Urbanization (To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities).

Finding: The subject property was added to the urban growth boundary in 2014. Currently, the subject property is zoned Future Development by Washington County. As noted earlier this is a County holding zone applied to urbanizable land within the urban growth boundary. Applying a City Comprehensive Plan Designation to the subject property promotes the efficient transition from rural to urban land by removing one obstacle to development.

3. Metro Regional Framework Plan

The following section demonstrates how the proposed Comprehensive Plan Map amendment complies with the Metro Regional Framework Plan. The Metro Regional Framework Plan establishes the regional vision for growth. The Framework Plan address land use, transportation, parks and open space, water, and geological hazards. Land use and transportation are the elements pertinent to the proposed Comprehensive Plan amendment as described below.

The proposed Comprehensive Plan amendment is consistent with the following policies contained in the Framework Plan:

Policy 1.4.1 Economic Opportunity: Locate expansions of the UGB for industrial or commercial purposes in locations consistent with this plan and where, consistent with state statutes and statewide goals, an assessment of the type, mix and wages of existing and anticipated jobs within subregions justifies such expansion.

Finding: The UGB expansion is adjacent to the Taylor Way Industrial Park. The area is suitable for the types of industrial activities promoted by the Campus Employment designation. The designation is consistent with state statutes and statewide goals including statutes and goals related to land use and transportation. The designation is consistent with statewide goals related to economic development as described by Land Use Planning Goal 9 (Economic Development). Goal 9 requires that Comprehensive Plans and policies contribute to a stable and healthy economy in all regions of the state. Designating the subject for Campus Employment development is consistent with Goal 9 since the designation will expand local employment opportunities contributing to a stable economy.

Policy 1.4.2 Economic Opportunity: Balance the number and wage level of jobs within each subregion with housing cost and availability with that subregion.

Finding: The proposed Comprehensive Plan amendment increases the opportunity for jobs in the western-Washington County. Designating the subject property Campus Employment will help balance the number and wage level of jobs within the western-Washington County. The Campus Employment designation provides opportunity for a variety of activities including in the high-technology sector. As stated in the EOA, the high-technology sector enjoys a relatively high average wage per worker of \$75,838. This type of employment will balance lower wage levels of retail and service jobs in Forest Grove.

Policy 1.5.3 Economic Opportunity: Ensure that all neighborhoods and all people have access to opportunity and share the benefits, as well as burdens, of economic and population growth in the region.

Finding: Designating the site Campus Employment provides opportunity for local residents to share in economic benefits through increased employment opportunities.

Policy 1.7.1 Urban/Rural Transition: Ensure that there is a clear transition between urban and rural land that makes best use of natural and built landscape features and that recognizes the likely long-term prospects for regional urban growth.

Finding: The southern boundary of the property subject to the Comprehensive Plan amendment is coterminous with the urban growth boundary. As such the property is at the urban/rural interface. The Campus Employment designation promotes a transition between agricultural uses to south and more intensive industrial activities to the north. As stated in the Comprehensive Plan, the Campus Employment designation is intended to promote industrial and office parks with high amenity value including landscaping and open space. The emphasis on landscaping and open space provides the opportunity for a clear transition between urban and rural land by taking advantage of the natural and built landscape features in site design.

Policy 1.9.3 Urban Growth Boundary: Use the regional UGB, a long-term planning tool, to separate urbanizable from rural land, based in aggregate on the region's 20-year projected need for urban land.

Finding: The southern boundary of the property affected by the proposed Comprehensive Plan map amendment is coterminous with the urban growth boundary. The area south of the urban growth boundary is designated rural reserve by Washington County. As such, the urban growth boundary establishes the long term boundary between urbanizable and rural land. Under current state law, rural reserves are intended to protect rural land from urban use for a 50-year period of time.

Policy 2.4.1 Consistency Between Land Use and Transportation Planning: Ensure the identified function; capacity and level of service of transportation facilities are consistent with applicable regional land use and transportation policies as well as the adjacent land use patterns.

Finding: Development anticipated within the proposed Campus Employment area is expected to increase traffic volumes above what is anticipated in the Forest Grove Transportation System Plan. This will impact the Highway 47/Elm Street intersection. The Transportation System Plan shows that the volume to capacity ratio will exceed 0.99 by 2035. The TSP includes a project to improve operation of the intersection. The intersection meets preliminary traffic signal warrants as noted in the TSP. Signalization could improve operation of the intersection to acceptable standards. Signalization requires ODOT approval as owner of Highway 47.

The proposed Comprehensive Plan amendment is also consistent with Metro Code Section 3.07.1120 (Planning for Areas Added to the UGB). Under this Metro Code section the city responsible for comprehensive planning shall adopt comprehensive plan provisions and land use regulations for an area added to the urban growth boundary. This includes assigning a specific plan designation to the area.

Finding: The proposed Comprehensive Plan amendment will result in designating the area added to the urban growth boundary Campus Employment. Assigning a comprehensive plan designation to the subject property addresses the requirement of Metro Code Section 3.07.1120 that the city responsible for comprehensive planning adopt a plan designation for new urban growth areas. Adopting the plan designation allows the City to zone and apply land use regulations to the property upon annexation.

The purpose of Metro Regional Framework Plan Land Use Policy 1.5.3 is to ensure that all neighborhoods and all people have access to opportunity and share the benefits, as well as burdens, of economic and population growth in the region. The recommended expands local opportunities for employment. As such, the amendment furthers the intent of Policy 1.5.3 by promoting employment for Forest Grove residents so that residents participate in the benefits of a strong local economy.

The Metro 2040 Growth Concept, implemented through Metro Regional Framework Plan, encourages the mixing of various types of employment. The Campus

Employment Comprehensive Plan designation and corresponding Business Industrial Park zone allows for a variety of employment activities meeting employment needs identified in the City's Economic Opportunities Analysis adopted in 2009. Such uses include industrial services, manufacturing, call centers, research and development, warehousing, wholesale sales, office, and limited retail. These activities meet the intent of the Metro 2040 Growth Concept.

4. Metro Regional Functional Plan and Regional Transportation Functional Plan

Finding: The recommended Comprehensive Plan amendment is consistent with Title 4 of the Metro Regional Functional Plan. Title 4 addresses industrial and other employment areas in the regional. The purpose and intent statement of Title 4 promotes a strong regional economy. To improve the economy, Title 4 seeks to cluster activities in proximity to one another rather than in dispersed locations. The recommended amendment provides an opportunity to zone land near existing industrial areas to provide complementary employment activities. Such activities include industrial services, warehousing, and uses serving employees working in industrial areas.

Regional Transportation Functional Plan

Finding: Title 5 of the Regional Transportation Functional Plan addresses amendments of City and County Comprehensive and Transportation System Plans. Under Title 5, when a city or county proposes to amend its comprehensive plan or its components the city or county shall consider certain strategies as part of the analysis required by the Transportation Planning Rule (OAR 660-012-00060). These strategies include:

- A. Transportation System Management strategies including localized transportation demand management, safety, operational and access management improvements;
- B. Transit, bicycle and pedestrian system improvements;
- C. Traffic-calming designs and designs.
- D. Land use strategies in OAR 660-012-0035(2) to help achieve applicable thresholds and standards.
- E. Connectivity Improvements to provide parallel arterials, collectors or local streets that include pedestrian and bicycle facilities, consistent with the connectivity standards in the RTP in order to provide alternative routes and encourage walking, biking, and access to transit.
- F. Motor vehicle capacity improvements, consistent with the RTP Arterial and Throughway Design and Network Concepts.

Each strategy is discussed below in turn.

- A. Transportation system and demand management strategies are identified in the Transportation System Plan. Appropriate measures to mitigate potential traffic impacts resulting from the proposed Comprehensive Plan amendment will be considered during the development review process. The TSP identifies safety

and operational improvements to the Highway 47 and Elm Street intersection to mitigate potential traffic impacts. Such improvements will be considered during the development review process. Improvements to the intersection will require ODOT approval as owner of Highway 47.

- B. Improvement to Elm Street to accommodate pedestrian and bicycle circulation will be considered during the development review process.
- C. Not applicable.
- D. OAR 660-012-0035(2) establishes a process for evaluation of transportation system alternatives to meet identified transportation system needs. The TSP includes two projects to address transportation needs in the vicinity of the area affected by the proposed Comprehensive Plan Map amendment. These projects are the Highway 47/Elm Street intersection improvement and Taylor Way extension. Since the TSP currently includes projects addressing transportation needs in the area no further evaluation is necessary since no additional projects are proposed.
- E. The TSP identifies an extension to Taylor Way. This local street extension will provide improved connectivity to the site.
- F. This strategy is not applicable to the proposed Comprehensive Plan amendment.

V. RECOMMENDATION

Staff recommends the Planning Commission recommend City Council approval of the proposed Comprehensive Plan Map amendment to designate the 38 acre urban growth boundary addition area Campus Employment.

ATTACHMENTS

- A. Business Industrial Park Zone Text
- B. Highway 47/Elm Street Intersection Analysis
- C. Highway 47/Elm Street Signal Warrant Analysis

ATTACHMENT C

**Planning Commission Findings and Decision Number 2016-04
Legislative Amendment to the Forest Grove Comprehensive Plan Map to Assign Campus
Employment Designation to "Haworth" Property
File Number: 311-15-00032-PLNG**

WHEREAS, on January 24, 2014, the City of Forest Grove approved Ordinance 2014-01 and 2014-02 updating the Forest Grove Comprehensive Plan; and

WHEREAS, the updated Forest Grove Comprehensive Plan includes a new plan map designation known as Campus Employment; and

WHEREAS, the Campus Employment designation is intended for development of industrial and office parks with amenities including landscaping and open space; and

WHEREAS, in 2014, the Oregon Legislature adopted House Bill 4078 which added approximately 38 gross acres of land to the urban growth boundary adjacent to the Forest Grove city limits south of Hwy. 47, near the Elm street terminus and south of the Bonneville Power Administration's easement and north of the revised floodplain boundary; and

WHEREAS, the new urban growth boundary area includes portions of Washington Tax Lots 1S3070000100 and 1S306D000700 owned by Hally L. Haworth; and

WHEREAS, the City desires to designate the new urban growth boundary area Campus Employment on the Forest Grove Comprehensive Plan map; and

WHEREAS, the Planning Commission held a duly noticed public hearing on the proposed amendment on February 15, 2016; and

WHEREAS, notice of the proposed amendments was mailed to property owners and residents within 300 feet of the subject site as required by Development Code Section 10.1.610; and

WHEREAS, notice of this request and public hearing was published in the Forest Grove News-Times on February 10, 2016; and

WHEREAS, a staff report addressing the review criteria and applicant's submittal was published on February 8, 2016; and

WHEREAS, the Planning Commission deliberated on the proposed Comprehensive Plan Map amendment and accepted the staff recommendation.

Now Therefore, The City of Forest Grove Planning Commission does hereby recommend City Council approval of the legislative amendment to the Forest Grove Comprehensive Plan Map to assign the Campus Employment Designation to the "Haworth" Property as described in the staff report and exhibits dated February 15, 2016, making the following specific findings in support of this decision:

1. Comprehensive Plan Policies

The Comprehensive Plan contains policies governing where certain Comprehensive Plan designations should apply. The land use location factors for the Campus Employment designation include:

A. Greater than 25 acres:

Finding: The subject property is 38 gross acres which is greater than 25 acres

B. Direct access to arterial:

Finding: The subject property has access to an arterial street (Highway 47) via Elm Street. This access is direct as it is the shortest route.

C. Buffered from surrounding residential and agricultural uses:

Finding: The site is not adjacent to residential areas.

Finding: the Floodplain provides a natural buffer with agricultural uses.

Finding: Article 2 of the Development Code requires site Development Review. This review applies to all development on vacant sites. The review criteria require that site plan ensures reasonable compatibility with surrounding uses.

In addition to the findings above, the Campus Employment designation implements Comprehensive Plan policies related to ensuring an adequate supply of land for office campus development. The Comprehensive Plan (Policy 8.2) supports designating between 55 and 70 acres of land for office and office campus development outside the Town Center. Applying the Campus Employment designation to the 38 acre UGB expansion area will help achieve the Comprehensive Plan policy.

The proposed Campus Employment designation complies with the Comprehensive Plan policies as described above.

2. Statewide Land Use Planning Goals

Goal 9: Economic Development (To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens).

- A. Campus Employment designation consistent with EOA findings
- B. Provides for a supply of land for a variety of industrial and office activities consistent with the comprehensive plan
- C. Compared with residential or community commercial the Campus Employment Designation is compatible with adjacent industrial area to north and agricultural areas to south.

Finding: Assigning the Campus Employment Comprehensive Plan designation to the subject property will create direct benefits in terms of traded-sector jobs since the designation limits uses to primarily industrial or traded-sector industries including offices associated with traded-sector industries. Anticipated retail will be incidental to industrial or traded sector development.

Goal 12: Transportation (To provide and encourage a safe, convenient and economic transportation system).

Finding: Goal 12 is implemented through Oregon Administrative Rules (OAR) Chapter 660-0012-0060. This chapter is also called the Oregon Transportation Planning Rule (TPR). Under OAR 660-0012-0060 an analysis must be done to demonstrate whether a proposed comprehensive plan amendment may have a significant effect on a transportation facility. Findings related to the TPR analysis are provided below.

Under the TPR a plan amendment significantly affects a transportation facility if it would:

- a. Change the functional classification of an existing or planned transportation facility;
- b. Change the standards implementing a functional classification system;
- c. Result in any of the following effects:
 - a. Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility
 - b. Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or degrade performance of an existing or planned transportation facility that it is otherwise projected to not meet the performance standards identified in the TSP or comprehensive plan.

The preceding considerations are discussed below.

- a. Finding: The proposed Comprehensive Plan amendment to designate the subject property Campus Employment will not change the functional classification of an existing or planned transportation facility. Elm Street is designated a collector and Highway 47 is designated a primary arterial in the Forest Grove Transportation Plan. The Oregon Highway Plan designates Highway 47 as a regional route. These classifications will not change as a result of the proposed amendment.
- b. Finding: The proposed amendment will not change the standards implementing the functional classification system contained in the TSP. The functional classification system provides a street hierarchy is based on access, mobility, multi-modal transportation and facility design. The subject property is located adjacent to a collector street (Elm Street) which feeds directly into a primary arterial roadway (Highway 47). The proposed amendment does not require changing the functional classification of either Elm Street or Highway 47.
- c. Finding: The type of traffic (auto and freight) and level of traffic described in this report is not inconsistent with the functional classification of existing facilities. This includes Elm Street (collector) and Highway 47 (primary arterial).

The proposed Comprehensive Plan amendment could degrade the performance of the Highway 47/Elm Street intersection such that it would not meet performance standards identified in the TSP and Comprehensive Plan. Both the TSP and Comprehensive Plan establish a minimum acceptable level of service as LOS D. On ODOT owned facilities the minimum level of services is based on a volume to capacity (v/c) ratio. The minimum acceptable v/c ratio is 0.99. Currently, the TSP shows the Highway 47/Elm Street intersection functions with LOS A/D and v/c ratio of 0.45. Currently, the intersection meets or exceeds acceptable standards. As noted above, ADT at the Highway 47/Elm Street intersection has fallen from 14,205 vehicles to 11,400 vehicles. This suggests the intersection has capacity to absorb additional traffic and maintain current level of service.

The 2035 preferred alternative mitigated intersection analysis for the TSP update shows an ideal flow of 1,800 vehicles per hour per lane during the afternoon peak. The 2035 mitigated volume on Highway 47 is 734 vehicles per hour for the westbound through movement and 488 vehicles in the eastbound through movement. The northbound movement from the industrial park on Elm Street is about 45 vehicles per hour for all movements (Attachment C). The southbound movement on Elm Street from the Town Center is about 270 vehicles per hour. This indicates that traffic generation from the industrial park is not a problem. Rather, traffic on Elm Street from the Town Center is more of a concern.

The Forest Grove TSP projects future intersection operations for the Year 2035 based on added local street connectivity. The Highway 47/Elm Street intersection is projected to operate at a level of service of A/F with a v/c ratio of 1.20. The level of service on the ODOT-owned facility remains at a level of service of A. The Elm Street traffic, however, will likely encounter average delay of 172 seconds if current intersection control remains with stops signs on Elm Street.

The Highway 47/Elm Street intersection meets Manual of Uniform Traffic Control Device (MUTCD) preliminary traffic signal warrants (Attachment C). As such, the Highway 47/Elm Street intersection is a possible candidate for full traffic signal control. Such improvement could improve intersection performance to acceptable standards. It should be noted, however, that meeting traffic signal warrants does not guarantee that a signal will be installed but provides data that could be used with engineering judgment. While the Highway 47/Elm Street intersection meets preliminary traffic signal warrants the City recognizes that approval of the proposed Comprehensive Plan map amendment may result in additional motor vehicle traffic congestion and that other facility providers (ODOT) is not expected to provide additional capacity for motor vehicles in response to this congestion.

In addition to intersection optimization to improve traffic transportation demand management (TDM) programs could help alleviate traffic. TDM Programs include increasing transit service. A more complete bicycle network could also help reduce vehicle demand.

Under the TPR, if a local government determines there would be a significant effect from the proposed amendment, then the local government must ensure that the land use allowed by the amendment are consistent with the identified function, capacity and performance standards of the facility measured at the end of the planning period. This can be approached in a number of ways including amendment the TSP to include transportation improvements adequate to support the proposed land uses. The TSP includes a project to improve the operation of the Highway 47/Elm Street intersection. As required by the TPR (OAR 660-012-0060-4(b)) the project is included on the financially constrained project list meaning funding is expected to be available during the planning period. The estimated project cost is \$520,000 with anticipated completion within the next six to ten years. Actual timing will be largely dependent on development. An amendment to the TSP is not necessary to support the proposed amendment.

The TPR provides the option to mitigate potential traffic impacts including requiring a condition of development approval or through a development agreement or similar technique. The Forest Grove Development Code allows for the use of development agreements to implement goals, policies or programs of the Comprehensive Plan (including the Transportation System Plan) or for the development of land.

The Development Code (Section 10.1.225) describes land use application requirements. As part of the land use application process, the Community Development Director may require a transportation study when a proposed project would have potential traffic circulation or safety impacts, need for off-site improvements or would increase traffic on City streets by at least 50 peak hourly trips, or a transportation study is required by the Oregon Department of Transportation. This provision ensures that potential traffic impacts are mitigated through the development approval process.

Goal 14: Urbanization (To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities).

Finding: The subject property was added to the urban growth boundary in 2014. Currently, the subject property is zoned Future Development by Washington County. As noted earlier this is a County holding zone applied to urbanizable land within the urban growth boundary. Applying a City Comprehensive Plan Designation to the subject property promotes the efficient transition from rural to urban land by removing one obstacle to development.

3. Metro Regional Framework Plan

The following section demonstrates how the proposed Comprehensive Plan Map amendment complies with the Metro Regional Framework Plan. The Metro Regional Framework Plan establishes the regional vision for growth. The Framework Plan address land use, transportation, parks and open space, water, and geological hazards. Land use and transportation are the elements pertinent to the proposed Comprehensive Plan amendment as described below.

The proposed Comprehensive Plan amendment is consistent with the following policies contained in the Framework Plan:

Policy 1.4.1 Economic Opportunity: Locate expansions of the UGB for industrial or commercial purposes in locations consistent with this plan and where, consistent with state statutes and statewide goals, an assessment of the type, mix and wages of existing and anticipated jobs within subregions justifies such expansion.

Finding: The UGB expansion is adjacent to the Taylor Way Industrial Park. The area is suitable for the types of industrial activities promoted by the Campus Employment designation. The designation is consistent with state statutes and statewide goals including statutes and goals related to land use and transportation. The designation is consistent with statewide goals related to economic development as described by Land Use Planning Goal 9 (Economic Development). Goal 9 requires that Comprehensive Plans and policies contribute to a stable and healthy economy in all regions of the state. Designating the subject for Campus Employment development is consistent with Goal 9 since the designation will expand local employment opportunities contributing to a stable economy.

Policy 1.4.2 Economic Opportunity: Balance the number and wage level of jobs within each subregion with housing cost and availability with that subregion.

Finding: The proposed Comprehensive Plan amendment increases the opportunity for jobs in the western-Washington County. Designating the subject property Campus Employment will help balance the number and wage level of jobs within the western-Washington County. The Campus Employment designation provides opportunity for a variety of activities including in the high-technology sector. As stated in the EOA, the high-technology sector enjoys a relatively

high average wage per worker of \$75,838. This type of employment will balance lower wage levels of retail and service jobs in Forest Grove.

Policy 1.5.3 Economic Opportunity: Ensure that all neighborhoods and all people have access to opportunity and share the benefits, as well as burdens, of economic and population growth in the region.

Finding: Designating the site Campus Employment provides opportunity for local residents to share in economic benefits through increased employment opportunities.

Policy 1.7.1 Urban/Rural Transition: Ensure that there is a clear transition between urban and rural land that makes best use of natural and built landscape features and that recognizes the likely long-term prospects for regional urban growth.

Finding: The southern boundary of the property subject to the Comprehensive Plan amendment is coterminous with the urban growth boundary. As such the property is at the urban/rural interface. The Campus Employment designation promotes a transition between agricultural uses to south and more intensive industrial activities to the north. As stated in the Comprehensive Plan, the Campus Employment designation is intended to promote industrial and office parks with high amenity value including landscaping and open space. The emphasis on landscaping and open space provides the opportunity for a clear transition between urban and rural land by taking advantage of the natural and built landscape features in site design.

Policy 1.9.3 Urban Growth Boundary: Use the regional UGB, a long-term planning tool, to separate urbanizable from rural land, based in aggregate on the region's 20-year projected need for urban land.

Finding: The southern boundary of the property affected by the proposed Comprehensive Plan map amendment is coterminous with the urban growth boundary. The area south of the urban growth boundary is designated rural reserve by Washington County. As such, the urban growth boundary establishes the long term boundary between urbanizable and rural land. Under current state law, rural reserves are intended to protect rural land from urban use for a 50-year period of time.

Policy 2.4.1 Consistency Between Land Use and Transportation Planning: Ensure the identified function; capacity and level of service of transportation facilities are consistent with applicable regional land use and transportation policies as well as the adjacent land use patterns.

Finding: Development anticipated within the proposed Campus Employment area is expected to increase traffic volumes above what is anticipated in the Forest Grove Transportation System Plan. This will impact the Highway 47/Elm Street intersection. The Transportation System Plan shows that the volume to capacity ratio will exceed 0.99 by 2035. The TSP includes a project to improve operation of the intersection. The intersection meets preliminary traffic signal warrants as noted in the TSP. Signalization could improve operation of the intersection to acceptable standards. Signalization requires ODOT approval as owner of Highway 47.

The proposed Comprehensive Plan amendment is also consistent with Metro Code Section 3.07.1120 (Planning for Areas Added to the UGB). Under this Metro Code section the city responsible for comprehensive planning shall adopt comprehensive plan provisions and land use regulations for an area added to the urban growth boundary. This includes assigning a specific plan designation to the area.

Finding: The proposed Comprehensive Plan amendment will result in designating the area added to the urban growth boundary Campus Employment. Assigning a comprehensive plan designation to the subject property addresses the requirement of Metro Code Section 3.07.1120 that the city responsible for comprehensive planning adopt a plan designation for new urban growth areas. Adopting the plan designation allows the City to zone and apply land use regulations to the property upon annexation.

The purpose of Metro Regional Framework Plan Land Use Policy 1.5.3 is to ensure that all neighborhoods and all people have access to opportunity and share the benefits, as well as burdens, of economic and population growth in the region. The recommended expands local opportunities for employment. As such, the amendment furthers the intent of Policy 1.5.3 by promoting employment for Forest Grove residents so that residents participate in the benefits of a strong local economy.

The Metro 2040 Growth Concept, implemented through Metro Regional Framework Plan, encourages the mixing of various types of employment. The Campus Employment Comprehensive Plan designation and corresponding Business Industrial Park zone allows for a variety of employment activities meeting employment needs identified in the City's Economic Opportunities Analysis adopted in 2009. Such uses include industrial services, manufacturing, call centers, research and development, warehousing, wholesale sales, office, and limited retail. These activities meet the intent of the Metro 2040 Growth Concept.

4. Metro Regional Functional Plan and Regional Transportation Functional Plan

Finding: The recommended Comprehensive Plan amendment is consistent with Title 4 of the Metro Regional Functional Plan. Title 4 addresses industrial and other employment areas in the regional. The purpose and intent statement of Title 4 promotes a strong regional economy. To improve the economy, Title 4 seeks to cluster activities in proximity to one another rather than in dispersed locations. The recommended amendment provides an opportunity to zone land near existing industrial areas to provide complementary employment activities. Such activities include industrial services, warehousing, and uses serving employees working in industrial areas.

Regional Transportation Functional Plan

Finding: Title 5 of the Regional Transportation Functional Plan addresses amendments of City and County Comprehensive and Transportation System Plans. Under Title 5, when a city or county proposes to amend its comprehensive plan or its components the city or county shall consider certain strategies as part of the analysis required by the Transportation Planning Rule (OAR 660-012-00060). These strategies include:

- A. Transportation System Management strategies including localized transportation demand management, safety, operational and access management improvements;
- B. Transit, bicycle and pedestrian system improvements;
- C. Traffic-calming designs and designs.
- D. Land use strategies in OAR 660-012-0035(2) to help achieve applicable thresholds and standards.
- E. Connectivity Improvements to provide parallel arterials, collectors or local streets that include pedestrian and bicycle facilities, consistent with the connectivity standards in the RTP in order to provide alternative routes and encourage walking, biking, and access to transit.
- F. Motor vehicle capacity improvements, consistent with the RTP Arterial and Throughway Design and Network Concepts.

Each strategy is discussed below in turn.

- A. Transportation system and demand management strategies are identified in the Transportation System Plan. Appropriate measures to mitigate potential traffic impacts resulting from the proposed Comprehensive Plan amendment will be considered during the development review process. The TSP identifies safety and operational improvements to the Highway 47 and Elm Street intersection to mitigate potential traffic impacts. Such improvements will be considered during the development review process. Improvements to the intersection will require ODOT approval as owner of Highway 47.
- B. Improvement to Elm Street to accommodate pedestrian and bicycle circulation will be considered during the development review process.
- C. Not applicable.
- D. OAR 660-012-0035(2) establishes a process for evaluation of transportation system alternatives to meet identified transportation system needs. The TSP includes two projects to address transportation needs in the vicinity of the area affected by the proposed Comprehensive Plan Map amendment. These projects are the Highway 47/Elm Street intersection improvement and Taylor Way extension. Since the TSP currently includes projects addressing transportation needs in the area no further evaluation is necessary since no additional projects are proposed.
- E. The TSP identifies an extension to Taylor Way. This local street extension will provide improved connectivity to the site.
- F. This strategy is not applicable to the proposed Comprehensive Plan amendment.

ORDINANCE NO. 2016-09**ORDINANCE AMENDING THE FOREST GROVE COMPREHENSIVE PLAN MAP TO DESIGNATE THE SOUTH URBAN GROWTH BOUNDARY ADDITION AS CAMPUS EMPLOYMENT (600 ELM STREET, WASHINGTON COUNTY TAX LOTS 1S30000100 AND 1S306D000700); FILE NO. 311-15-00032-PLNG**

WHEREAS, The City of Forest Grove approved Ordinance 2014-01 and 2014-02 updating the Forest Grove Comprehensive Plan; and

WHEREAS, the updated Forest Grove Comprehensive Plan includes a new plan designation title Campus Employment; and

WHEREAS, the Campus Employment Comprehensive Plan designation is an employment plan designation intended for development of industrial and office parks with a high level of amenity value including landscaping and open space; and

WHEREAS, the Campus Employment designation is intended to allow for industrial and corporate office uses and supporting service activities; and

WHEREAS, in 2014, the Oregon Legislature passed House Bill 4078 modifying the Portland regional urban growth boundary including approximately 38 acres of land south of the Taylor Way Industrial Park near the Elm Street terminus; and

WHEREAS, the City desires to amend the Comprehensive Plan map to designate said 38 acres south of the Taylor Way Industrial Park as Campus Employment; and

WHEREAS, on February 15, 2016, the Planning Commission held a duly-noticed Public Hearing on the proposed Comprehensive Plan map amendment; and

WHEREAS, the Planning Commission adopted Planning Commission Findings and Decision Number 16-04 recommending approval of the proposed Development Code amendments; and

WHEREAS, the City Council held a duly-noticed Public Hearing on the proposed ordinance on March 28, 2016, and continued the hearing on April 11, 2016.

NOW THEREFORE, THE CITY OF FOREST GROVE ORDAINS AS FOLLOWS:

Section 1: The City Council of the City of Forest Grove hereby amends the Forest Grove Comprehensive Plan map as shown on Exhibit A.

Section 2: The City Council hereby adopts the Planning Commission's Findings and Decision 2016-04 as shown on Exhibit B.

Section 3: The City Council hereby finds that the proposed amendments are consistent with and meet the provisions of the Forest Grove Comprehensive Plan Pertaining to Comprehensive Plan Map Amendments as shown on Exhibit B.

Section 4. This ordinance is effective 30 days following its enactment by the City Council.

PRESENTED AND PASSED the first reading this 28th day of March, 2016.

PASSED the second reading this 11th day of April, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 11th day of April, 2016.

Peter B. Truax, Mayor

**Planning Commission Findings and Decision Number 2016-04
Legislative Amendment to the Forest Grove Comprehensive Plan Map to Assign Campus
Employment Designation to “Haworth” Property
File Number: 311-15-00032-PLNG**

WHEREAS, on January 24, 2014, the City of Forest Grove approved Ordinance 2014-01 and 2014-02 updating the Forest Grove Comprehensive Plan; and

WHEREAS, the updated Forest Grove Comprehensive Plan includes a new plan map designation known as Campus Employment; and

WHEREAS, the Campus Employment designation is intended for development of industrial and office parks with amenities including landscaping and open space; and

WHEREAS, in 2014, the Oregon Legislature adopted House Bill 4078 which added approximately 38 gross acres of land to the urban growth boundary adjacent to the Forest Grove city limits south of Hwy. 47, near the Elm street terminus and south of the Borneville Power Administration’s easement and north of the revised floodplain boundary; and

WHEREAS, the new urban growth boundary area includes portions of Washington Tax Lots 1S3070000100 and 1S306D000700 owned by Hally L. Haworth; and

WHEREAS, the City desires to designate the new urban growth boundary are Campus Employment on the Forest Grove Comprehensive Plan map; and

WHEREAS, the Planning Commission held a duly noticed public hearing on the proposed amendment on February 15, 2016; and

WHEREAS, notice of the proposed amendments was mailed to property owners and residents within 300 feet of the subject site as required by Development Code Section 10.1.610; and

WHEREAS, notice of this request and public hearing was published in the Forest Grove News-Times on February 10, 2016; and

WHEREAS, a staff report addressing the review criteria and applicant’s submittal was published on February 8, 2016; and

WHEREAS, the Planning Commission deliberated on the proposed Comprehensive Plan Map amendment and accepted the staff recommendation.

Now Therefore, The City of Forest Grove Planning Commission does hereby recommend City Council approval of the legislative amendment to the Forest Grove Comprehensive Plan Map to assign the Campus Employment Designation to the “Haworth” Property as described in the staff report and exhibits dated February 15, 2016, making the following specific findings in support of this decision:

1. Comprehensive Plan Policies

The Comprehensive Plan contains policies governing where certain Comprehensive Plan designations should apply. The land use location factors for the Campus Employment designation include:

A. Greater than 25 acres:

Finding: The subject property is 38 gross acres which is greater than 25 acres

B. Direct access to arterial:

Finding: The subject property has access to an arterial street (Highway 47) via Elm Street. This access is direct as it is the shortest route.

C. Buffered from surrounding residential and agricultural uses:

Finding: The site is not adjacent to residential areas.

Finding: the Floodplain provides a natural buffer with agricultural uses.

Finding: Article 2 of the Development Code requires site Development Review. This review applies to all development on vacant sites. The review criteria require that site plan ensures reasonable compatibility with surrounding uses.

In addition to the findings above, the Campus Employment designation implements Comprehensive Plan policies related to ensuring an adequate supply of land for office campus development. The Comprehensive Plan (Policy 8.2) supports designating between 55 and 70 acres of land for office and office campus development outside the Town Center. Applying the Campus Employment designation to the 38 acre UGB expansion area will help achieve the Comprehensive Plan policy.

The proposed Campus Employment designation complies with the Comprehensive Plan policies as described above.

2. Statewide Land Use Planning Goals

Goal 9: Economic Development (To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens).

- A. Campus Employment designation consistent with EOA findings
- B. Provides for a supply of land for a variety of industrial and office activities consistent with the comprehensive plan
- C. Compared with residential or community commercial the Campus Employment Designation is compatible with adjacent industrial area to north and agricultural areas to south.

Finding: Assigning the Campus Employment Comprehensive Plan designation to the subject property will create direct benefits in terms of traded-sector jobs since the designation limits uses to primarily industrial or traded-sector industries including offices associated with traded-sector industries. Anticipated retail will be incidental to industrial or traded sector development.

Goal 12: Transportation (To provide and encourage a safe, convenient and economic transportation system).

Finding: Goal 12 is implemented through Oregon Administrative Rules (OAR) Chapter 660-0012-0060. This chapter is also called the Oregon Transportation Planning Rule (TPR). Under OAR 660-0012-0060 an analysis must be done to demonstrate whether a proposed comprehensive plan amendment may have a significant effect on a transportation facility. Findings related to the TPR analysis are provided below.

Under the TPR a plan amendment significantly affects a transportation facility if it would:

- a. Change the functional classification of an existing or planned transportation facility;
- b. Change the standards implementing a functional classification system;
- c. Result in any of the following effects:
 - a. Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility
 - b. Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or degrade performance of an existing or planned transportation facility that it is otherwise projected to not meet the performance standards identified in the TSP or comprehensive plan.

The preceding considerations are discussed below.

- a. Finding: The proposed Comprehensive Plan amendment to designate the subject property Campus Employment will not change the functional classification of an existing or planned transportation facility. Elm Street is designated a collector and Highway 47 is designated a primary arterial in the Forest Grove Transportation Plan. The Oregon Highway Plan designates Highway 47 as a regional route. These classifications will not change as a result of the proposed amendment.
- b. Finding: The proposed amendment will not change the standards implementing the functional classification system contained in the TSP. The functional classification system provides a street hierarchy is based on access, mobility, multi-modal transportation and facility design. The subject property is located adjacent to a collector street (Elm Street) which feeds directly into a primary arterial roadway (Highway 47). The proposed amendment does not require changing the functional classification of either Elm Street or Highway 47.
- c. Finding: The type of traffic (auto and freight) and level of traffic described in this report is not inconsistent with the functional classification of existing facilities. This includes Elm Street (collector) and Highway 47 (primary arterial).

The proposed Comprehensive Plan amendment could degrade the performance of the Highway 47/Elm Street intersection such that it would not meet performance standards identified in the TSP and Comprehensive Plan. Both the TSP and Comprehensive Plan establish a minimum acceptable level of service as LOS D. On ODOT owned facilities the minimum level of services is based on a volume to capacity (v/c) ratio. The minimum acceptable v/c ratio is 0.99. Currently, the TSP shows the Highway 47/Elm Street intersection functions with LOS A/D and v/c ratio of 0.45. Currently, the intersection meets or exceeds acceptable standards. As noted above, ADT at the Highway 47/Elm Street intersection has fallen from 14,205 vehicles to 11,400 vehicles. This suggests the intersection has capacity to absorb additional traffic and maintain current level of service.

The 2035 preferred alternative mitigated intersection analysis for the TSP update shows an ideal flow of 1,800 vehicles per hour per lane during the afternoon peak. The 2035 mitigated volume on Highway 47 is 734 vehicles per hour for the westbound through movement and 488 vehicles in the eastbound through movement. The northbound movement from the industrial park on Elm Street is about 45 vehicles per hour for all movements (Attachment C). The southbound movement on Elm Street from the Town Center is about 270 vehicles per hour. This indicates that traffic generation from the industrial park is not a problem. Rather, traffic on Elm Street from the Town Center is more of a concern.

The Forest Grove TSP projects future intersection operations for the Year 2035 based on added local street connectivity. The Highway 47/Elm Street intersection is projected to operate at a level of service of A/F with a v/c ratio of 1.20. The level of service on the ODOT-owned facility remains at a level of service of A. The Elm Street traffic, however, will likely encounter average delay of 172 seconds if current intersection control remains with stops signs on Elm Street.

The Highway 47/Elm Street intersection meets Manual of Uniform Traffic Control Device (MUTCD) preliminary traffic signal warrants (Attachment C). As such, the Highway 47/Elm Street intersection is a possible candidate for full traffic signal control. Such improvement could improve intersection performance to acceptable standards. It should be noted, however, that meeting traffic signal warrants does not guarantee that a signal will be installed but provides data that could be used with engineering judgment. While the Highway 47/Elm Street intersection meets preliminary traffic signal warrants the City recognizes that approval of the proposed Comprehensive Plan map amendment may result in additional motor vehicle traffic congestion and that other facility providers (ODOT) is not expected to provide additional capacity for motor vehicles in response to this congestion.

In addition to intersection optimization to improve traffic transportation demand management (TDM) programs could help alleviate traffic. TDM Programs include increasing transit service. A more complete bicycle network could also help reduce vehicle demand.

Under the TPR, if a local government determines there would be a significant effect from the proposed amendment, then the local government must ensure that the land use allowed by the amendment are consistent with the identified function, capacity and performance standards of the facility measured at the end of the planning period. This can be approached in a number of ways including amendment the TSP to include transportation improvements adequate to support the proposed land uses. The TSP includes a project to improve the operation of the Highway 47/Elm Street intersection. As required by the TPR (OAR 660-012-0060-4(b)) the project is included on the financially constrained project list meaning funding is expected to be available during the planning period. The estimated project cost is \$520,000 with anticipated completion within the next six to ten years. Actual timing will be largely dependent on development. An amendment to the TSP is not necessary to support the proposed amendment.

The TPR provides the option to mitigate potential traffic impacts including requiring a condition of development approval or through a development agreement or similar technique. The Forest Grove Development Code allows for the use of development agreements to implement goals, policies or programs of the Comprehensive Plan (including the Transportation System Plan) or for the development of land.

The Development Code (Section 10.1.225) describes land use application requirements. As part of the land use application process, the Community Development Director may require a transportation study when a proposed project would have potential traffic circulation or safety impacts, need for off-site improvements or would increase traffic on City streets by at least 50 peak hourly trips, or a transportation study is required by the Oregon Department of Transportation. This provision ensures that potential traffic impacts are mitigated through the development approval process.

Goal 14: Urbanization (To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities).

Finding: The subject property was added to the urban growth boundary in 2014. Currently, the subject property is zoned Future Development by Washington County. As noted earlier this is a County holding zone applied to urbanizable land within the urban growth boundary. Applying a City Comprehensive Plan Designation to the subject property promotes the efficient transition from rural to urban land by removing one obstacle to development.

3. Metro Regional Framework Plan

The following section demonstrates how the proposed Comprehensive Plan Map amendment complies with the Metro Regional Framework Plan. The Metro Regional Framework Plan establishes the regional vision for growth. The Framework Plan address land use, transportation, parks and open space, water, and geological hazards. Land use and transportation are the elements pertinent to the proposed Comprehensive Plan amendment as described below.

The proposed Comprehensive Plan amendment is consistent with the following policies contained in the Framework Plan:

Policy 1.4.1 Economic Opportunity: Locate expansions of the UGB for industrial or commercial purposes in locations consistent with this plan and where, consistent with state statutes and statewide goals, an assessment of the type, mix and wages of existing and anticipated jobs within subregions justifies such expansion.

Finding: The UGB expansion is adjacent to the Taylor Way Industrial Park. The area is suitable for the types of industrial activities promoted by the Campus Employment designation. The designation is consistent with state statutes and statewide goals including statutes and goals related to land use and transportation. The designation is consistent with statewide goals related to economic development as described by Land Use Planning Goal 9 (Economic Development). Goal 9 requires that Comprehensive Plans and policies contribute to a stable and healthy economy in all regions of the state. Designating the subject for Campus Employment development is consistent with Goal 9 since the designation will expand local employment opportunities contributing to a stable economy.

Policy 1.4.2 Economic Opportunity: Balance the number and wage level of jobs within each subregion with housing cost and availability with that subregion.

Finding: The proposed Comprehensive Plan amendment increases the opportunity for jobs in the western-Washington County. Designating the subject property Campus Employment will help balance the number and wage level of jobs within the western-Washington County. The Campus Employment designation provides opportunity for a variety of activities including in the high-technology sector. As stated in the EOA, the high-technology sector enjoys a relatively

high average wage per worker of \$75,838. This type of employment will balance lower wage levels of retail and service jobs in Forest Grove.

Policy 1.5.3 Economic Opportunity: Ensure that all neighborhoods and all people have access to opportunity and share the benefits, as well as burdens, of economic and population growth in the region.

Finding: Designating the site Campus Employment provides opportunity for local residents to share in economic benefits through increased employment opportunities.

Policy 1.7.1 Urban/Rural Transition: Ensure that there is a clear transition between urban and rural land that makes best use of natural and built landscape features and that recognizes the likely long-term prospects for regional urban growth.

Finding: The southern boundary of the property subject to the Comprehensive Plan amendment is coterminous with the urban growth boundary. As such the property is at the urban/rural interface. The Campus Employment designation promotes a transition between agricultural uses to south and more intensive industrial activities to the north. As stated in the Comprehensive Plan, the Campus Employment designation is intended to promote industrial and office parks with high amenity value including landscaping and open space. The emphasis on landscaping and open space provides the opportunity for a clear transition between urban and rural land by taking advantage of the natural and built landscape features in site design.

Policy 1.9.3 Urban Growth Boundary: Use the regional UGB, a long-term planning tool, to separate urbanizable from rural land, based in aggregate on the region's 20-year projected need for urban land.

Finding: The southern boundary of the property affected by the proposed Comprehensive Plan map amendment is coterminous with the urban growth boundary. The area south of the urban growth boundary is designated rural reserve by Washington County. As such, the urban growth boundary establishes the long term boundary between urbanizable and rural land. Under current state law, rural reserves are intended to protect rural land from urban use for a 50-year period of time.

Policy 2.4.1 Consistency Between Land Use and Transportation Planning: Ensure the identified function; capacity and level of service of transportation facilities are consistent with applicable regional land use and transportation policies as well as the adjacent land use patterns.

Finding: Development anticipated within the proposed Campus Employment area is expected to increase traffic volumes above what is anticipated in the Forest Grove Transportation System Plan. This will impact the Highway 47/Elm Street intersection. The Transportation System Plan shows that the volume to capacity ratio will exceed 0.99 by 2035. The TSP includes a project to improve operation of the intersection. The intersection meets preliminary traffic signal warrants as noted in the TSP. Signalization could improve operation of the intersection to acceptable standards. Signalization requires ODOT approval as owner of Highway 47.

The proposed Comprehensive Plan amendment is also consistent with Metro Code Section 3.07.1120 (Planning for Areas Added to the UGB). Under this Metro Code section the city responsible for comprehensive planning shall adopt comprehensive plan provisions and land use regulations for an area added to the urban growth boundary. This includes assigning a specific plan designation to the area.

Finding: The proposed Comprehensive Plan amendment will result in designating the area added to the urban growth boundary Campus Employment. Assigning a comprehensive plan designation to the subject property addresses the requirement of Metro Code Section 3.07.1120 that the city responsible for comprehensive planning adopt a plan designation for new urban growth areas. Adopting the plan designation allows the City to zone and apply land use regulations to the property upon annexation.

The purpose of Metro Regional Framework Plan Land Use Policy 1.5.3 is to ensure that all neighborhoods and all people have access to opportunity and share the benefits, as well as burdens, of economic and population growth in the region. The recommended expands local opportunities for employment. As such, the amendment furthers the intent of Policy 1.5.3 by promoting employment for Forest Grove residents so that residents participate in the benefits of a strong local economy.

The Metro 2040 Growth Concept, implemented through Metro Regional Framework Plan, encourages the mixing of various types of employment. The Campus Employment Comprehensive Plan designation and corresponding Business Industrial Park zone allows for a variety of employment activities meeting employment needs identified in the City's Economic Opportunities Analysis adopted in 2009. Such uses include industrial services, manufacturing, call centers, research and development, warehousing, wholesale sales, office, and limited retail. These activities meet the intent of the Metro 2040 Growth Concept.

4. Metro Regional Functional Plan and Regional Transportation Functional Plan

Finding: The recommended Comprehensive Plan amendment is consistent with Title 4 of the Metro Regional Functional Plan. Title 4 addresses industrial and other employment areas in the regional. The purpose and intent statement of Title 4 promotes a strong regional economy. To improve the economy, Title 4 seeks to cluster activities in proximity to one another rather than in dispersed locations. The recommended amendment provides an opportunity to zone land near existing industrial areas to provide complementary employment activities. Such activities include industrial services, warehousing, and uses serving employees working in industrial areas.

Regional Transportation Functional Plan

Finding: Title 5 of the Regional Transportation Functional Plan addresses amendments of City and County Comprehensive and Transportation System Plans. Under Title 5, when a city or county proposes to amend its comprehensive plan or its components the city or county shall consider certain strategies as part of the analysis required by the Transportation Planning Rule (OAR 660-012-00060). These strategies include:

- A. Transportation System Management strategies including localized transportation demand management, safety, operational and access management improvements;
- B. Transit, bicycle and pedestrian system improvements;
- C. Traffic-calming designs and designs.
- D. Land use strategies in OAR 660-012-0035(2) to help achieve applicable thresholds and standards.
- E. Connectivity Improvements to provide parallel arterials, collectors or local streets that include pedestrian and bicycle facilities, consistent with the connectivity standards in the RTP in order to provide alternative routes and encourage walking, biking, and access to transit.
- F. Motor vehicle capacity improvements, consistent with the RTP Arterial and Throughway Design and Network Concepts.

Each strategy is discussed below in turn.

- A. Transportation system and demand management strategies are identified in the Transportation System Plan. Appropriate measures to mitigate potential traffic impacts resulting from the proposed Comprehensive Plan amendment will be considered during the development review process. The TSP identifies safety and operational improvements to the Highway 47 and Elm Street intersection to mitigate potential traffic impacts. Such improvements will be considered during the development review process. Improvements to the intersection will require ODOT approval as owner of Highway 47.
- B. Improvement to Elm Street to accommodate pedestrian and bicycle circulation will be considered during the development review process.
- C. Not applicable.
- D. OAR 660-012-0035(2) establishes a process for evaluation of transportation system alternatives to meet identified transportation system needs. The TSP includes two projects to address transportation needs in the vicinity of the area affected by the proposed Comprehensive Plan Map amendment. These projects are the Highway 47/Elm Street intersection improvement and Taylor Way extension. Since the TSP currently includes projects addressing transportation needs in the area no further evaluation is necessary since no additional projects are proposed.
- E. The TSP identifies an extension to Taylor Way. This local street extension will provide improved connectivity to the site.
- F. This strategy is not applicable to the proposed Comprehensive Plan amendment.

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<i>CITY RECORDER USE ONLY:</i>	
AGENDA ITEM #:	14, 15 & 16
FINAL ACTION:	_____

CITY COUNCIL STAFF REPORT

WORK SESSION

TO: *City Council*

FROM: *Jesse VanderZanden, City Manager*

MEETING DATE: *March 28, 2016*

PROJECT TEAM: *Jesse VanderZanden, City Manager, and Anna Ruggles, CMC, City Recorder*

SUBJECT TITLE: *Approve Council Goals and Objectives for FY 2016-17 and Council Team Agreement*

ACTION REQUESTED: Ordinance Order Resolution Motion Informational

X all that apply

BACKGROUND:

The purpose of this report is to request that Council approve their Council Key Goals and Council Objectives for FY 2016-17 (Attachment A) and adopt their Council Team Agreement (Attachment B). Council met in Work Sessions on February 20 and March 14 and will continue their discussion at the work session scheduled on Monday, March 28, 2016.

Goals and Objectives for FY 2016-17: Pursuant to City Council Rules of Procedure, Section 16, the City Council shall adopt its Key Council Goals and Council Objectives no later than second regular meeting in March of each year. The Council proposed 12 new Objectives and carried over 13 Objectives from the previous year that were identified as ongoing for a total of 25 Objectives for Fiscal Year 2016-17 (Attachment A). Once Council adopts its Goals and Objectives, the City Manager will assign the Goals and Objectives to departments and departments will incorporate any significant Council Goals and Objectives into their work plans and make presentations to Council at a later date.

Team Agreement: A draft copy of the Council Team Agreement (Attachment B) contains the proposed amendments that Council collectively concurred to include at previous work sessions. Council may:

- Approve as submitted;
- Suggest edits and approve as amended; or
- Not approve and provide further direction.

STAFF RECOMMENDATION:

Staff recommends Council approve the attached resolutions adopting the Council Key Goals, Council Objectives for FY2016-17 (Attachment A), and Council Team Agreement (Attachment B).

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ATTACHMENT A

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RESOLUTION NO. 2016-21**RESOLUTION OF THE CITY OF FOREST GROVE CITY COUNCIL
ADOPTING CITY COUNCIL GOALS FOR FISCAL YEAR 2016-17**

WHEREAS, pursuant to City Council Rules of Procedure, Section 16, the City Council must set its Council Goals annually; and

WHEREAS, the Forest Grove City Council held an Annual Town Meeting on January 30, 2016; and

WHEREAS, the Forest Grove City Council and Management Team met in Goal-Setting Work Session on February 20, 2016, to discuss goals for the next fiscal year; and

WHEREAS, the City Council has determined three goals for Fiscal Year 2016-17.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FOREST GROVE AS FOLLOWS:

Section 1. That the City of Forest Grove City Council hereby adopts the City Council Goals for Fiscal Year 2016-17 as follows (Exhibit A):

- GOAL 1 Promote Safe, Livable, and Sustainable Neighborhoods
and a Prosperous Dynamic, Green City**
- GOAL 2 Promote a Prudent Financial Plan to Maintain Effective
Service Levels of a Full-Service City**
- GOAL 3 Promote the Interests and Needs of Forest Grove
in Local, State, and National Affairs**

Section 2. This resolution is effective immediately upon its enactment by the City Council.

PRESENTED AND PASSED this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor



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CITY COUNCIL GOALS 2016-17

**Promote Safe, Livable and
Sustainable Neighborhoods and
a Prosperous Dynamic, Green City**

**Promote a Prudent Financial Plan to
Maintain Effective Service Levels
of a Full-Service City**

**Promote the Interests and Needs of
Forest Grove in Local, State, and
National Affairs**

RESOLUTION NO. 2016-22

**RESOLUTION OF THE CITY OF FOREST GROVE CITY COUNCIL
ADOPTING CITY COUNCIL GOAL OBJECTIVES FOR FISCAL YEAR 2016-17**

WHEREAS, the City Council adopted three key Council Goals for Fiscal Year 2016-17 pursuant to Resolution No. 2016-21; and

WHEREAS, the City Council and Management Team met in Council Goal Setting Work Sessions on February 20, 2016, to discuss goal objectives for Fiscal Year 2016-17; and

WHEREAS, the City Council has identified the City Council Goal Objectives for Fiscal Year 2016-17 as attached in Exhibit A.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FOREST GROVE AS FOLLOWS:

Section 1. That the City of Forest Grove City Council hereby adopts the City Council Goal Objectives for Fiscal Year 2016-17 as attached in Exhibit A.

Section 2. This resolution is effective immediately upon its enactment by the City Council.

PRESENTED AND PASSED this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

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FOREST GROVE CITY COUNCIL

GOALS AND OBJECTIVES

City Council has proposed a set of goals and priority objectives for Fiscal Year 2016-17. Once Council adopts the Goals and Objectives, the City Manager shall assign to departments and the objectives become part of the departments' work plans. For Fiscal Year 2016-17, the Council is proposing to carry over 13 objectives and identified 12 new proposed objectives (# shown in **Bold**) for a total of 25 objectives.

GOAL 1: PROMOTE SAFE, LIVABLE AND SUSTAINABLE NEIGHBORHOODS AND A PROSPEROUS DYNAMIC, GREEN CITY

		Actions:		Budget:	Time Frame:		Depts:	COUNCIL DIRECTION <i>Proposed</i>	STATUS <i>Proposed Next Steps</i>
		Needs Direction	Council Action	Fiscal Impact	1-3 Years	4+ Years	Assigned		
1.1	Acquisition Process for Land Parcels in Watershed Drainage Areas		◆	\$	■		PW; Admin	Reserve a portion of timber sales for acquisition of land parcels in watershed drainage areas	<i>Develop land acquisition process. Determine percentage of reserves as part of budget process FY2016-17.</i>
1.2	Energy Reduction Programs		◆	\$	■		L&P; PW	1) Residential rebate programs for high efficiency toilets and solar; 2) Continue LED replacement project; and 3) Conduct a wind analysis in watershed	<i>Continue energy conservation efforts and LED replacement project. Conduct analysis's for residential rebate programs for high efficiency toilets and solar. Conduct a wind analysis in watershed.</i>
1.3	Industrial Area Planning		◆	\$	■		CD; ECD; L&E; L&P	Complete feasibility study for industrial area site planning	<i>Continue work to certify readiness of industrial sites and complete area planning</i>
1.4	Multiplex Ballfields			\$	■		P&R; L&E	Determine economic feasibility for multiplex ballfields	<i>Conduct economic feasibility analysis and/or incorporate concept analysis as part of Parks Recreation and Open Space Master Plan</i>

Resolution No. 2016-22, Exhibit A

GOAL 1 (continued)		Actions:		Budget:	Time Frame:		Depts:	COUNCIL DIRECTION <i>Proposed</i>	STATUS <i>Next Steps</i>
		Needs Direction	Council Action	Fiscal Impact	1-3 Years	4+ Years	Assigned		
1.5	Neighborhood Watch Program			\$	■		PD	Draft a map showing established neighborhoods and Annual Report	<i>Prepare a map showing established neighborhoods. Draft 1-2 page annual executive summary report.</i>
1.6	Parks Recreation and Open Space Master Plan		◆	\$	■		P&R	Continue efforts and draft brochure listing parks/open spaces by location	<i>Continue efforts. Prepare brochure listing parks/open spaces by location. Submit final master plan recommendation to Council.</i>
1.7	Planned Residential Developments		◆		■		CD	Draft simple flow chart for PRD process	<i>Draft a simple flow chart showing the steps of the process in sequential order for PRD.</i>
1.8	Plaza Downtown		◆	\$		■	CD; L&E	Identify concept location	<i>Incorporate concept analysis into URA downtown storefront revitalization program.</i>
1.9	Rogers Park Upgrades		◆	\$	■		P&R	Complete renovation efforts in FY16-17	<i>Issue an RFP for renovations including, but not limited to, new restroom facilities and playground equipment.</i>
1.10	Staff Succession Planning				■		Admin; L&E	Draft succession planning guidelines for staff	<i>Draft 1-2 page executive summary report outlining procedural guidelines for staff succession planning efforts.</i>
1.11	Urban Renewal Agency Downtown Storefront Revitalization Program		◆	\$	■		CD; ECD; Eng	Draft downtown storefront revitalization program	<i>Draft downtown storefront revitalization program. Hold URA work session.</i>

GOAL 2: PROMOTE A PRUDENT FINANCIAL PLAN TO MAINTAIN EFFECTIVE SERVICE LEVELS OF A FULL-SERVICE CITY

		Actions:		Budget:	Time Frame:		Dept	COUNCIL DIRECTION <i>Proposed</i>	STATUS <i>Next Steps</i>
		Needs Direction	Council Action	Fiscal Impact	1-3 Years	4+ Years	Assigned		
2.12	Capital Improvement Projects		◆	\$	■		PW; L&E; Admin	Develop a list and financial plan/policy for CIP Projects	<i>Draft 1-2 page executive summary report listing long-term CIP projects. Draft a financial plan/policy.</i>
2.13	City Full-Service Definition		◆		■		L&E; Admin; CD	Annual Town Meeting (ATM) topic and format	<i>Schedule joint work session with Committee for Citizen Involvement regarding ATM format and topic.</i>
2.14	Community Services Local Option Levy Renewal		◆	\$	■		Admin; L&E; FD; PD; Lib; P&R	Preparation for local option levy renewal	<i>Draft 1-2 page executive summary report outlining process and potential election dates for submitting local option levy to voters and schedule Council work session discussion.</i>
2.15	Fire Authority IGA		◆	\$	■		FD; Admin; L&E	Draft Fire Authority IGA. Compare costs to TVF&R.	<i>Draft Fire Authority IGA. Compare costs to TVF&R. Continue work session discussions with Council.</i>
2.16	Forest Grove Senior and Community Center		◆	\$	■		Admin; L&E	Review Senior Center financial request as part of budget process FY2016-17	<i>Review Senior Center financial request as part of budget process FY2016-17.</i>
2.17	Police Department Facility		◆	\$	■		Admin; L&E; PD	Continue request for proposal (RFP) for planning and design	<i>Release RFP. Continue work session discussions with Council.</i>

GOAL 3: PROMOTE THE INTERESTS AND NEEDS OF FOREST GROVE IN LOCAL, STATE, AND NATIONAL AFFAIRS

		Actions:		Budget:	Time Frame:		Dept	<i>COUNCIL DIRECTION Proposed</i>	<i>STATUS Next Steps</i>
		Needs Direction	Council Action	Fiscal Impact	1-3 Years	4+ Years	Assigned		
3.18	Affordable Housing				■		CD	1) Participate in local and regional efforts; 2) draft white paper; and 3) review code for tiny houses	<i>Draft white paper for Council review that outlines city's efforts.</i>
3.19	City Charter Review	◆	◆	\$	■		Admin; L&E	Conduct Charter review	<i>Hold work session for the purpose of seeking direction from Council.</i>
3.20	City Success Stories				■		Admin; L&E	Boast City success stories	<i>Review and revamp City website.</i>
3.21	Council Academy	◆	◆	\$	■		L&E	Discuss viability of a Council Academy	<i>Hold work session for the purpose of seeking content and concept from Council.</i>
3.22	Fernhill Wetlands Research & Education Center		◆	\$	■		Admin; L&E	City anticipated funding participation	<i>Initiate joint meeting with Clean Water Services to discuss feasibility and next steps.</i>
3.23	Legislative Priorities; Local, State, Regional & National			\$	■		L&E	Continue supporting legislative-related efforts	<i>Continue supporting legislative-related efforts.</i>
3.24	Tourism			\$	■		ECD; L&E	Enhance collaboration efforts with FG/Cornelius Chamber	<i>Establish a private-public work group for tourism collaboration efforts with FG/Cornelius Chamber.</i>
3.25	Transportation		◆	\$	■		CD; Eng	1) List of strategically important transportation projects; and 2) Economic feasibility of transferring county roads to city jurisdiction	<i>Draft list of strategically important transportation projects. Conduct economic feasibility analysis for transferring county roads to city jurisdiction.</i>

RESOLUTION NO. 2016-23

**RESOLUTION OF THE CITY OF FOREST GROVE
ADOPTING COUNCIL TEAM AGREEMENT**

WHEREAS, the Forest Grove City Council is the duly elected governing body for the City of Forest Grove; and

WHEREAS, the City Charter, Section 10, specifies that the City Council shall determine its own set of rules to govern its meetings and proceedings beyond those specified in the Charter; and

WHEREAS, the Council Rules of Procedures, Section 5, specifies the rules of decorum and order; and

WHEREAS, the City Council met in Council Work Sessions on February 20, March 14 and March 28, 2016, and collectively reviewed and discussed their Council Team Agreement; and

WHEREAS, the City Council desires to adopt their Council Team Agreement attached as Exhibit A.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FOREST GROVE AS FOLLOWS:

Section 1. That the City of Forest Grove City Council hereby adopts their City Council Team Agreement attached as Exhibit A.

Section 2. This resolution is effective immediately upon its enactment by the City Council.

PRESENTED AND PASSED this 28th day of March, 2016.

Anna D. Ruggles, City Recorder

APPROVED by the Mayor this 28th day of March, 2016.

Peter B. Truax, Mayor

CITY COUNCIL TEAM AGREEMENT

Pursuant to Resolution No. 2016-23, the Agreements for Conducting Council Meetings and Business

1. Attendance at Council meetings is first priority; if unable to attend, please contact the City Recorder.
2. Be on time to meetings and read the packet prior to the meeting - be prepared to work.
3. Mayor will take the lead in keeping the meeting and discussion focused.
4. Distribute information in advance of Council discussion.
5. Mayor will recognize Councilors when indicating they wish to speak.
6. Put a time limit on audience testimony and ask them not to repeat previous speakers.
7. Use formal procedure (point of order, call for question, etc.) to focus the meeting. Formal procedure may be used when necessary for effective discussion in lieu of Council's usual, more informal, process. Individuals should use procedure appropriately and courteously.
8. Council meetings are televised live; this requires Council to act professionally by:
 - Treating the public and each other with courtesy;
 - Speaking in turn and on the issue;
 - No interrupting;
 - No engaging in side conversations; and
 - No use of personal communication electronic devices while conducting business at the dais.
9. Refrain from personal attacks, including to presenters, staff, and Council.
10. Agree to be diplomatic about disagreement; leave disagreement at the dais and do not try to polarize other Councilors.
11. Call the City Manager or designee with questions and requests prior to the meeting.
12. Information available to one council member will be available to all, in a timely manner.
13. Every effort shall be made to adjourn meetings by 9:30 p.m. Council recess shall be called if meeting extends past 9:30 p.m.
14. Councilors may request on their own accord one-on-one meetings with the Mayor.

Individual Council Member Conduct Agreements

Council Members agree to:

- Be straightforward about goals and issues.
- Cultivate exchange of views with other councilors.
- Avoid saying or doing anything that would discredit or harm the City.

Commitments as a Council

Council strives to:

- Continue to improve citizen involvement, awareness and participation.
- Improve follow-up and resolution of citizen concerns or complaints.
- Act as an advocate for the City.

Reviewed and Adopted by City Council on March 28, 2016

Signed:

- Concur
- Non-concur

Peter B. Truax, Mayor Date: _____

- Concur
- Non-concur

Thomas L. Johnston, Council President Date: _____

- Concur
- Non-concur

Richard G. Kidd III, Councilor Date: _____

- Concur
- Non-concur

Victoria J. Lowe, Councilor Date: _____

- Concur
- Non-concur

Ronald C. Thompson, Councilor Date: _____

- Concur
- Non-concur

Elena Uhing, Councilor Date: _____

- Concur
- Non-concur

Malynda H. Wenzl, Councilor Date: _____

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A place where families and businesses thrive

CITY MANAGER'S REPORT TO COUNCIL

March 25, 2016

Dates to Remember:

- March:** CEP Applications Available, Due 5 pm on March 31
- Mar 31:** State of County Address, 7:30 AM, Intel Ronler Acres
- April 2:** Meet the Artist Dinner, 6 pm, Community Auditorium
- April 5:** Friends Cultural Series: Mrs. Pittock Returns, 7 pm, Rogers Room, Library
- April 6:** Art of the Story Festival Program: Patrick Ball, 7 pm, Rogers Room, Library
- April 13:** CEP Committee Meeting, 6 pm, CEP Presentations and Discussions, Community Auditorium
- April 15:** If I Were Mayor Contest Entries Due

CITY MANAGER:

- The terms of the Disposition and Development Agreement (DDA) are close to being finalized per the terms the URA Board approved on February 22. There have been no significant or material changes to the DDA that would require further approval.
- Joint Water Commission (JWC): The back-up power system for the Water Treatment Plant (WTP) has been tested and is fully operational. The WTP retrofit to 75 MGD and 85 MGD is progressing with design expected to reach 30% in August commensurate with hiring a contractor to initiate construction in the Fall/Winter. JWC is meeting April 8 to approve the FY 2016-17 budget and adopt proposed findings supporting the use of CM/GC contracting method.
- The first of four LOC Community Development Policy Committee Meetings is on March 25 in Salem. More to follow.

ADMINISTRATIVE SERVICES:

- Solid Waste: The City was informed that it received the \$27,256 grant from DEQ to purchase and distribute the kitchen pails as part of the food waste into the yard debris program. The kick-off date for the program has been moved to June 1, 2016, so the pails can be purchased and distributed at the start of the program. Staff will bring the IGA for the grant to Council at a meeting in April once the grant paperwork is received. The proposed rate increase to WM rates to implement the food waste program will also be brought to the Council at a meeting in April. WM has informed the City that it will probably be requesting an additional rate increase for operations. Staff has not yet received that rate increase so no details for the reason(s) for that rate increase have been discussed.
- City Website: The City has been reviewing proposals to re-design its website and will be awarding the contract this month. The re-design will start in June or July and is intended to refresh the look of the website and make the website more usable for mobile devices. The project should be completed by September or October.
- Police Facility: The mandatory pre-proposal meeting for the request for proposal (RFP) for architectural and other design services was held on March 17, 2016. One architect firm was at the meeting so that is the only firm that may submit a proposal. This firm has designed the majority of the new police facilities built in Oregon over the last several years. Proposal is due on April 11, 2016, although the City expects to receive the proposal on April 4 or 5. The RFP was advertised in the Daily Journal of Commerce, the RFP was sent to an information clearinghouse for these types of proposals, and the architect firms that submitted quotes for the earlier needs assessment received the RFP directly from City staff.
- Health Insurance Changes as January 1, 2017: CIS has informed all member cities that as of January 1, 2017, CIS will be discontinuing the following health insurance plans: 1) Regence Plan V, this will affect only Fire and IBEW employees; and 2) Kaiser Copay (HMO) which will affect all City employees with

Page 1 of 4

Kaiser coverage. Employees covered by collective bargaining agreements in place as of January 2016, can remain in Plan V or Kaiser Copay plans until the end of the agreement. CIS has alternative plans that staff will be reviewing.

- Salary Survey: HR is collecting information from other agencies for the non-represented employee salary survey which is done every three years.
- Recruitments: Current recruitments underway include: Police Officer, Code Enforcement Officer, Police Reserve Officer, Building Inspector, IT Manager and Human Resources Technician.

PARKS and AQUATICS:

- The Parks Master Plan Citizens Advisory Committee met March 16. Agenda items for the meeting consisted of a review of progress made thus far. However, the majority of the meeting was focused on specific site development of several park facilities. The committee will continue to work on the assignment from home, as several sites were not discussed. The next meeting of the committee will occur in late May or early June to review the results of the surveys that have been conducted. Shortly after this meeting, the committee will begin to receive recommendations for the future of the system.
- Invitations have been sent out to many of the citizens that have worked with Steve Huffman over the 25 years of his service to the community. Steve's career will be celebrated April 1st from 2 to 5 p.m. in the community auditorium.
- Spring vacation public swims were very busy during the past week. Extra staff was added to accommodate the surprise increase in attendance.

LIBRARY:

- The Library Commission held its annual goal-setting retreat at Councilor Wenzl's home. They agreed to seven goals, largely focused on the outcome of the Strategic Planning Process.
- Friends Cultural Series: Tuesday, April 5, 7 pm: "Mrs. Pittock Returns". You will hear how the 1854 wagon train, despite cholera, dysentery and death, brought some of the first roses to the city of Portland.
- Art of the Story featuring Patrick Ball: The Fine Beauty of the Island (Adult program) Wednesday, April 6, 7 PM in the Rogers Room. A musical journey to the legendary Blasket Islands in County Kerry, Ireland, in search of the origin and meaning of a mystical Irish melody.
- The Friends Spring Book Sale will take place Monday, April 18, through Saturday, April 23, in the Rogers Room of the Forest Grove City Library, 2114 Pacific Avenue, in Forest Grove. Sale Hours are coordinated with Library open hours. On Saturday, April 23 (10:00-3:00), current members may fill a standard grocery bag with books and pay just \$5 for the bag full. This benefit is similar to the bag full sales of the past BUT current Friends membership is required.
- Friends of the Library Annual Meeting, Wednesday, April 27, 3 PM, Rogers Room. Nominations for officers for 2016-17 are: President - Mike Smith; Vice President - Susan Schubothe; Treasurer - Ralph Vasey; Secretary - Katie Allnutt. All Friends members are invited. Light refreshments will be served.
- Dia de Los Ninos/ Dia de Los Libros Children's Day/ Book Day: A celebration of bilingual literacy will be held on Friday, April 29, a no school day in the Forest Grove School District. The free event will be held between 10 am and Noon. More volunteers are needed. Please contact Linda Taylor if interested.

POLICE:

- Reserve Officer Austin Adams has accepted a position as a Police Officer and will begin his full-time employment March 30, 2016.
- The Department currently has 3 police officers attending the DPSST Academy.
- Preparations continue for National Night Out 2016 coordinated by COS Quinsland.

- A much larger prescription medication drop box has been installed so that citizens may easily dispose of their medications without having to wait for staff to assist them.

LIGHT & POWER:

- Outage on March 13: Crews responded to a large power outage related to a squirrel contact at Forest Grove Substation. The squirrel made contact with the 12 KV line buss and the grounded steel structure causing severe damage to the structure and hardware. The protective clearing devices activated turning off all incoming 115 KV power from BPA. The area affected was the eastern, northern and part of the central portion of the City resulting in loss of power to approximately 4500 customers. Crews isolated all damaged portions of the grid and began the restoration process immediately. Customer service was restored circuit by circuit with the final customers being energized 102 minutes after the initial contact. Crews continue the process of replacing/repairing damaged equipment and hardware while the engineering staff works with restoration switching, arranging for testing of one unit, a testing process of the main breaker, analysis of event reports for cause and coordination with BPA engineer for resolution and re-energization approval. One unit was put back into service the day of the outage while the second unit is still out of service. Wildlife guards are being aggressively researched in order to lower the risk of a future fault and increase reliability.
- Current preliminary designs being routed for review include: Forest Grove Storage and Marijuana Grow, Tokola feeder underground and service, ODOT signal project at Hwy 8 and Quince.
- Work has been completed replacing the failing high voltage underground cable at Grove Mobile Row D.

ECONOMIC DEVELOPMENT:

- Sent taxing district notice as part of re-authorization process of the Forest Grove Enterprise Zone, which is set to expire on June 30 of this year.
- Completed pre-contract administrative requirements for \$325,000 Washington County CDBG grant application for Senior Center Kitchen Improvements approved.
- Preparing CEP application for EDC to improve downtown storefront design attractiveness.
- Determining impacts of new marijuana laws in industrial and commercial areas especially TTM facility.
- Progress on industrial expansion project for food processor on 24th Avenue: held meeting to clarify utility requirements, making progress on remaining sewer issue.
- Met with Chaucer to discuss future expansion possibilities.
- Updating and promoting workforce development resources available to local employers; developing one day Small Business Resources Workshop in Forest Grove for late spring.
- Completing next steps to trademark three leaves logo and help integrate into city departments.

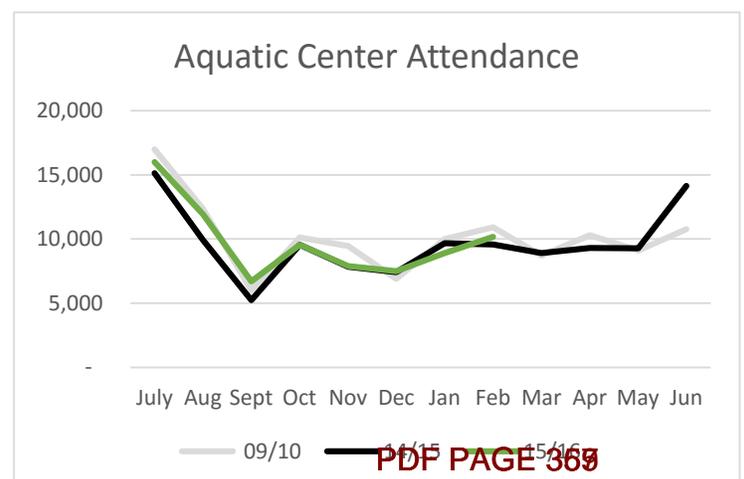
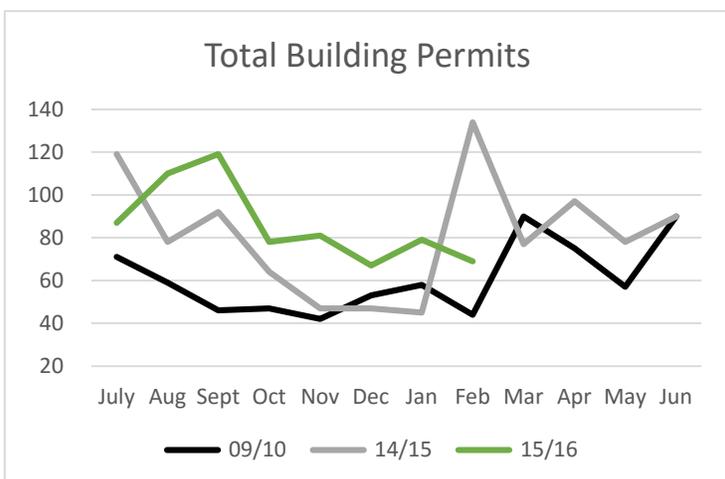
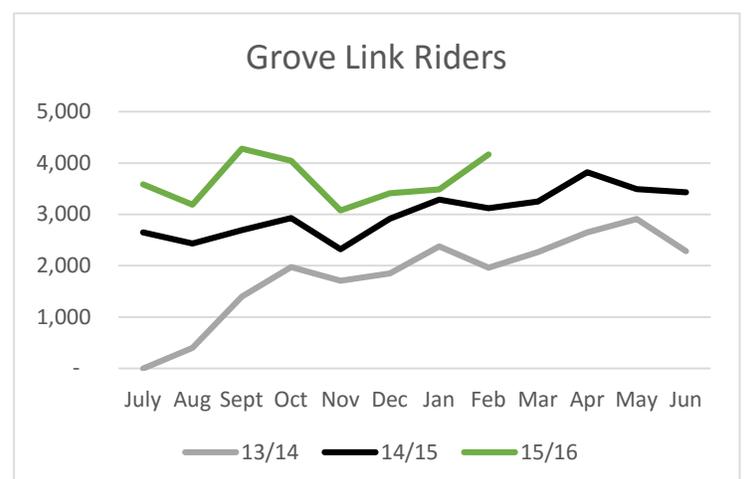
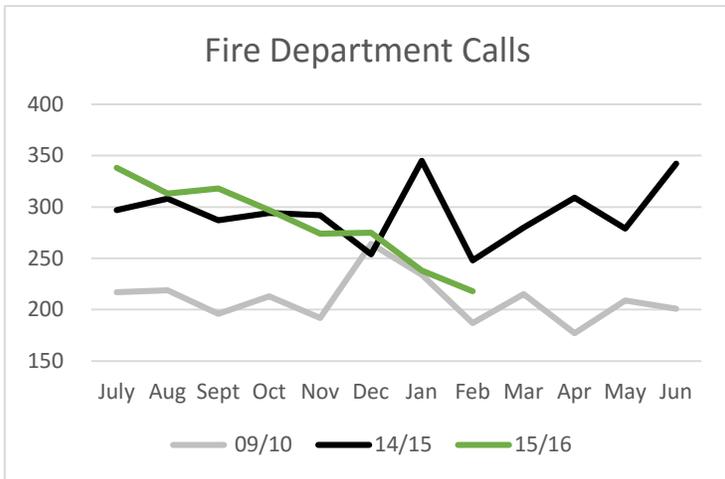
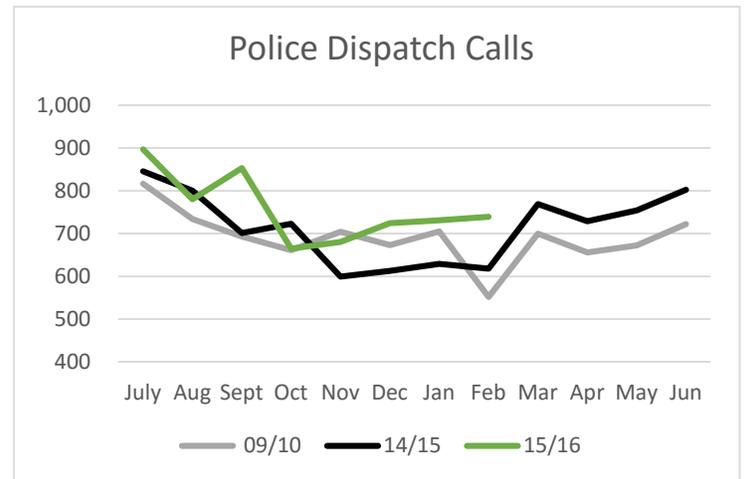
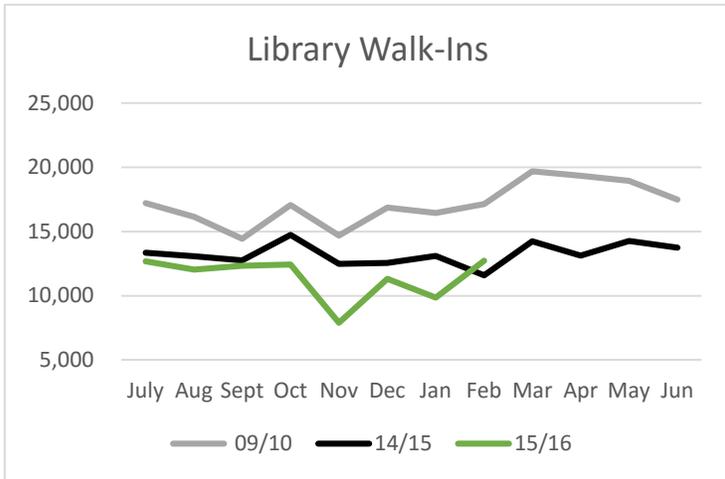
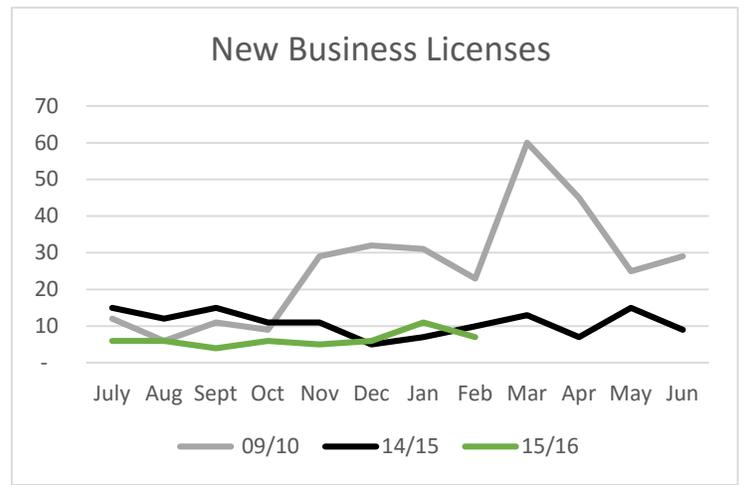
COMMUNITY DEVELOPMENT:

- Marijuana project – Staff has received an inquiry to place a marijuana grow operation in the Matsushita industrial building on Heather Street. The facility is located across the street from Fern Hill Elementary and in close proximity to Echo Shaw Elementary and Neil Armstrong Middle schools. As required by staff, the developer conducted a neighborhood meeting at Echo Shaw Elementary on Wednesday, March 16. It was reported that few parents, if any, attended the presentation. This may be partially due to parent-teacher conferences being held that night. To date, staff has not received any comments from parents about the meeting or the proposal. The developer worked with the Forest Grove School District staff on setting up the meeting and sending out notices to parents. Given the situation, staff will require a public hearing at the Planning Commission if the application is filed.

ENGINEERING / PUBLIC WORKS:

- Grovelink Bus Shelters: Staff has received authorization to purchase the bus shelters. Staff is preparing work orders and organizing site construction.
- Firwood Lane Sanitary Sewer Extension: Stuntzner Engineering has completed the topo survey. Staff is working on design work.
- FEMA: Staff attended an Open House conducted by FEMA to review the Flood Insurance Rate Map. Washington County hosted the event.
- Timber Harvest: The logging operation is in full swing after being shut down for a few weeks due to a road closure and wet weather. Logging operations are about 85% complete.
- Deep Creek Road Improvement Project: The RFP for road work services will be published on March 28. Bids will be due by April 14. Once the contractor is selected work will commence on or shortly after July 1. The work must be completed between July 1 and September 30 to abide by ODFW In-Stream Work Period.

Activity Report Feb 2016



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