



# Site Plan and Design Review Staff Report and Recommendation

Community Development Department, Planning Division

**REPORT DATE:** February 22, 2019

**HEARING DATE:** March 4, 2019

**LAND USE REQUEST:** Site Plan and Design Review of a proposed 3,773-square-foot credit union building with drive-through and parking

**FILE NUMBER:** 311-18-000039-PLNG

**PROPERTY LOCATION:** 3532 Pacific Avenue

**LEGAL DESCRIPTION:** Washington County Tax Lot 1S305BA00100

**OWNER/APPLICANT(S):** Applicant/Property Owner: Wauna Federal Credit Union  
101 Truehaak Street SE, Clatskanie, Oregon 97016

Applicant's Representative: Doug Circosta (AIA)  
14670 SW Forest Drive, Beaverton, Oregon 97007

**COMPREHENSIVE PLAN  
MAP AND ZONING MAP  
DESIGNATIONS:** Community Commercial (CC)  
Community Commercial (CC)

**APPLICABLE  
STANDARDS  
AND CRITERIA:** City of Forest Grove Development Code  
§10.2.300 et. seq. *Design Review*  
§10.2.400 et. seq. *Site Review*  
§10.3.300 - .340 *Commercial and Mixed Use Zones*  
§10.7.200 et. seq. *Solid Waste and Recycling*  
§10.8.000 et. seq. *General Development Standards*

City of Forest Grove Design Guideline Handbook  
Focus Area II *Commercial Corridor Focus Area*

**REVIEWING STAFF:** James Reitz (AICP) Senior Planner

**RECOMMENDATION:** Staff recommends approval with conditions

## I. LAND USE HISTORY

This application is for a site and design review permit to allow for a new credit union building and drive-through. Development Code (DC) §10.3.320 Table 3-10 *Commercial and Mixed Use Zones – Use Table* lists *Personal Services* as permitted uses in the Community Commercial zoning district. Personal Services are defined as “Establishments which provide consumer services such as banks and credit unions, barber and beauty shops, pet grooming, laundromats and dry cleaners, copy centers, photographic studios, trade/vocational schools, and mortuaries” (DC §10.12.130(D)2)).

Because the proposed building would exceed 3,000 square feet of gross floor area, the Design Guidelines require Planning Commission review pursuant to Type III review procedures. Because this activity requires design review, all associated reviews (in this case, site plan approval) are also subject to Planning Commission review, pursuant to DC §10.1.510.

Public notice for this application was mailed to property owners and residents within 300 feet of the site on February 11, 2019, as required by DC §10.1.160. Notice of this request was also provided to ODOT, the Plans Review Board, and published in the *News Times*. ODOT’s comments are attached as Exhibit B. Comments from the Building Division, Engineering and Fire departments are included below in the Public Services section. No issues or concerns were registered by the Light and Power, Police or Public Works departments. As of the writing of this report, no comments have been received from the public.

## II. PROJECT DESCRIPTION AND ANALYSIS

1. Description of Proposal: The proposal would result in the construction of a new credit union building with 2 drive-up automated teller machines (ATMs) and associated parking and landscaping. The building would be a single-story and have 3,773 square feet of floor area. It would take vehicular access from Poplar Street via an existing access and utility easement over the abutting property. No vehicular access to Pacific Avenue or to Highway 47 is proposed.
2. Existing Comprehensive Plan Designation and Zoning of Site and Area

LOCATION	COMPREHENSIVE PLAN DESIGNATION	ZONE DISTRICT	LAND USE
Site	Community Commercial (CC)	Community Commercial (CC)	Vacant Land
North	Community Commercial (CC)	Community Commercial (CC)	McMenamins Grand Lodge
South	Community Commercial (CC)	Community Commercial (CC)	Vacant Land
East	Community Commercial (CC)	Community Commercial (CC)	Ace Hardware (across Hwy 47)
West	Community Commercial (CC)	Community Commercial (CC)	Vacant Land

3. Site Examination: The project site has an area of 0.64 acres (28,000 square feet more or less). It is located at the southwest corner of Pacific Avenue and Highway 47. It is flat and virtually devoid of landscaping except for a few small trees and shrubs that would be removed, and a single Douglas fir tree near the north property

line that the applicant intends to preserve. The site had been previously developed with a single-family home; it was demolished in 1989.

A gas station was located on the adjacent property at 3510 Pacific Avenue. It was demolished in 2001. It and the adjoining property to the south were land-banked by Albertsons in the 1990s in anticipation of developing a shopping center. That concept never progressed beyond pre-application discussions. Staff knows of no plans for that site.

4. Site Design: The site plan consists of a new building to be located at the northeast corner of the site, parking for 20 cars – including 1 ADA space – and a 2-lane drive-through to access the ATMs. The ATMs would not be housed in the building, but would instead be at a remote location, thus separating drive-through traffic from those wishing to park.

Access to the site would be via a driveway to Poplar Street over an existing easement. There would be no direct vehicular access to Pacific Avenue or to Hwy 47 (see Exhibit 1 Sheet C1.0 *Site Plan*).

A new sidewalk would be constructed along Pacific Avenue to connect with the existing walkway along Hwy 47. Walkways would connect the car park with the building and the building with the Pacific Avenue sidewalk. The balance of the site would be landscaped (see Exhibit 1 Sheets L3.0 – 3.2 *Landscape Plans*). Further discussion follows in the Landscape section below.

5. Building Design Standards for Commercial Development: The standards of DC §10.8.710(B) apply to commercial development in the Community Commercial zoning district, as follows:

- Building Orientation. This section requires that buildings be designed and constructed so that at least one façade is within 60 feet of a public street; and that a minimum of one primary building entrance for each building either faces the street or an internal roadway. The building would abut the Pacific Avenue right-of-way (r.o.w.), and the entrance would face Pacific Avenue at an oblique angle, while also providing access to the car park.
- Massing and Form. This section requires building articulation including changes in depth of façade, maximum building length, varied rooflines, and vertical elements. The building exhibits these characteristics, with changes in façade depth, varied roof heights, and vertical elements. The building length of 93 feet would be well short of the 200-foot maximum.
- Design Elements. This section requires buildings near the street to have an entrance every 100 feet; weather protection if adjacent to the sidewalk; transparent windows; and no synthetic materials on the ground floor. The building would have an entrance facing the street, albeit at an oblique angle. The entrance would be sheltered; and windows would be transparent. Some synthetic material (cultured stone veneer, which is a mixture of cement and other components molded and colored to look like real stone) is proposed, for architectural embellishment.

- Compatibility. This section requires a “step-down” in building height near adjacent residential uses; and a gabled roof. As no residential uses are located nearby, this section is not applicable.
- Safety. This section prohibits security bars and motion activated lighting, none of which is proposed. This section further requires public access to all plazas, etc., but none are proposed in this project. Lastly, this section requires that car parks not be gated or secured; no gates are proposed.

6. Setbacks and Height Requirements:

- The site is located in the Community Commercial (CC) zoning district. No front, side, or rear setbacks apply. The building would be angled relative to the intersection. At its closest point, the building would abut the Pacific Avenue r.o.w., and be about 10 feet off the Hwy 47 r.o.w. The side (west) setback (including the integral trash and recycling enclosure) would be about 90 feet while the rear (south) setback would be about 65 feet. No setbacks are required in the CC zoning district.
- The height limit in the CC zone district is 45 feet. The height of the building would be about 23 feet at the roof peak, well below the maximum allowed.

As there are no setbacks required in the CC zone district, and the building height would be less than the maximum allowed, these provisions have been met.

7. Compatibility: The Community Commercial zoning district allows a range of permitted uses, including day care, restaurants, personal services such as banks and beauty shops, and offices. As a credit union is a permitted use in the CC zoning district, it would be compatible with the purpose statement of the CC zoning district, to wit: a concentration of mixed uses, including retail, service, offices and residential uses along the regional transit corridor.
8. Traffic and Circulation: The site fronts Pacific Avenue, a designated Arterial street. It has two travel lanes in each direction and a central left-turn lane. The site would have vehicular access only to Poplar Street, which is a designated Local street. Both streets are under City jurisdiction.

The site also abuts Hwy 47. It is a limited access highway under ODOT jurisdiction. ODOT does not permit private accesses onto limited access highways.

Because the site abuts Hwy 47 and its intersection with Hwy 8 (a.k.a. Pacific Avenue) notice of this application was provided to ODOT. ODOT staff comments are included in their entirety in Exhibit B – the salient comments are as follows:

- *Based on the geometry at the intersection, a right-turn lane is needed on Pacific Avenue. We recommend that the City condition the applicant to dedicate additional right-of-way as needed for the right-turn lane and that the sidewalk be placed back in its ultimate location. Since the right-of-way varies, the amount of right-of-way needed should be measured from the middle of the median or about 60.5 feet with a 6-foot sidewalk without the planter strip. If the City requires a planter strip the additional feet should be added on (see attached email message for details).*

- *(Because) the existing bike/pedestrian path along the property's OR 47 frontage is in poor condition, we recommend the applicant be conditioned to repave the 10-foot path and construct an ADA ramp at the crosswalk.*

Staff concurs with these conclusions. To address these issues, ODOT staff has recommended these conditions of approval:

- Dedicate right-of-way to the City of Forest Grove as necessary to accommodate a right-turn lane on Pacific Avenue.*
- The applicant shall pave the 10-foot bike/pedestrian path along the property's OR 47 frontage and put in a cross walk ramp at the intersection to be consistent with local, ODOT and ADA standards.*

Staff concurs with these recommendations, and has proposed including them in the **conditions** of approval.

In addition, because the Pacific Avenue frontage is not fully improved to City standards (it lacks curbs, gutters, sidewalks, and landscaping), staff is proposing a **condition** to require these frontage improvements.

A sidewalk is proposed to be installed in compliance with DC §10.8.620(A). No minimum dimension width is stipulated by code. Exhibit 1 Sheet A100 *Site Plan* indicates a 6-foot sidewalk would be constructed along Pacific Avenue to connect with the existing Hwy 47 bike path. FGC §151.002 *Sidewalk Specifications* requires that sidewalks comply with City Engineering standards for grades, locations, widths, materials and construction.

Depending on the ultimate site and right-of-way design, the existing Douglas fir tree may need to be removed for safety purposes, as the root zone on both the north and south sides of it might be compromised by the public improvements and ATM drive aisle respectively.

Vehicular access would be provided by a paved driveway from Poplar Street. The driveway would be located within an existing 24-foot-wide access and utility easement. Staff is recommending a **condition** to require some type of physical barrier on both sides of the driveway to prevent, or at least discourage, vehicular trespass onto the adjoining properties. The barriers could be removed at such time as the adjoining properties develop.

Conclusion: With the conditions noted above, traffic access and circulation requirements would be met.

#### 9. Pedestrian Walkways:

- Concrete pedestrian walkways are proposed to connect the car park to the building and to the Pacific Avenue sidewalk. DC §10.8.115(4) requires a minimum walkway width of four feet. The proposed walkways would be not less than five feet wide; the walkway connection to Pacific Avenue would be 10 feet wide.
- While the walkway widths exceed the minimum required, staff has included a **condition** to require a minimum 6 ½-foot-width for walkways adjacent to the parking stalls. The additional width would allow for vehicle overhang, and would eliminate the need for wheel stops, which can be a pedestrian tripping hazard

and a barrier to mechanized parking area cleaning equipment. As the proposed standard parking stalls would be 18 feet long, shortening them by 1-½ feet would not compromise their functionality since the curb/wheel stop would be at the 16-½-foot mark, and still function as a full-sized space.

Conclusion: With the condition noted above, pedestrian access requirements would be met.

10. Parking: DC §10.8.515 Table 8-5 *Parking Requirements* regulates the minimum number of parking stalls required and the maximum number of parking stalls allowed.

Use	Proposed Floor Area	Minimum Spaces per 1,000 S.F.	Maximum Spaces per 1,000 S.F.	Spaces Required
Bank with Drive-In	3,773 Square Feet	4.30	5.40	16 Minimum 20 Maximum

Between 16 and 20 spaces would be required overall. As designed, 20 spaces would be constructed. All would be standard-sized (including 1 ADA-accessible space). Because both the minimum and maximum number of spaces would be provided, no conditions of approval are necessary.

11. Environmental Quality: There is no City record of any prominent environmental conditions. Runoff from new impervious surfaces would be routed to a new on-site water quality facility. Additional landscaping is proposed to be installed (see below) that may improve air quality and reduce glare and heat.
12. Landscaping: DC §10.8.545 requires landscaping and screening of car parks. Proposed landscaping would include a variety of both deciduous and conifer trees scattered throughout the site. The plans also include various deciduous and evergreen shrubs, groundcover and ornamental grasses. The landscape plan appears to comply with the provisions of this code section.
13. Public Services: The application has been reviewed by the Engineering, Fire, Light and Power, Police, and Public Works departments. Specific comments included:
- a. Building Division Review: Submit a geo-tech (soils) report for the site and incorporate those recommendations into the building plans.
  - b. Engineering Department Review: Public infrastructure shall comply with City-standard design and construction standards including CWS requirements.

The above item is included as a **condition** of approval.

- c. Fire Department Review:
  - o Upgrade the fire hydrant at the intersection of Pacific Avenue and Poplar Street to current City standards.
  - o The building address shall be identified with digits at least 6 inches tall that contrasts with their background.

The above items are included as **conditions** of approval.

14. Site Development Approval Criteria: DC §10.2.450 *Site Development Review Criteria* are as follows:

The Planning Commission shall review and approve, conditionally approve, or deny the site development plan based on the following criteria:

- A. The site development plan complies with all applicable standards of the base zoning district, any overlay district, and the applicable general development standards of Article 8.

Finding: The site is located in the Community Commercial (CC) zoning district. No front, side, or rear setbacks apply. At its closest point, the building would abut the Pacific Avenue r.o.w., and be about 10 feet off the Hwy 47 r.o.w. The side (west) setback would be about 90 feet while the rear (south) setback would be about 65 feet. The project complies with setback standards.

Finding: The trash and recycling enclosure would be integral to the building. It would be screened with a 6-foot 4-inch tall wall finished with cultured stone veneer. As there would be no 2-story or taller buildings nearby, the enclosure would not need to be roofed.

Finding: The height limit in the CC zone district is 45 feet. The height of the building would be about 23 feet at the roof peak, well below the maximum height allowed.

Finding: While walkway widths would meet or exceed the minimum required, a minimum 6 ½-foot-width for walkways adjacent to the parking stalls would allow for vehicle overhang, and would eliminate the need for wheel stops, which can be a pedestrian tripping hazard and are a barrier to mechanized parking area cleaning equipment. As the proposed standard parking stalls would be 18 feet long, shortening them by 1-½ feet would not compromise their functionality since the curb/wheel stop would be at the 16-½-foot mark, and still function as a full-sized space.

Finding: With 3,773 square feet of floor area, between 16 and 20 parking spaces would be required. The proposed site plan shows 20 spaces; therefore, it complies with the number of spaces required.

Finding: DC §10.8.545 requires landscaping and screening of car parks. Proposed landscaping would include a variety of both deciduous and conifer trees scattered throughout the site. The plans also include various deciduous and evergreen shrubs, groundcover and ornamental grasses. The landscape plan complies with the provisions of this code section.

Conclusion: With the conditions noted above, the project would comply with Development Code standards.

- B. The site development plan ensures reasonable compatibility with surrounding uses as it relates to the following factors:
1. Building mass and scale do not result in substantial visual and privacy impacts to nearby residential properties; and

Finding: There are no residential properties nearby; this criterion does not apply.

2. Proposed structures, parking lots, outdoor use areas or other site improvements that could cause substantial off-site impacts such as noise, glare and odors are oriented away from nearby residential uses and/or adequately mitigated through other design techniques.

Finding: There are no residential properties nearby; this criterion does not apply.

- C. The site development plan preserves or adequately mitigates impacts to unique or distinctive natural features including, but not limited to:

1. Significant on-site vegetation and trees;
2. Prominent topographic features; and
3. Sensitive natural resource areas such as wetlands, creek corridors and riparian areas.

Finding: No sensitive natural resource areas, prominent topographic features, or sensitive natural resource areas exist on or adjacent to the site. This criterion does not apply to those features. There is a mature Douglas fir tree on-site near the north property line. While the intent is to preserve this tree, it may need to be removed because the installation of street improvements on the north and the ATM drive aisle on the south may adversely impact the root zone.

- D. The site development plan preserves or adequately mitigates impacts to designated historic resources.

Finding: No designated historic resources are present on or adjacent to the site. This criterion does not apply.

- E. The site development plan provides adequate right-of-way and improvements to abutting streets to meet the street standards of the City. This may include, but not be limited to, improvements to the right-of-way, sidewalks, bikeways, and other facilities needed because of anticipated vehicular and pedestrian traffic generation.

Finding: The Pacific Avenue r.o.w. may need to be widened to provide for a right-turn lane to Hwy 47. In addition, the r.o.w. is not fully improved, lacking curbs, gutters, sidewalks, and landscaping. Approval of the application has been conditioned to require additional r.o.w. if necessary, and full City-standard frontage improvements. With these conditions, the site plan would comply with City street standards.

- F. The site development plan promotes safe, attractive and usable pedestrian facilities that connect building entrances, public sidewalks, bicycle and auto parking spaces, transit facilities, and other parts of a site or abutting properties that may attract pedestrians.

Finding: DC §10.8.115(4) requires a minimum walkway width of four feet. Concrete pedestrian walkways are proposed to connect the building to the car park and public sidewalk. All walkways would be at least five feet wide. This criterion is met.

- G. Design Review Criteria: Projects subject to design review by the Director (Type II) or the Design Review Commission (Type III) shall be evaluated based on the following:
- A. The development standards of the applicable zoning district and any overlay district;
  - B. The general development standards of Article 8.
  - C. Departures from code requirements may be permitted as part of a Track 2 Design Review Process, when the following criteria are met:
    - 1. The design guidelines contained in the applicable section of the "Design Guideline Handbook" are adequately addressed.
    - 2. The applicant demonstrates that the overall development would result in a development that better meets the intent of the design guidelines than a design that simply meets the Code.

The Design Guideline Handbook Section II *Commercial Corridor Design Guidelines* establishes specific design review criteria intended to respond to typical commercial development common to Forest Grove, as follows. With minor exceptions, the project complies with the clear and objective criteria established in the Development Code.

#### **Site – Circulation**

- *Access to parking shall be from a local or collector street, an adjacent development, alley, or existing driveways along arterial roadways.* Complies. Access would be from Poplar Street; it is a designated Local street.
- *The site plan shall provide connection to vehicular and pedestrian system into adjacent developments.* Complies. Vehicular access would be provided via an easement over the adjacent property. Additional vehicular and pedestrian connections can be installed when the adjacent parcels develop.
- *Where buildings are setback from right-of-ways, a continuous and direct sidewalk with a 12-foot minimum width shall be provided between the public sidewalk and the front door to all buildings on site.* Complies. A connection would be provided to the public sidewalk along Pacific Avenue.
- *All walkways shall include canopy trees spaced at maximum of 30 feet on center. Trees shall be placed in ornamental cast iron tree grates or planting beds.* Complies. Canopy trees are proposed throughout the car park.
- *All roadways or drives shall include sidewalks on both sides of travel lanes with the exception of vehicle facilities that provide access to loading and service areas.* Does not comply. The driveway would be located in an easement over an adjacent parcel; the easement is 24 feet wide. While that width is sufficient for a 2-way driveway and 4-foot wide walkway, any walkway installed with this project would likely be demolished when the adjoining parcels develop. In addition, the Poplar Street right-of-way is not fully improved, so the walkway would not connect to an improved pedestrian network.

#### **Site – Parking**

- *Construct off-street surface parking that does not occupy more than 50% of the street frontage. Where a site has frontage along a side street a surface parking lot may occupy more than 50% of the side street frontage.* Complies. The parking area is located completely behind the building.

- *A minimum 5' landscaped strip abutting property lines and at least 8% of the interior parking lot area shall be landscaped.* Complies. The proposed landscape strips would be at least 10 feet wide. Interior car park landscaping will total 1,174 square feet, or about 9.5% of the interior parking lot area.
- *Screen parking with solid perimeter wall, earthen berm or evergreen hedge (or a combination thereof) at a minimum spacing of 3 feet and a maximum of 5 feet in height.* Complies. The parking area would be largely screened because it would be located behind the building. Landscaping would also be located throughout the car park and along the perimeter to provide additional screening.
- *Provide a minimum of 15% of parking lot as water pervious surface – either paving or landscaped retention areas.* Complies. Landscape areas totaling more than 31% of the site (about 8,800 square feet) would provide water pervious surfaces.
- *Development sites shall not consist of paved parking areas of more than 50% of the entire site area.* Complies. Total site area is about 28,000 square feet. Car park pavement equals about 12,550 square feet, or about 45% of the site area.

#### **Site – Building Orientation**

- *Design and construct buildings no greater than sixty (60) feet from a dedicated public right-of-way.* Complies. The building would be located adjacent to the Pacific Avenue right-of-way.
- *Design and construct a minimum of one primary building entrance for each building ground floor use or tenant along a public right-of-way or internal roadway built to City public street engineering standards.* Complies. The building entrance would face both Pacific Avenue and the car park drive aisle.

#### **Site – Pedestrian Environment**

- *The sidewalk system shall connect all customer entrances to the public sidewalk.* Complies. A walkway would extend from the building entrance to the public sidewalk.
- *Minimum sidewalk width of ten (10) feet. Sidewalks shall meet American with Disabilities Act (ADA) requirements and may include landscaped areas, benches and other pedestrian amenities.* While the site plan does not comply with this section, sidewalk width is proposed to be 5 feet, which is consistent with the sidewalks constructed outside the town center.
- *Plazas, squares or courts shall be fronted by commercial building facades with 75% or more transparency/window openings of first 15 feet of building.* Not applicable, as no plazas, etc. are proposed.
- *All development shall provide a sheltered entry area at the entrance to each commercial establishment with a minimum area of fifty (50) square feet and a minimum dimension of six (6) feet.* Complies. The building would have an entrance that exceeds these dimensions.

#### **Site – Screening/Service Areas**

- *All loading areas shall be physically separated from customer pedestrian and residential areas with a solid fence, evergreen hedge, arbor, berm or a combination of these elements.* Not applicable, as there are no loading areas proposed or required.
- *See landscape requirements in §10.8.545 Landscaping and Screening of Parking and Loading Areas.* Complies. Perimeter car park landscaping and interior parking lot landscaping comply with this standard, as indicated on Sheet L3.1 of the submittal.

- *All ground level and roof-top mechanical equipment shall be screened from view with landscaping, fencing and or walls. The height of the screen shall equal or exceed the height of the equipment, and include screening from views from above when visible from adjacent inhabited buildings. No mechanical equipment is described in the application. Screening of such equipment is included in the conditions of approval.*

#### **Site – Landscaping**

- *A landscaped strip at least five (5) feet in width shall be provided between a parking area and any property line facing a street, except in the Community Commercial zone, the minimum width shall be ten (10) feet. The landscaped strip shall be appropriately landscaped with ground cover, planted berm, shrubbery and/or trees. Complies. The landscape strip located along both the Pacific Avenue and Highway 47 frontages would be at least 10 feet wide.*
- *At least 75% of required landscaped area shall be planted with any suitable combination of trees, shrubs, or evergreen ground cover. Complies. All landscaped areas are proposed to be planted with trees, shrubs, ground cover and grasses.*
- *A maximum of 25% of required landscaped area may include architectural features or artificial ground covers such as sculptures, benches, masonry or stone walls, rock groupings, decorative hard paving and gravel areas interspersed with planting areas. Not applicable, as no such materials are proposed.*
- *Buffering and screening is required as specified in §10.8.400 et. seq. Complies. The parking area would be largely screened because it would be located behind the building. Landscaping would also be located throughout the car park and along the perimeter to provide additional screening.*
- *All development projects fronting on a public or private street more than 100 feet in length approved after the adoption of this title shall be required to plant street trees (spacing, and tree type varies according to §10.5.120(A)(1)). No street trees were described in the application. Installation of parkway landscaping is included in the conditions of approval.*

#### **Building – Massing and Form**

- *Provide changes in the depth of the façade plane in excess of two (2) feet for all building walls more than 75 feet in length. Complies. The building articulation includes changes in depth of façade, varied rooflines, and vertical elements.*
- *Maximum length of building facades shall be 200 feet. Complies; building length is approximately 93 feet.*
- *Roofline heights must be varied a minimum of 2 feet for building facades greater than 75 feet in length. Complies. The shed roof plan includes several heights.*
- *Provide vertical partitioning of facades by using columns, pilasters and other vertical elements a minimum of every twenty-five (25) feet. Complies. Vertical elements include columns, window treatments, vertical siding, and changes in material.*
- *Articulation of building fronts through changes in depth or building relief (e.g., windows, doors, cornices, columns, and a change in materials) shall occur a minimum of every fifteen (15) feet in the horizontal or vertical dimensions. Complies. The building has changes in both depth and materials.*

### **Building – Design Elements**

- *Every building elevation adjacent to a street with a horizontal dimension of more than 100 feet shall have a building entrance.* Complies. The building would be 93 feet long and has a building entrance near Pacific Avenue.
- *Building facades adjacent to sidewalks or pedestrian connections must include weather protection canopies or awnings along at least 75% of the length of the ground floor façade.* The building façade would not be adjacent to the sidewalk or pedestrian connection. This criterion does not apply.
- *Incorporate a minimum of 50% transparency or window openings on the ground floor facades of buildings facing the public right-of-way.* Complies. Multiple window openings are proposed on all ground floor facades.
- *Windows shall not be reflective, tinted or treated in such a way as to block views into or out of windows.* Complies; no such window treatments are proposed.
- *Windows must be recessed a minimum of 4 inches from building facades.* Complies, based on the submitted architectural renderings.

The following applies for facades with primary building entries within 60 feet of public streets:

- *Materials such as synthetic stucco (EIFS, Dryvit, etc.) are prohibited at ground floor.* Complies; no such materials are proposed.
- *Jumbo bricks with dimensions larger than 4 x 8 x 2 inches are prohibited at ground floor.* Complies; no such bricks are proposed.
- *Walls constructed of spandrel glazing/curtain wall (i.e. smooth glass concealing structural elements) are prohibited at ground floor.* Complies; no curtain walls are proposed.
- *Vinyl cladding of the building façade is prohibited at ground floor.* Complies; no vinyl cladding is proposed.

### **Building – Compatibility**

- *Any building or portion of a building that is less than 50 feet from existing residential dwellings must not be higher than 15 feet above the top of the roofline of the nearest existing dwelling.* Not applicable, as no residential building is located nearby.
- *When adjacent to existing residential development, all commercial structures with floor areas less than 20,000 square feet of total building area must have sloped roofs.* Not applicable, as no residential building is located nearby.

### **Building – Safety**

- *Security gates or bars on windows or doors are prohibited along ground floors of commercial buildings, excluding loading and storage areas.* Complies, as no security gates, bars or doors are proposed.
- *Motion activated security lighting is prohibited along building facades that front onto public rights-of-way.* Complies, as no motion-activated lighting along the public way is proposed.
- *Public access shall be provided to all commercial uses that provide plazas, squares, or courts intended for food, beverage or entertainment uses.* Not applicable, as no such features are proposed.
- *Parking lots or roadways shall not be gated or secured excluding loading and storage areas or residential parking areas.* Complies, as no gates are proposed.

**Lighting – Exterior Building**

- *Building lighting shall be directed downward with no splay of light off the site.* Complies; the lights are to be shielded.
- *Any light source over ten (10) feet high shall incorporate cut-off shield.* Complies, as the lights are to be shielded.

**Lighting – Parking Lot**

- *Lights shall not obstruct vehicular or pedestrian traffic.* Complies; the lights will not be installed in travel lanes or within walkways.
- *Poles outside of the public right-of-way shall be a maximum of 14 feet in height.* Light poles shall be required to comply with DC §10.8.755(E)(2)(b).
- *Concrete light fixture bases taller than 8" are prohibited.* Fixture bases shall be required to comply with DC §10.8.755(E)(2)(b).
- *Lights shall be equipped with cut-off fixture to ensure that light does not spill onto adjacent residential property.* Lights shall be required to comply with DC §10.8.755(E)(2)(b).

**Lighting – Sidewalk and Landscape**

- *All bollard, wall or stairway lighting shall be directed exclusively downward.* Complies, as all lighting shall be directed downward.
- *All uplighting fixtures shall be screened by landscaping or concealed within structures or located below or flush with grade.* None proposed; not applicable.
- *No walkway lighting shall exceed 18 feet in height.* Light poles shall be required to comply with DC §10.8.755(E)(2)(b).

**Lighting – Signs**

- *Interior lighted plastic signs are prohibited.* Noted; this will be addressed when the sign permits are submitted.
- *Moving or flashing signs are prohibited.* Noted; this will be addressed when the sign permits are submitted.

**Pole and Monument Signs**

- *No more than one (1) pole or free-standing sign shall be allowed per individual business or frontage (whichever is less). All pole signs shall have a minimum clearance of ten (10) feet below the sign and shall have a maximum height of twenty (20) feet. No pole sign shall have a total area of more than forty (40) square feet per face.*
- *The maximum height for a free-standing sign is eight feet.*

Noted; these standards will be addressed when sign permits are submitted.

Conclusion: With the conditions described in the section above, the design guidelines have been adequately addressed by the application.

### III. ALTERNATIVES

The Planning Commission may approve the application as submitted, approve it with conditions, continue deliberations to a date certain, or deny this request.

## IV. RECOMMENDATION

Based on the information provided in the application and the findings above, staff recommends approval of the application for site design and design review for the proposed Wauna Federal Credit Union at 3532 Pacific Avenue, with the following conditions:

### GENERAL

1. The applicant is bound to the project description and all representations made by the applicant during the application and decision-making proceeding.
2. The applicant must comply with all applicable City and CWS building and development standards, including all dimensional standards and public works specifications.
3. All utility connections shall be underground (DC §10.8.645(A) *Underground Utilities*).
4. The building address shall be identified with digits at least 6 inches tall that contrast with their background.
5. All ground level and roof-top mechanical equipment shall be screened from view with landscaping, fencing and or walls. The height of the screen shall equal or exceed the height of the equipment, and include screening from views from above when visible from adjacent buildings.

### STREET AND PUBLIC IMPROVEMENTS

6. Dedicate as needed additional right-of-way along Pacific Avenue to provide for a right-turn lane to Highway 47, as per ODOT standards and specifications.
7. Construct a right-turn lane along Pacific Avenue to Highway 47, as per ODOT standards and specifications.
8. Improve the Pacific Avenue street frontage including curbs, gutters, sidewalks, and landscaping.
9. Improve the Highway 47 frontage to ODOT standards and specifications.
10. Public infrastructure shall comply with City-standard design and construction standards including CWS requirements.
11. Upgrade the fire hydrant at the intersection of Pacific Avenue and Poplar Street to current City standards.

### ACCESS, CIRCULATION AND PARKING

12. Except where required by other codes (e.g. ADA), parking stalls shall be reduced in length to 16 ½ feet, provided the adjoining walkways and landscape areas are commensurately enlarged. No additional curb stops shall be required.
13. Install a physical barrier on both sides of the access driveway to prevent, or at least discourage, vehicular trespass onto the adjoining properties.

### LIGHTING

14. All lighting shall comply with the provisions of DC §10.8.755(C) *Pedestrian Lighting Standards* and §10.8.755(E) *Lighting Standards for Commercial Development*.

## V. LIST OF EXHIBITS

The following exhibits were received, marked, and entered into the record as evidence for this application at the time this staff report was written. Exhibits received after the date of this report will be marked beginning with the next consecutive letter and will be entered into the record at the time the public hearing is opened, prior to oral testimony.

- Exhibit A** Site Plan and Design Review application materials, submitted by the applicant
- Exhibit B** ODOT Comments
- Exhibit C** PowerPoint Slides

# EXHIBIT A

Site Review Application Materials  
Prepared and Submitted by the Applicant



**AAI ENGINEERING**  
 502 820 3000 • 502 820 3525 • 1000 NE Oregon Street, Suite 100  
 WAUNA, OREGON 97142

**WAUNA CREDIT UNION**

FOREST GROVE, OR

EXISTING CONDITIONS AND DEMOLITION PLAN  
 DATE: 11/19/18  
 DRAWN: NAD  
 CHECKED: DSE  
 REVISIONS:

**C0.4**

JOB NUMBER: A17081.11

**VERTICAL DATUM:**  
 DATUM: NATIONAL GRID  
 DESCRIPTION: FOUND BRASS DISK IN TOP OF CONCRETE CURB AND GROUND SURFACE, APPROXIMATELY 50 FEET EAST OF THE INTERSECTION OF STATE STREET.  
 ELEVATION: 174.53'

**UTILITY REQUISITES:**  
 ALL UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM LOCATE PART MARKINGS SET IN THE FIELD SURVEY AND SURVEY DATA. THIS SURVEY DOES NOT SHOW ANY PART MARKINGS PROVIDED BY THE UTILITY OWNERS. THE CONTRACTOR SHALL VERIFY THE INFORMATION THAT WAS NOT PROVIDED IS NOT REFLECTED ON THIS SURVEY. LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THIS SURVEY ARE BASED ON THE INFORMATION PROVIDED. LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THIS SURVEY ARE BASED ON THE INFORMATION PROVIDED. LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THIS SURVEY ARE BASED ON THE INFORMATION PROVIDED. LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THIS SURVEY ARE BASED ON THE INFORMATION PROVIDED.

**SHEET NOTES**

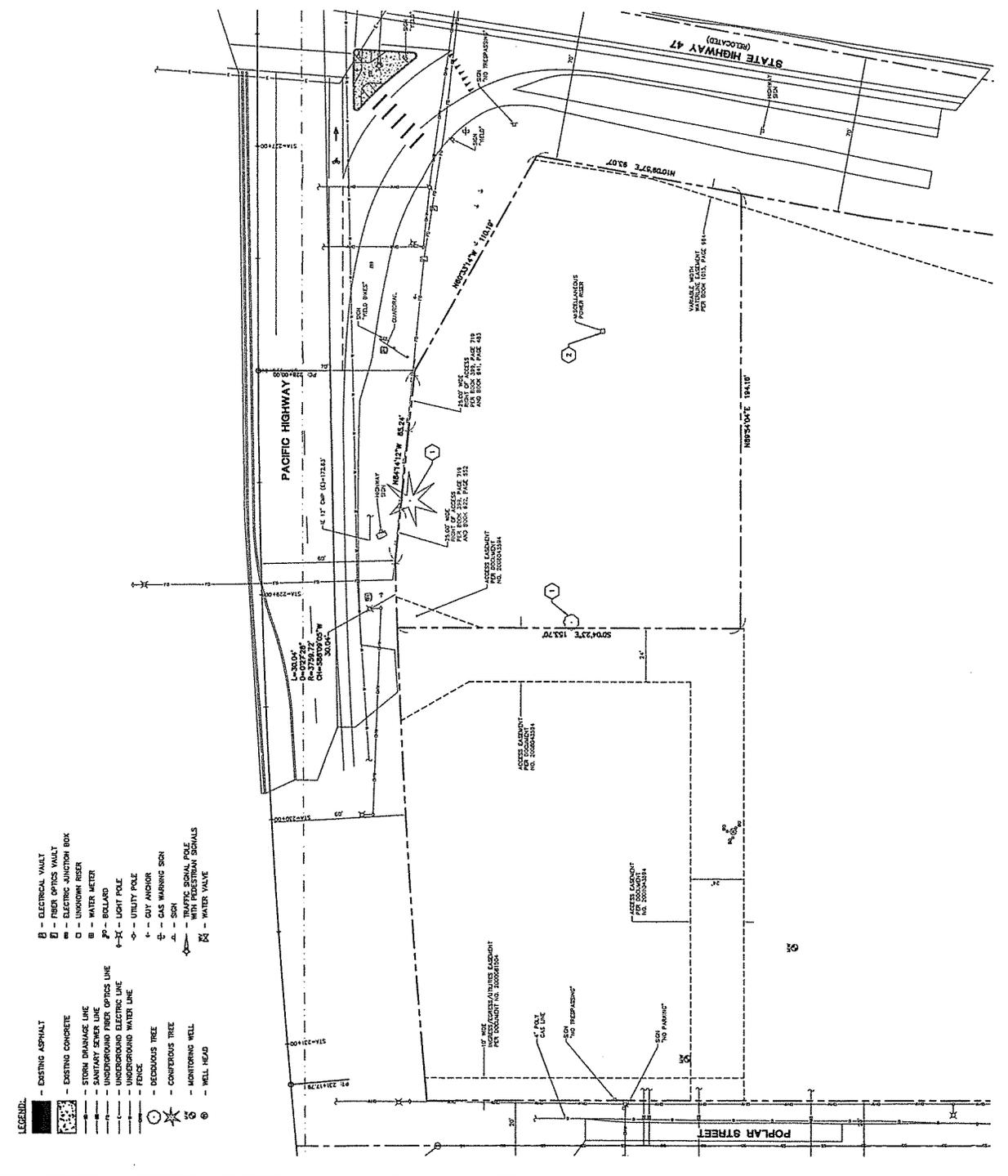
- SEE SHEET C0.3 FOR GENERAL SHEET NOTES.
- REMOVE ALL UTILITIES SHOWN ON THIS SHEET.
- REMOVE ALL SITE COMPONENTS AND RECORDS.
- ALL TRADE LICENSES AND PERMITS NECESSARY FOR THE WORK SHALL BE OBTAINED BY THE CONTRACTOR PRIOR TO COMMENCING WORK.
- PROTECT ALL EXISTING RIGHT-OF-WAY SURVEY MARKS FROM DAMAGE. ALL EXISTING RIGHT-OF-WAY SURVEY MARKS SHALL BE REPAIRED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND MAINTAINING ALL NECESSARY PERMITS AND RECORDS.
- PROTECT ALL UTILITIES SHOWN ON THIS SHEET AND IN THE RIGHT-OF-WAY INCLUDING BUT NOT LIMITED TO: WATER, GAS, ELECTRIC, FIBER OPTIC, TELEPHONE, CABLE, PAVEMENT AND DRIVEWAYS. ALL UTILITIES SHALL BE PROTECTED AND RESTORED TO ORIGINAL CONDITION.
- PROTECT STRUCTURES, UTILITIES, SIDEWALKS, AND OTHER EXISTING CONDITIONS. ALL EXISTING CONDITIONS SHALL BE PROTECTED AND RESTORED TO ORIGINAL CONDITION. ALL EXISTING CONDITIONS SHALL BE PROTECTED AND RESTORED TO ORIGINAL CONDITION.
- PROTECT ALL EXISTING UTILITY STRUCTURES AND TRANSPORTATION OF DEBRIS, ALL STREET UTILITIES SHALL BE PROTECTED AND RESTORED TO ORIGINAL CONDITION. THE CONSTRUCTION ZONE MUST BE KEPT CLEAN.
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**DEMOLITION NOTES**

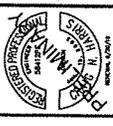
- REMOVE TREE.
- REMOVE OR RELOCATE EXISTING POWER RISER.



11/19/2018 — DESIGN REVIEW



- LEGEND:**
- EXISTING ASPHALT
  - EXISTING CONCRETE
  - STORM DRAINAGE LINE
  - UNDERGROUND FIBER OPTICS LINE
  - UNDERGROUND ELECTRIC LINE
  - UNDERGROUND WATER LINE
  - FENCE
  - BEADWOOD TREE
  - CONFERRED TREE
  - MONITORING WELL
  - WELL HEAD
  - ELECTRICAL VAULT
  - FIBER OPTICS VAULT
  - ELECTRIC JUNCTION BOX
  - UNKNOWN RISER
  - WATER METER
  - BOLLARD
  - LIGHT POLE
  - UTILITY POLE
  - CLAY ANCHOR
  - SON
  - WATER VALVE
  - TRAFIC SIGNAL POLE WITH PEDESTRIAN SIGNALS
  - WATER VALVE



**AAI ENGINEERING**  
 4175 SW Crater Drive, Suite 200 | Beaverton, OR 97005  
 503.638.2004 | 503.638.2029 | www.aaieng.com

**WAUNA CREDIT UNION**  
 FOREST GROVE, OR

**SITE PLAN**  
 DATE: 11/19/18  
 DRAWN: NLD  
 CHECKED: DSE  
 REVISIONS:

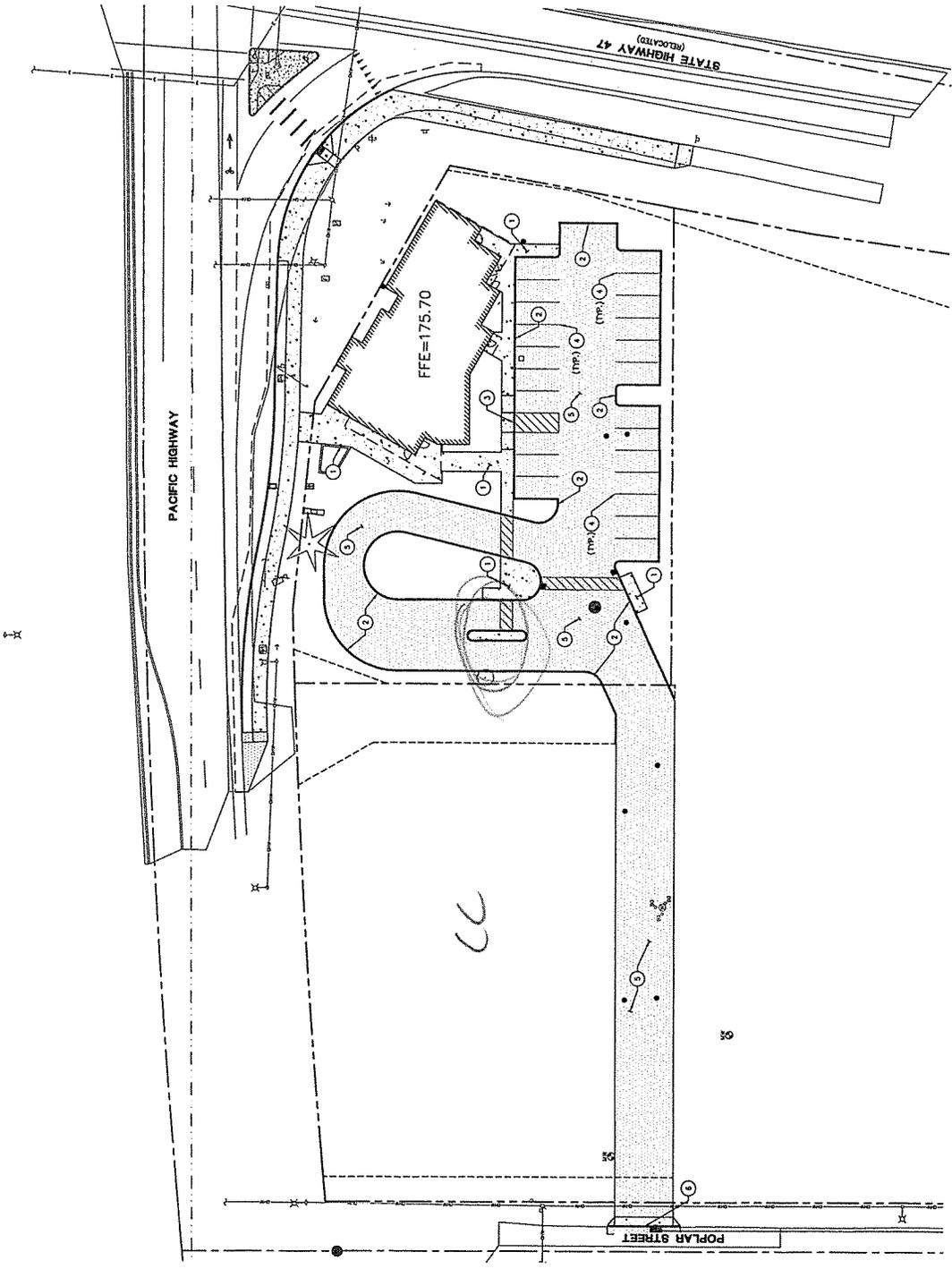
© All rights reserved. No part of this drawing may be reproduced or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the prior written permission of the engineer.

**C1.0**  
 JOB NUMBER: A17061.11

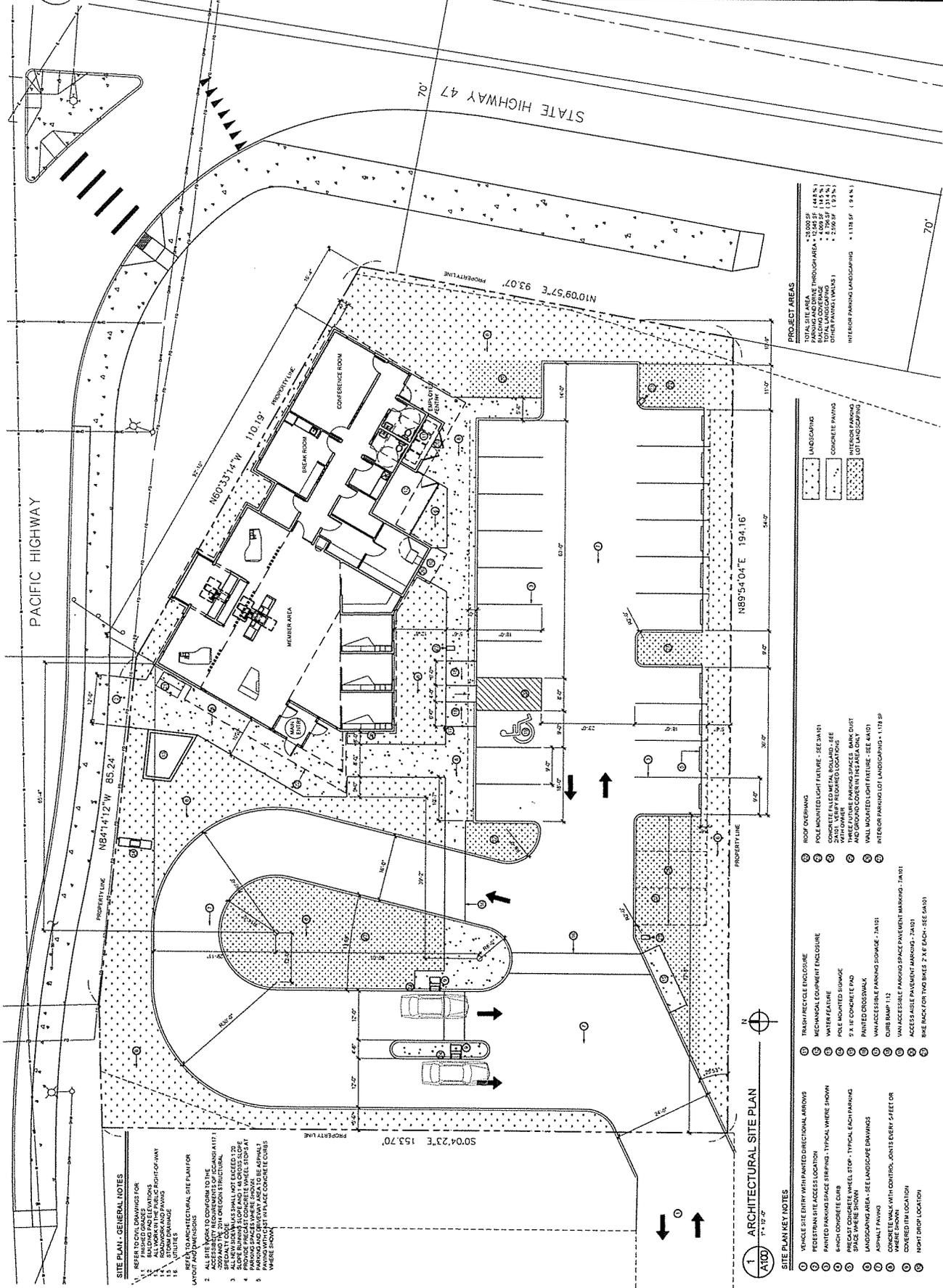
- SHEET NOTES**
- SEE SHEET C0.2 AND C0.3 FOR GENERAL SHEET NOTES.
  - SEE ARCHITECTURAL PLANS FOR ADDITIONAL SITE INFORMATION.
  - THE CONTRACTOR SHALL HAVE A FULL SET OF THE DESIGN APPROVED CONSTRUCTION DOCUMENTS ON SITE AT ALL TIMES.
  - THE CONTRACTOR SHALL KEEP THE ENGINEER AND ARCHITECT INFORMED OF CONSTRUCTION PROGRESS TO ALLOW FOR ANY NECESSARY ADJUSTMENTS AT 24-HOUR INTERVALS.

- CONSTRUCTION NOTES**
- INSTALL PRIVATE SIDEWALK
  - INSTALL PRIVATE CURB
  - INSTALL ADA RAMP
  - INSTALL WHITE STRIPE SEE ARCHITECTURAL PLANS FOR DIMENSIONS
  - INSTALL ASPHALT
  - INSTALL DRIVEWAY

- LEGENDS**
- PROPERTY LINE
  - PROPERTY SIDEWALK
  - PROPERTY DRIVEWAY
  - ASPHALT SURFACING
  - PROPOSED BUILDING



11/19/2018 -- DESIGN REVIEW



**SITE PLAN - GENERAL NOTES**

1. REFER TO CIVIL DRAWINGS FOR BUILDING AND ELEVATIONS.
2. ALL SITE WORK TO CONFORM TO THE 2019 OREGON CONSTRUCTION CODE AND THE 2011 OREGON STRUCTURAL CODE.
3. ALL GRASSY AREAS SHALL NOT EXCEED 1% SLOPE. MINIMUM SLOPE AND 1% EXCESS SLOPE SHALL BE MAINTAINED.
4. PAVING SPACES WHERE SHOWN SHALL BE PAVED WITH 4" THICK SURFACE CONCRETE CURBS WHERE SHOWN.
5. REFER TO ARCHITECTURAL SITE PLAN FOR LAYOUT AND DIMENSIONS.

6. ALL SITE WORK TO CONFORM TO THE 2019 OREGON CONSTRUCTION CODE AND THE 2011 OREGON STRUCTURAL CODE.
7. ALL GRASSY AREAS SHALL NOT EXCEED 1% SLOPE. MINIMUM SLOPE AND 1% EXCESS SLOPE SHALL BE MAINTAINED.
8. PAVING SPACES WHERE SHOWN SHALL BE PAVED WITH 4" THICK SURFACE CONCRETE CURBS WHERE SHOWN.

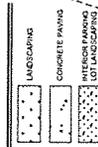
**1 ARCHITECTURAL SITE PLAN**

**SITE PLAN KEY NOTES**

- ① VEHICLE SITE ENTRY WITH PAINTED DIRECTIONAL ARROWS
- ② PEDESTRIAN SITE ACCESS LOCATION
- ③ PAINTED PARKING SPACE STRIPING - TYPICAL WHERE SHOWN
- ④ 6" HIGH CONCRETE CURB
- ⑤ 5" x 1" IF CONCRETE PAD
- ⑥ PAINTED CROSSWALK
- ⑦ VAN ACCESSIBLE PARKING SIGNAGE - 7A101
- ⑧ CURB RAMP 1:12
- ⑨ VAN ACCESSIBLE PARKING SPACE PAVEMENT MARKING - 7A101
- ⑩ ACCESSIBLE PAVEMENT MARKINGS - 7A101
- ⑪ BIKE RACK FOR TWO BIKES 7' X 6' EACH - SEE SA101
- ⑫ TRASH RECYCLE ENCLOSURE
- ⑬ MECHANICAL EQUIPMENT ENCLOSURE
- ⑭ WATER FEATURE
- ⑮ POLE MOUNTED SIGNAGE
- ⑯ 5" x 1" IF CONCRETE PAD
- ⑰ PAINTED CROSSWALK
- ⑱ VAN ACCESSIBLE PARKING SIGNAGE - 7A101
- ⑲ CURB RAMP 1:12
- ⑳ VAN ACCESSIBLE PARKING SPACE PAVEMENT MARKING - 7A101
- ㉑ ACCESSIBLE PAVEMENT MARKINGS - 7A101
- ㉒ BIKE RACK FOR TWO BIKES 7' X 6' EACH - SEE SA101
- ㉓ ROOF OVERHANG
- ㉔ POLE MOUNTED LIGHT FEATURE - SEE SA101
- ㉕ CONCRETE FALLED METAL BOLLARD - SEE WITH OWNER
- ㉖ REQUIRED LOCATIONS
- ㉗ INTERIOR LIGHTING
- ㉘ WALL MOUNTED LIGHT FEATURE - SEE SA101
- ㉙ INTERIOR PARKING LOT LANDSCAPE - 1.178 SF

**PROJECT AREAS**

TOTAL SITE AREA	28,000 SF
BUILDING FOOTPRINT AREA	14,121 SF
LANDSCAPE AREA	1,000 SF
OTHER PAVING AREAS	2,000 SF
INTERIOR PARKING LANDSCAPING	1,178 SF (9.4%)



**TREE PRESERVATION LEGEND**

- TREE PROTECTION FENCING TO BE INSTALLED AT EDGE OF ROOT PROTECTION ZONE OF EACH TREE/GROUP OF TREES
- ⊗ EXISTING CONIFEROUS TREE TO REMAIN
- ⊙ EXISTING DECIDUOUS TREE TO BE REMOVED

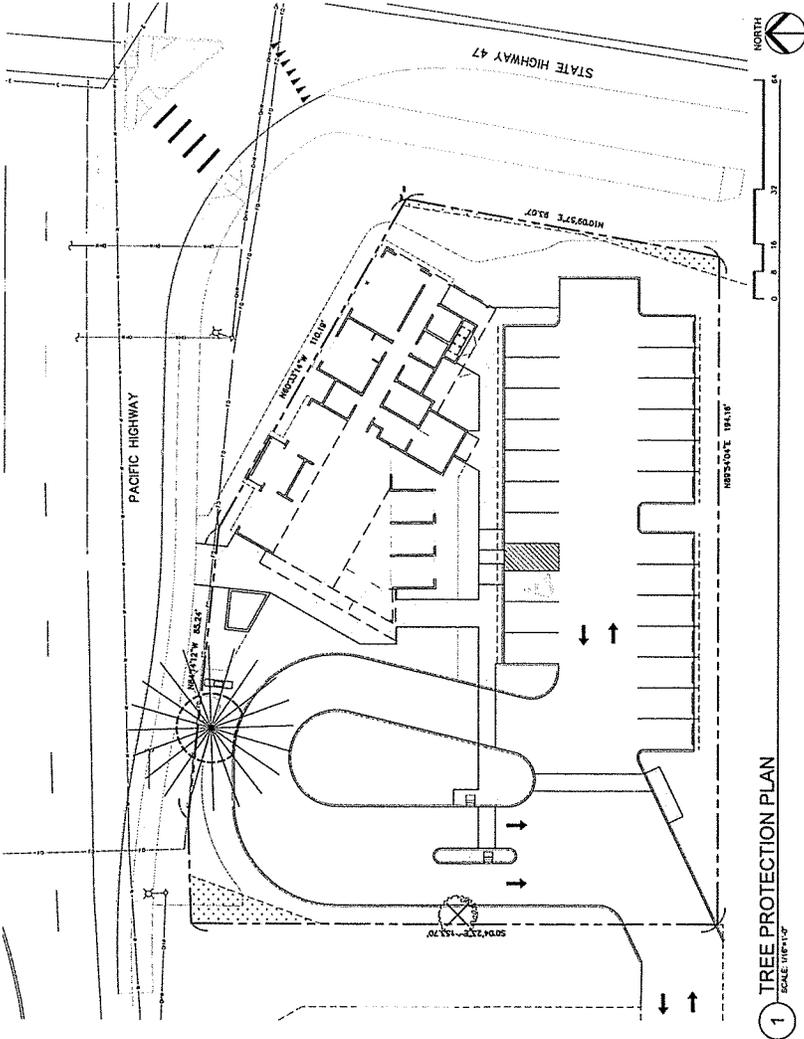
NOTE:  
 ORIGINAL SURVEY AMENDED TO DISTINGUISH BETWEEN TREES TO BE REMOVED AND TREES TO REMAIN

**TREE PRESERVATION NOTES**

1. BEFORE DEVELOPMENT, LAND CLEARING, FILLING OR ANY OPERATION FOR WHICH A TREE REMOVAL PERMIT IS REQUIRED, THE CONTRACTOR:
  - A. SHALL ERECT AND MAINTAIN READILY VISIBLE PROTECTIVE TREE FENCING ALONG THE OUTER EDGE AND COMPLETELY SURROUNDING THE PROTECTED AREA OF ALL PROTECTED TREES. FENCES SHALL BE CONSTRUCTED OF CHAIN LINK AND AT LEAST FOUR FEET HIGH.
  - B. SHALL PROHIBIT EXCAVATION OR COMPACTION OF SOIL OR OTHER ACTIVITIES DAMAGING ACTIVITIES WITHIN THE BARRIERS.
  - C. SHALL MAINTAIN THE PROTECTIVE BARRIERS IN PLACE UNTIL THE PLANNING OFFICIAL AUTHORIZES THEIR REMOVAL OR A FINAL CERTIFICATE OF OCCUPANCY IS ISSUED, WHICHEVER OCCURS FIRST.
  - D. SHALL ENSURE THAT ANY LANDSCAPING DONE IN THE PROTECTED ZONE SUBSEQUENT TO THE REMOVAL OF THE BARRIERS SHALL BE ACCOMPLISHED WITH LIGHT MACHINERY OR HAND LABOR.
2. IN ADDITION TO THE ABOVE, THE PLANNING OFFICIAL MAY REQUIRE THE FOLLOWING:
  - A. COVER WITH MULCH TO A DEPTH OF AT LEAST SIX (6) INCHES OR WITH PLYWOOD OR SIMILAR MATERIAL THE AREAS ADJOINING THE CRITICAL ROOT ZONE OF A TREE IN ORDER TO PROTECT ROOTS FROM DAMAGE CAUSED BY HEAVY EQUIPMENT.
  - B. MINIMIZE ROOT DAMAGE BY EXCAVATING A TWO (2) FOOT DEEP TRENCH AT EDGE OF CRITICAL ROOT ZONE, TO CLEANLY SEVER THE ROOTS OF TREES TO BE RETAINED.
  - C. HAVE CORRECTIVE PRUNING PERFORMED ON PROTECTED TREES IN ORDER TO AVOID DAMAGE FROM MACHINERY OR BUILDING ACTIVITY.
  - D. MAINTAIN TREES THROUGHOUT CONSTRUCTION PERIOD BY WATERING AND FERTILIZING.
3. TREE PRESERVATION SHALL BE IN COMPLIANCE WITH CITY OF FOREST GROVE REGULATIONS
4. SEE CIVIL DRAWINGS FOR GRADING AND EROSION CONTROL.

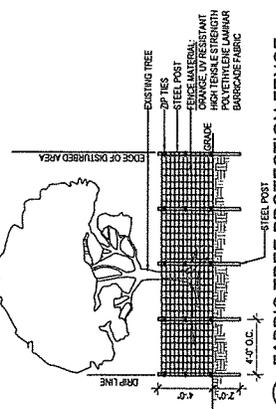
**ONSITE TREE INVENTORY**

TREE #	SPECIES	SIZE	CONDITION	STATUS
#1	FIR	24"	HEALTHY	TO REMAIN
#2	LOCUST	6"	HEALTHY	TO BE REMOVED



1 TREE PROTECTION PLAN  
 SCALE: 1/8"=1'-0"

- NOTES:
- A. SEE TREE PROTECTION NOTE ON LOT 1 FOR MORE DETAIL.
  - B. SEE TREE PROTECTION PLAN FOR LOCATIONS OF TREE PROTECTION FENCE.



2 FABRIC TREE PROTECTION FENCE  
 SCALE: NTS



**PLANT LEGEND**

EVERGREEN TREES

EVERGREEN TREES	NAME	QTY.	SIZE	TYPE
	CHAMAECYPARIS HOOTKAIENSIS Araska Cedar	5	8-9' HT.	BBB
	THUJA PLICATA 'ATROVIRENS' Red Cedar 'Atrovirens'	3	7-8' HT.	BBB
	TSUGA MERTENSIANA Mountain Hemlock	2	6-7' HT.	BBB

DECIDUOUS TREES

DECIDUOUS TREES	NAME	QTY.	SIZE	TYPE
	ACER PALMATUM 'SANGO NAKU' Sango kaku Japanese Maple	1	2" CAL.	BBB
	ACER RUBRUM 'FRANK J.' PP Reppert Maple	5	2" CAL.	BBB
	CERCIDIPHYLLUM JAPONICUM Jubilee	3	8-10' HT.	1 TRUNK BBB
	CORNUS X RUTGAN Chinese Dogwood	3	2" CAL.	BBB
	SYCAMORE TRICHANTHOS 'SICOYLE'	5	2" CAL.	BBB
	QUERCUS BICOLOR 'JFS-KW17 PP2832' American Drac Oak	1	2" CAL.	BBB

EVERGREEN SHRUBS

EVERGREEN SHRUBS	NAME	QTY.	SIZE	TYPE
	ILEX CRENATA 'CONVEXA' Japanese Holly	35	15-16"	CONTAINER
	LONICERA PILEATA Privet Honey-suckle	60	2 GAL.	CONTAINER
	OSMANTHUS HETEROPHYLLUS 'GOSHIKI' Goshiki Variegated Falas Holly	28	24-30"	CONTAINER
	PIERIS JAPONICA Japanese Andromeda	10	24-30"	CONTAINER
	POLYSTICHUM MUNITUM Western Sword Fern	17	2 GAL.	CONTAINER
	SPIRAEA X BUMALDA 'GOLD FLAME' Gold Flame Spirea	24	2 GAL.	CONTAINER
	VIBURNUM TINUS 'COMPACTUM' Viburnum	45	7 GAL.	CONTAINER
	VIBURNUM P. P. G. TAM. F. TOMENTOSUM 'MARIESII' Doxbush Viburnum	3	45" HT.	CONTAINER

DECIDUOUS SHRUBS

DECIDUOUS SHRUBS	NAME	QTY.	SIZE	TYPE
	ACER CIRCINATUM Vine Maple	5	7-8' HT.	3 TRUNK BBH
	BERBERIS THUNBERGII 'ATROPURPUREA' Japanese Red Barberry	24	5 GAL.	CONTAINER
	CORNUS SERICEA 'KELSEY' Kelsey Red Osier Dogwood	48	2 GAL.	CONTAINER
	ROSA X RADCOY 'PP #15,070' Pink Knock Out Rose	17	2 GAL.	CONTAINER

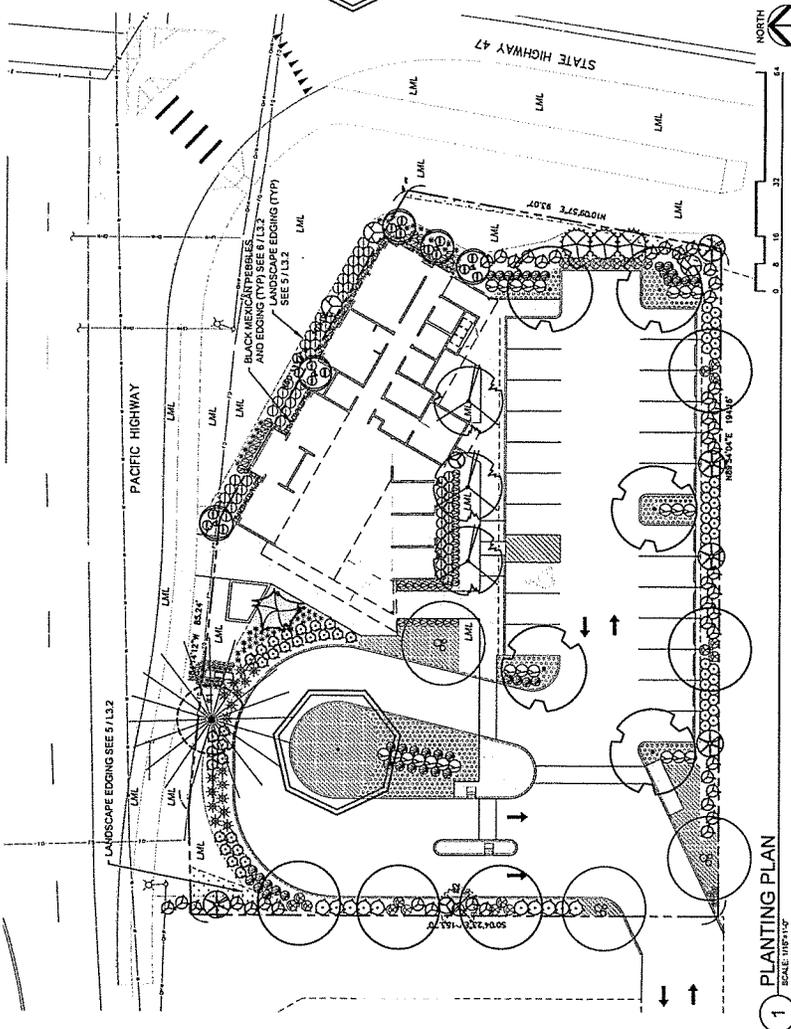
GROUND COVERS & PERENNIALS

GROUND COVERS & PERENNIALS	NAME	QTY.	SIZE	SPACING
	ARCHOSTAPHYLOS UVA URSI Horseshoe	XX	1 GAL.	24" O.C.
	HELICTOTRICHON SEMPERVIRENS Blue Grass	XX	1 GAL.	18" O.C.
	HELIOSCILLA STELLA 'DE ORO' Deli Lily	21	1 GAL.	AS SHOWN.
	MEGACALLIS 18-547 CBRS 4,873 Fusch Dill Rose	XX	1 GAL.	24" O.C.
	OPHIOPOGON PLANUS 'CAPENSIS' Black Mondo Grass	XX	1 GAL.	18" O.C.

GRASSES AND VINES

GRASSES AND VINES	NAME	QTY.	SIZE	TYPE
	CALAMAGROSTIS ACUTIFOLIA 'MARL FOENSTER' Marl Foenster Feather Reed Grass	39	3 GAL.	AS SHOWN
	CAREX MORROWII 'AUREA VARIEGATA' Variegata Sedge	58	2 GAL.	CONTAINER

LML LOW MAINTENANCE LAWN SEED -XX' at @ 8 lbs. per 1,000sqf  
-DOT Mulch/soil turf by Sunmark Seeds International or approved



1 PLANTING PLAN  
SCALE: 1/8"=1'-0"

**LANDSCAPE NOTES**

- LANDSCAPE CONTRACTOR SHALL SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- ALL LANDSCAPING SHALL BE INSTALLED ACCORDING TO ACCEPTED BEST INDUSTRY STANDARDS FOR PLANTING PROCEDURES.
- PLANT MATERIALS SHALL BE OF HIGH GRADE, AND SHALL MEET THE QUALITY AND SIZE STANDARDS FOR NURSERY STOCK.
- PLANTINGS SHALL BE MULCHED TO A MINIMUM DEPTH OF 2 INCHES AND 18 INCHES IN DIAMETER.
- PLANTS INDICATED AS SPECIMEN ARE TO BE HAND SELECTED BY LANDSCAPE ARCHITECT.
- LANDSCAPE SHALL BE IN COMPLIANCE WITH THE CITY OF FOREST GROVE. PLANT QUANTITIES MUST BE ACCURATELY REFLECTED ON THE PLAN AND PLANT LEGEND TO DEMONSTRATE COMPLIANCE SPECIFICATIONS.
- LANDSCAPE SHALL COMPLY WITH CITY OF FOREST GROVE ARTICLE 8 AT TIME OF BUILDING PERMIT.

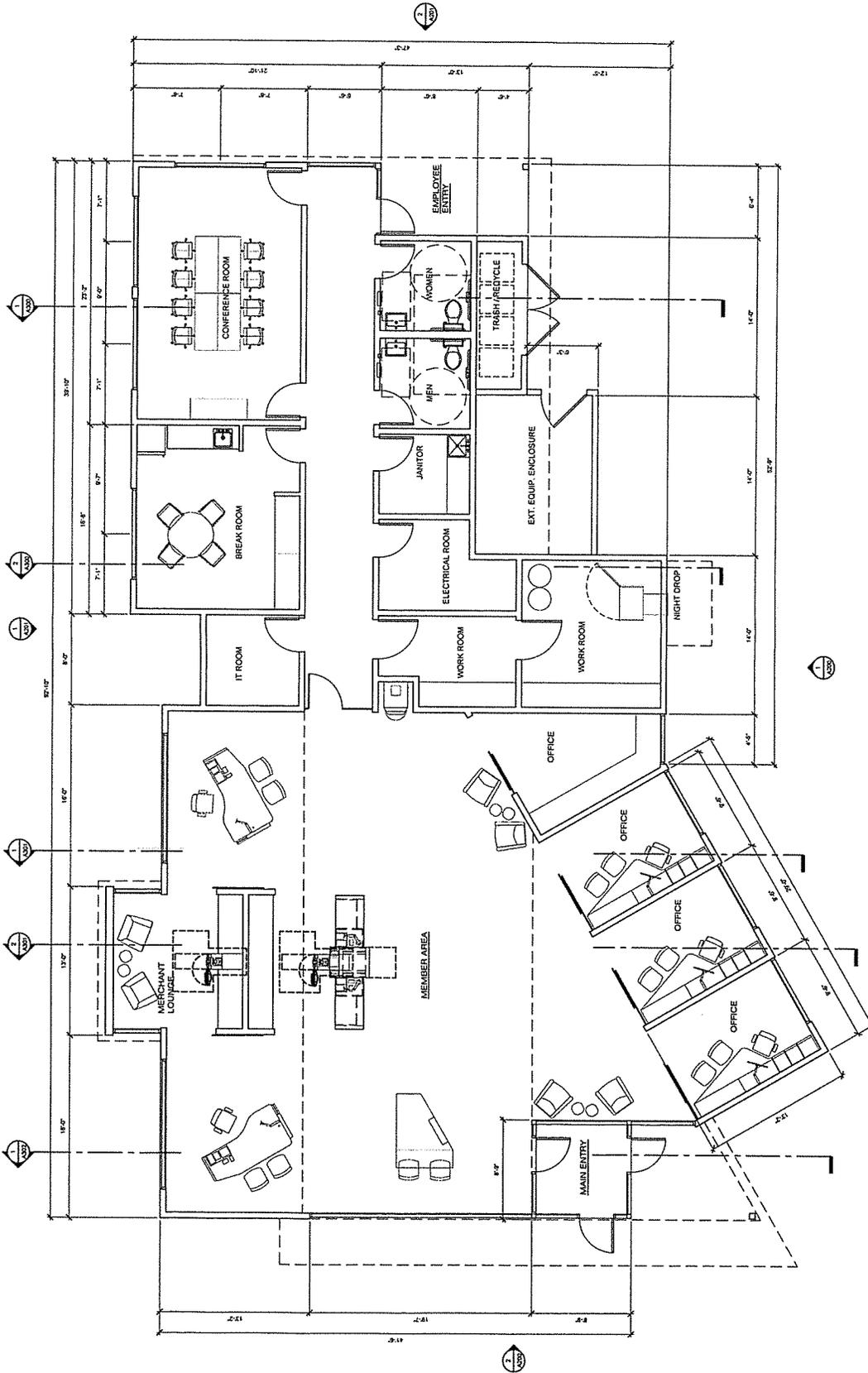


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 Doug Orosco, Architect  
 14670 SW Forest Drive  
 Beaverton, OR 97007

NEW BRANCH FACILITY  
 WAUNA CREDIT UNION  
 FOREST GROVE, OREGON 97006

1711  
 0804 11-16-2018  
 FLOOR PLAN

A110



1 FIRST FLOOR PLAN - 3,773 SF  
 SCALE: 1/4" = 1'-0"

A110



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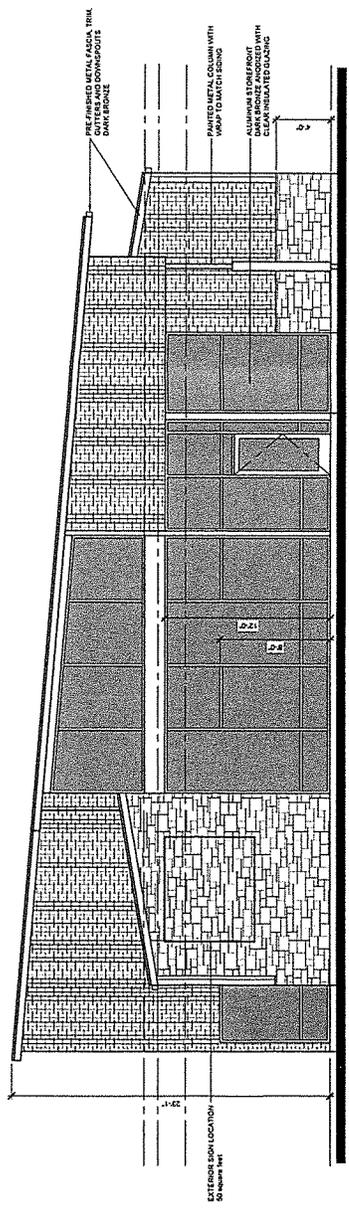
NEW BRANCH FACILITY  
 WAUNA CREDIT UNION  
 FOREST GROVE, OREGON 97006

JOB NO.: 1711  
 DATE: 11-16-2018  
 BUILDING  
 ELEVATIONS

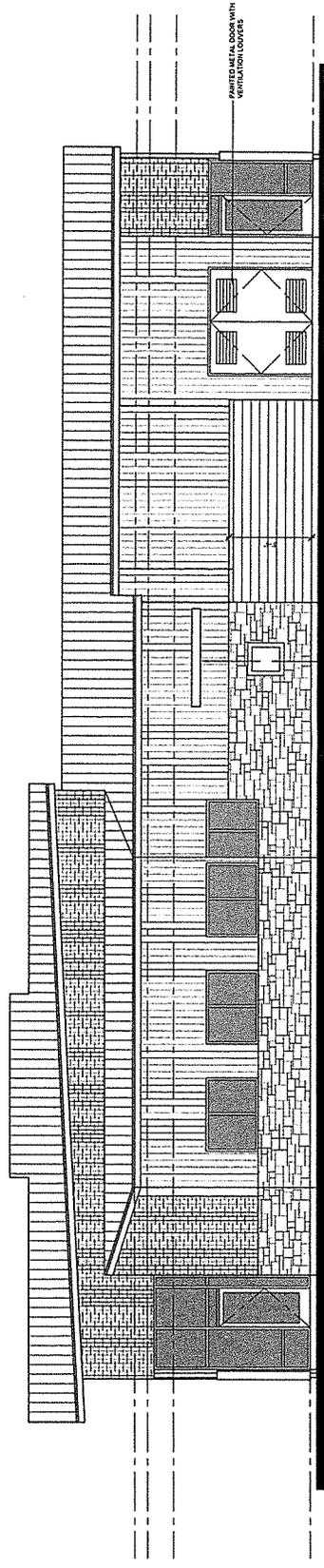
A200

MATERIAL SYMBOLS

- METAL ROOFING  
 TAYLOR METALS M150  
 DARK BRONZE
- METAL SIDING  
 TAYLOR METALS EASY-LOCK  
 DARK BRONZE
- COMPOSITE WOOD SIDING  
 NEW TECH WOOD - ALL WEATHER  
 PERUVIAN TEAK (TK)
- CULTURED STONE VENEER  
 TERRAZZO CHARDONNAY  
 LIMESTONE
- 8" STANDARD CMU  
 PAINTED - COLOR TO MATCH  
 METAL SIDING



2 WEST ELEVATION  
 SCALE: 1/4" = 1'-0"



1 SOUTH ELEVATION  
 SCALE: 1/4" = 1'-0"



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 Beaverton, OR 97007

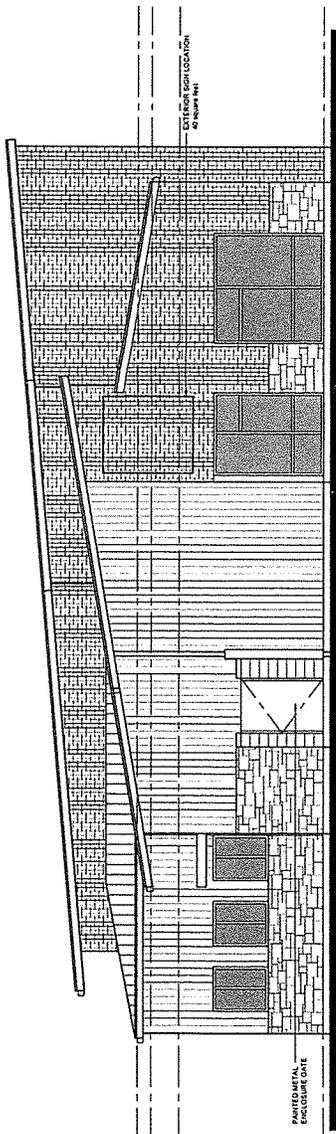
FOREST GROVE, OREGON 97006

NEW BRANCH FACILITY  
 WAUNA CREDIT UNION

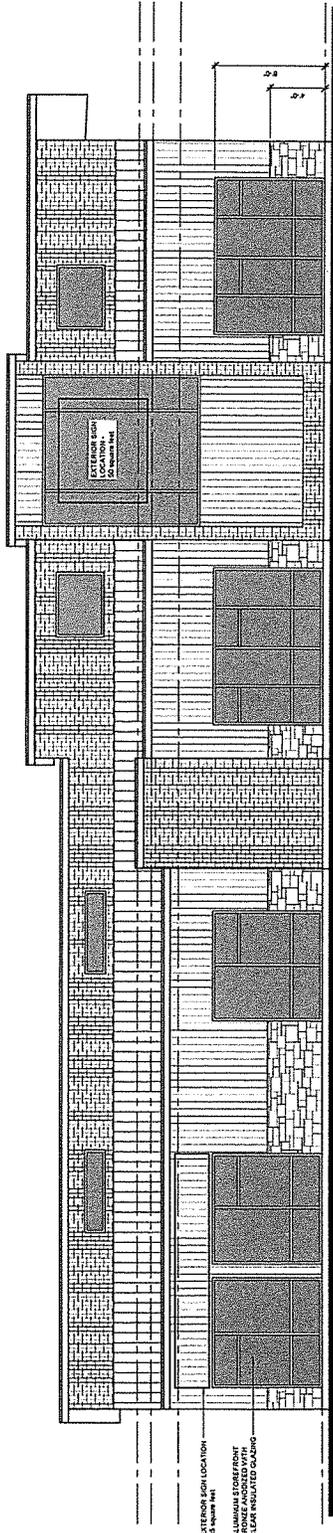
job no.: 1711  
 date: 11-16-2018

BUILDING  
 ELEVATIONS

A201



2 EAST ELEVATION  
 SCALE: 1/4" = 1'-0"



1 NORTH ELEVATION  
 SCALE: 1/4" = 1'-0"

EXTERIOR SIGN LOCATION  
 42' height  
 ALUMINUM STOREFRONT  
 CLEAR INSULATED GLAZING

# EXHIBIT B

ODOT Comments



# Oregon

Kate Brown, Governor

## Department of Transportation

Region 1 Headquarters  
123 NW Flanders Street  
Portland, Oregon 97209  
(503) 731.8200  
FAX (503) 731.8259

December 27<sup>st</sup>, 2018

ODOT #7244

## ODOT Response

<b>Project Name:</b> Wauna Credit Union	<b>State Highway:</b> OR 47
<b>Jurisdiction:</b> City of Forest Grove	<b>Jurisdiction Case #:</b> 311-18-000039-PLNG
<b>Site Address:</b> 3532 Pacific Av (OR08), Forest Grove, OR	<b>Legal Description:</b> 01S 03W 05BA <b>Tax Lot(s):</b> 00100

The site of this proposed land use action is adjacent to OR 47. ODOT has permitting authority for this facility and an interest in ensuring that this proposed land use is compatible with its safe and efficient operation. **Please direct the applicant to the Development Review Coordinator Contact indicated below to determine permit requirements and obtain application information.**

### COMMENTS/FINDINGS

Based on the geometry at the intersection, a right turn lane is needed on Pacific Ave. We recommend that the City condition the applicant to dedicate additional right of way as needed for the right turn lane and that the sidewalk be placed back in its ultimate location. Since the right of way varies the amount of right of way needed should be measured from the middle of the median or about 60.5ft with a 6ft sidewalk without the planter strip. If the City requires a planter strip the additional feet should be added on (see attached email message for details).

The existing bike/pedestrian path along the property's OR 47 frontage is in poor condition, we recommend the applicant be conditioned to repave the 10ft path and construct an ADA ramp at the crosswalk.

All alterations within the State highway right of way are subject to the ODOT Highway Design Manual (HDM) standards. Alterations along the State highway but outside of ODOT right-of-way may also be subject to ODOT review pending its potential impact to safe operation of the highway. If proposed alterations deviate from ODOT standards a Design Exception Request must be prepared by a licensed engineer for review by ODOT Technical Services. Preparation of a Design Exception request does not guarantee its ultimate approval. Until more detailed plans have been reviewed, ODOT cannot make a determination whether design elements will require a Design Exception.

Note: Design Exception Requests may take up to 3 months to process.

All ODOT permits and approvals must reach 100% plans before the District Contact will sign-off on a local jurisdiction building permit, or other necessary requirement prior to construction.

### ODOT RECOMMENDED LOCAL CONDITIONS OF APPROVAL

#### Frontage Improvements and Right of Way

- The applicant shall pave the 10ft bike/pedestrian path along the property's OR 47 frontage and put in a cross walk ramp at the intersection to be consistent with local, ODOT and ADA standards.

- Right of way dedicated to the City of Forest Grove as necessary to accommodate a right turn lane on Pacific Ave..

**Please send a copy of the Notice of Decision including conditions of approval to:**

ODOT Region 1 Planning  
Development Review  
123 NW Flanders St  
Portland, OR 97209

[Region1\\_DEVREV\\_Applications@odot.state.or.us](mailto:Region1_DEVREV_Applications@odot.state.or.us)

Development Review Planner: Marah Danielson	503.731.8258, marah.b.danielson@odot.state.or.us
Traffic Contact: Avi Tayar, P.E.	503.731.8221
Development Review Coordinator: David Smith	503.986.2849 David.R.SMITH@odot.state.or.us

## James Reitz

---

**From:** LAM Canh T  
**Sent:** Monday, December 24, 2018 5:08 PM  
**To:** DANIELSON Marah B  
**Subject:** 3532 Pacific Ave - OR8/OR47  
**Attachments:** RW\_figure.docx

Please take a look at the attached figure. Since the RW line is not straight and we do not have records for Pacific Avenue we would have to do some estimation from the "assumed RW" centerline (middle of the 16-foot median). Based on the records from different projects, Pacific Ave existing cross-section would be:

- 12-foot lanes: 4
- 16-foot median: 1
- 6-foot bike lanes: 2

Since the RW is varied we could state as the proposed RW measured from the middle of the median should be about 60.5 ft (if only 1 foot behind walk) to 61 ft (if 1.5 feet behind walk). This dimension would include the following:

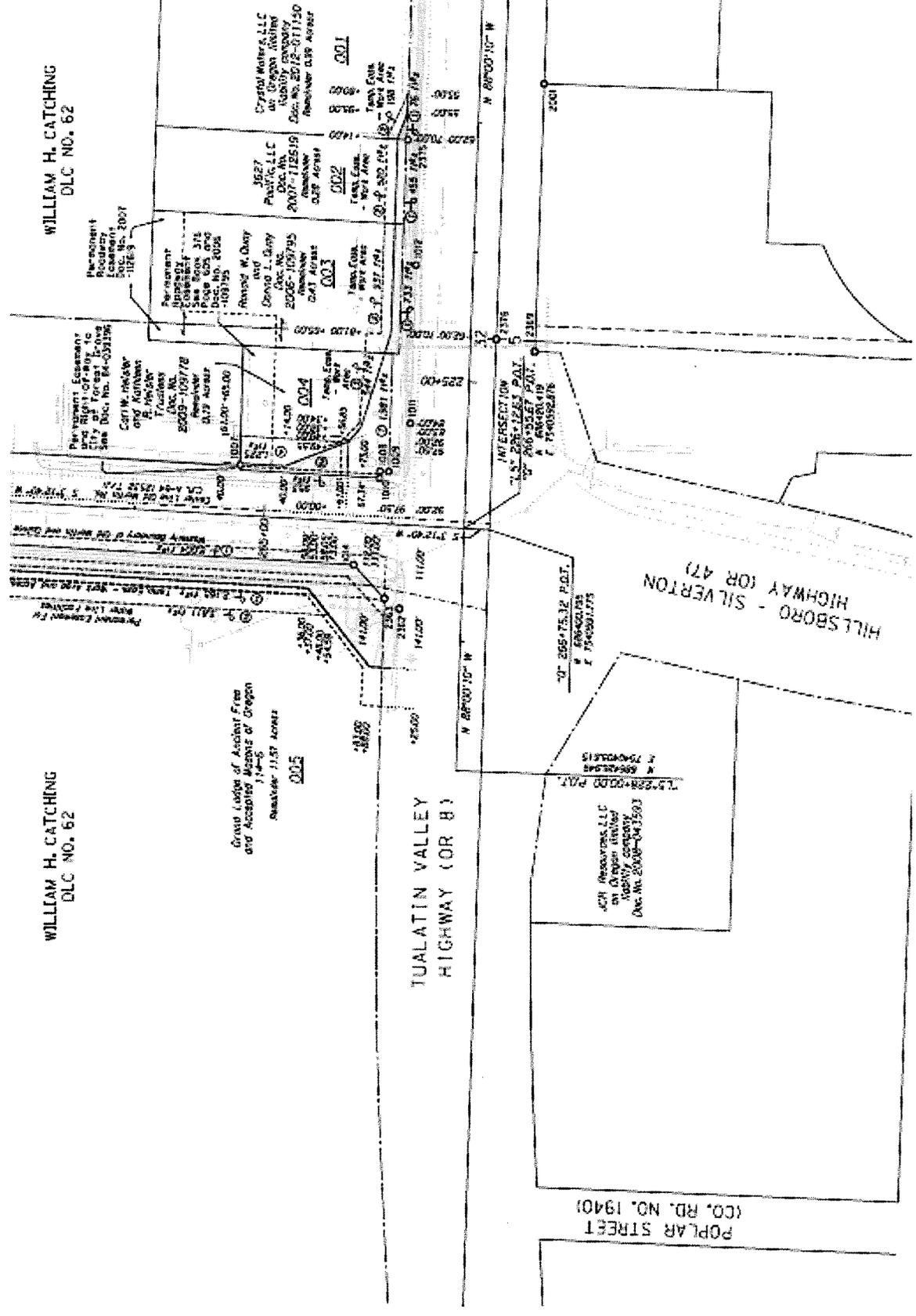
- $\frac{1}{2}$  of the 16-ft median = 8 ft
- 2 12-foot lanes = 24 ft
- 6-foot bike lane = 6 ft
- 15-foot right turn lane = 15 ft PLUS the turning radius
- 0.5-foot curb = 0.5 ft
- 6-foot sidewalk = 6 ft
- 1 foot or 1.5 feet behind walk.
- If planter strip is required additional 4 feet - 8 feet would need to be included.

Hope this helps.

Thanks,  
ctl

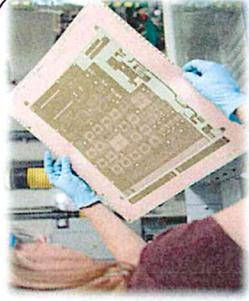
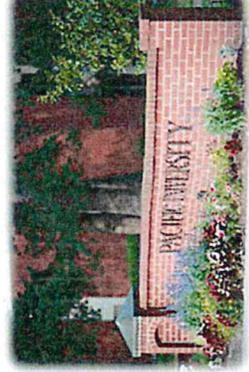
WILLIAM H. CATCHING  
DLC NO. 62

WILLIAM H. CATCHING  
DLC NO. 62



# EXHIBIT C

PowerPoint Slides



# Planning Commission Meeting March 4, 2019

WAUNA CREDIT UNION

3532 Pacific Avenue

SITE & DESIGN REVIEW

James Reitz (AICP), Senior Planner

*A place where families and businesses thrive.*

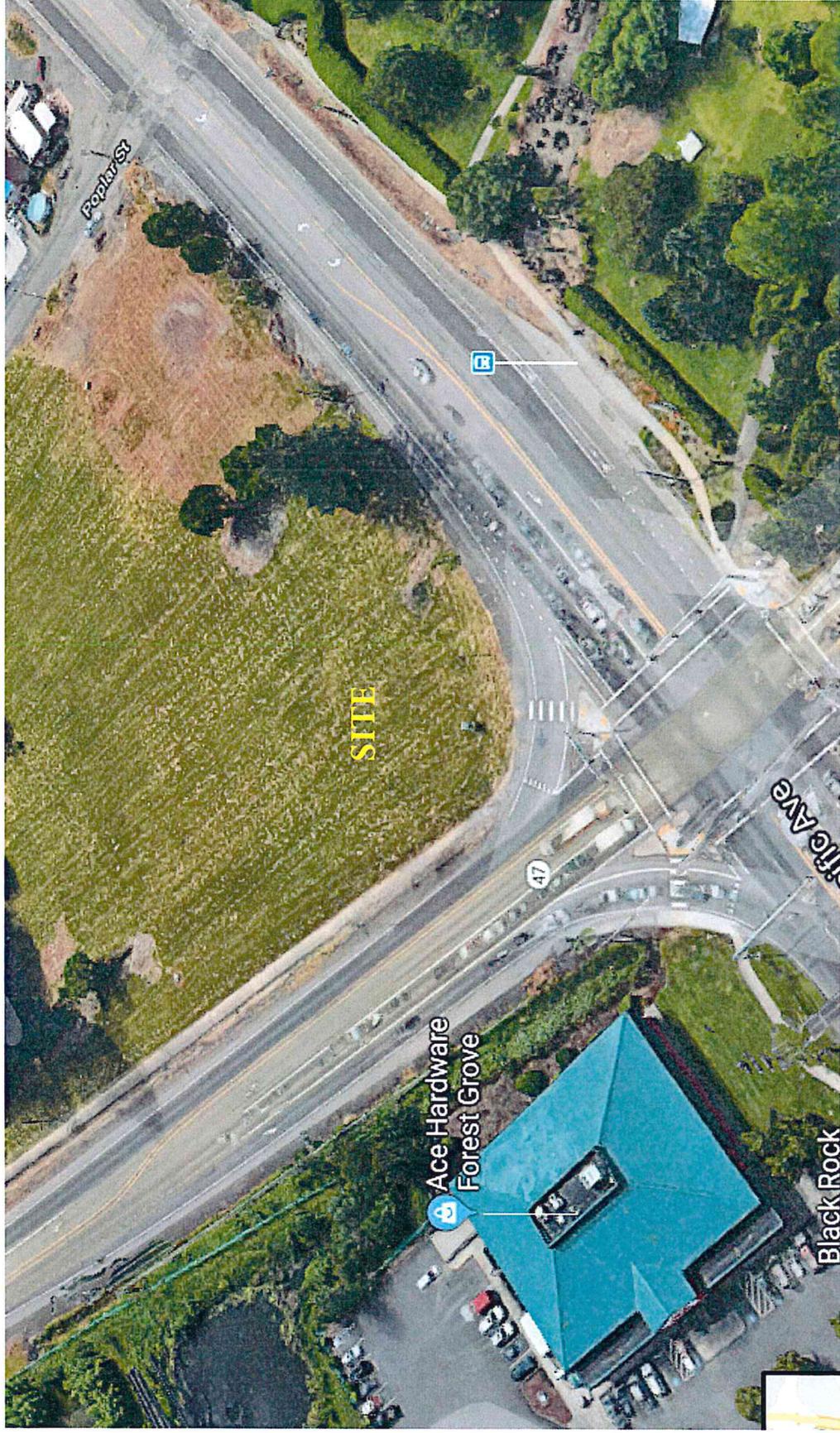


# Aerial View - Site & Neighborhood



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# Oblique View



A place where families and businesses thrive.



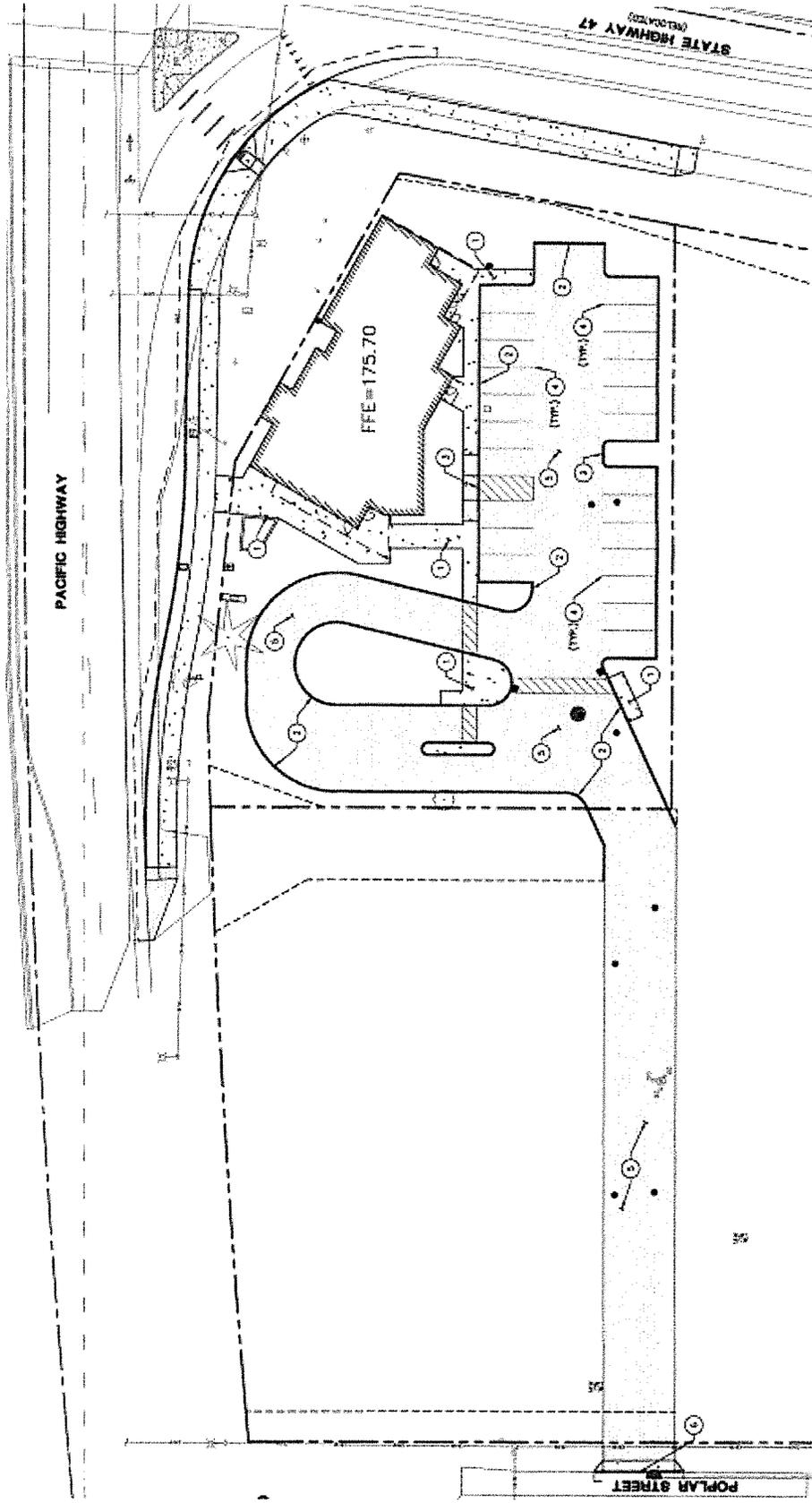
# Street View - Looking West



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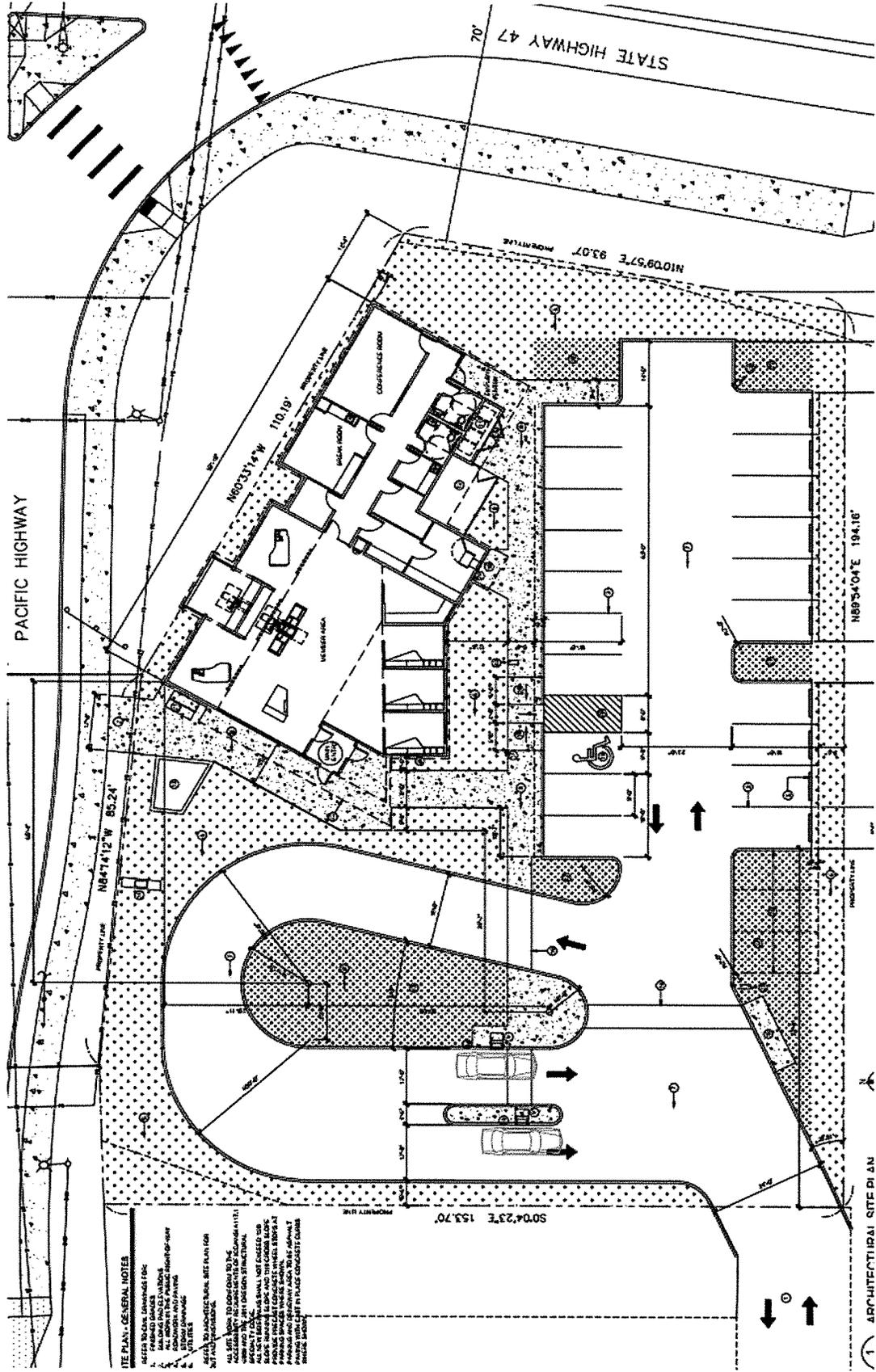
# Site Plan

Driveway to Poplar Street



A place where families and businesses thrive.

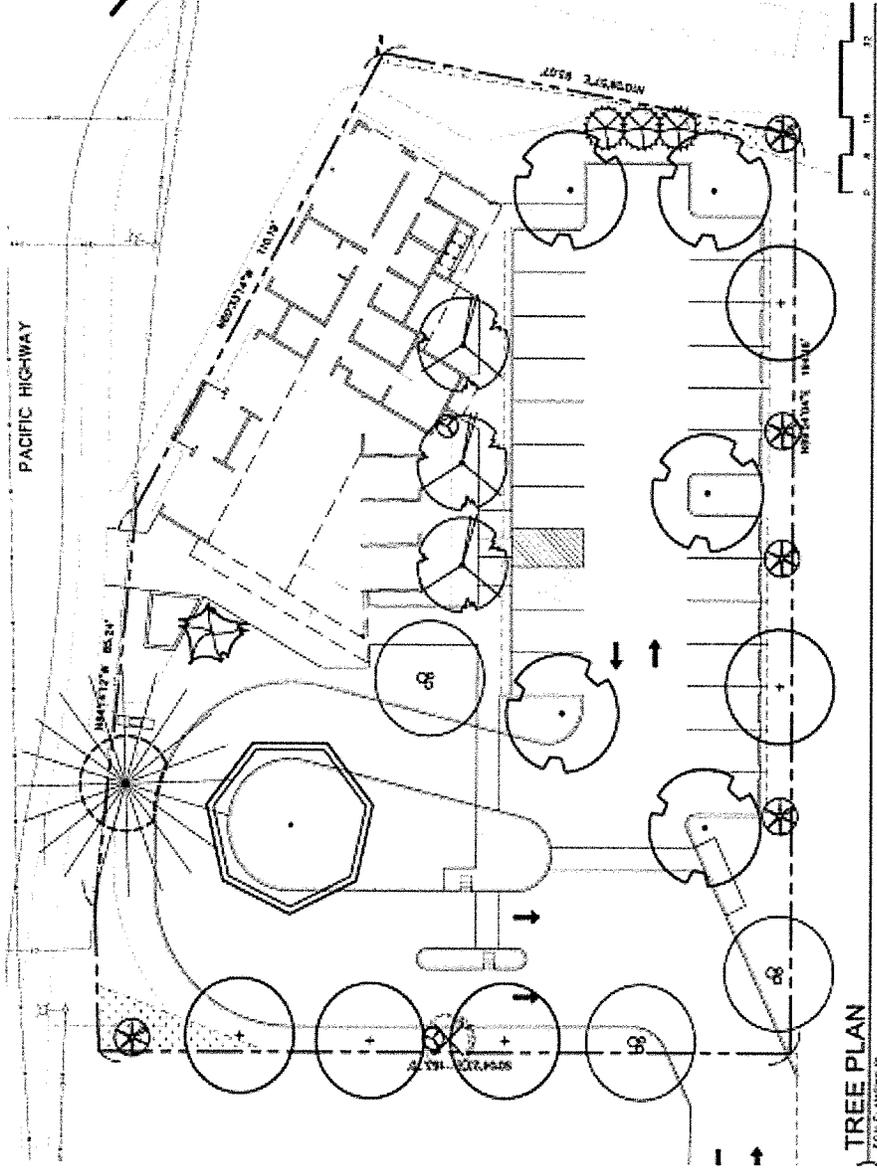
# Detailed Site Plan



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# Tree Plan



## TREE LEGEND

### EVEREGREEN TREES NAME

-  **CHAMACYPARIS MOOTKATENSIS**  
Alaska Cedar
-  **THUJA PLICATA 'ATROVIRENS'**  
Rad Cedar 'Atrovirens'
-  **TSUGA MERTENSIANA**  
Mountain Hemlock

### DECIDUOUS TREES

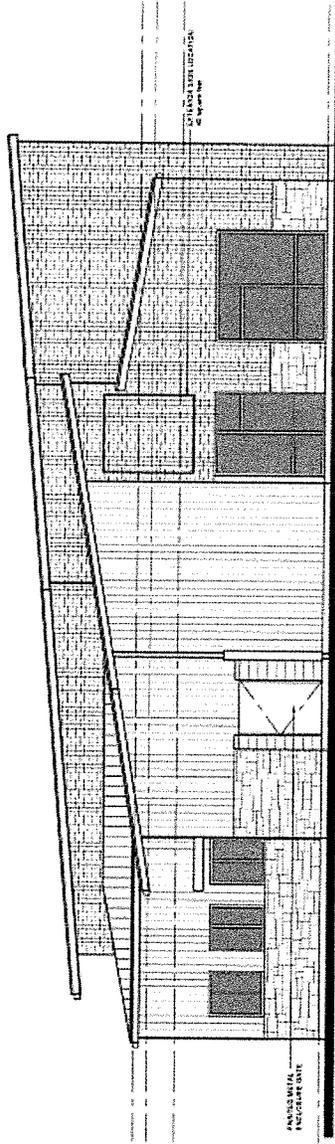
-  **ACER PALMATUM SANGO KAKU'**  
Sango Kaku Japanese Maple
-  **ACER RUBRUM 'FRANK JR. PP'**  
Redpoint Maple
-  **CERCIDIPHYLLUM JAPONICUM**  
Katsura
-  **CORNUS X 'RUTGAN'**  
Celestial Dogwood
-  **GLEDITSIA TRIACHANTHOS 'SKYCOLE'**  
Skyline® Honeylocust
-  **QUERCUS BICOLOR 'JFS-KW12' PP23632**  
American Dream Oak

TREE PLAN  
SCALE: 1/8"=1'-0"

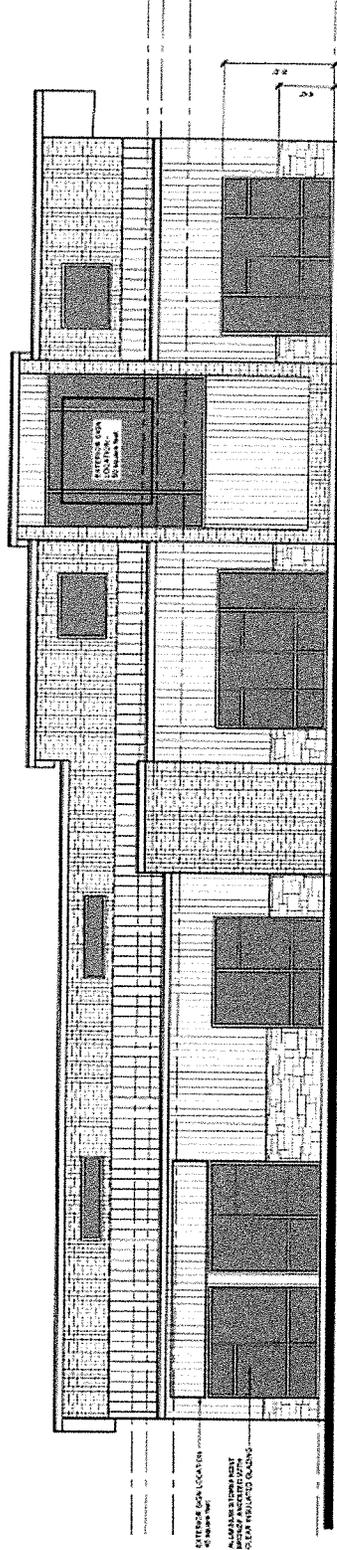
A place where families and businesses thrive.



# East & North Building Elevations



2 EAST ELEVATION  
SCALE 1/4" = 1'-0"



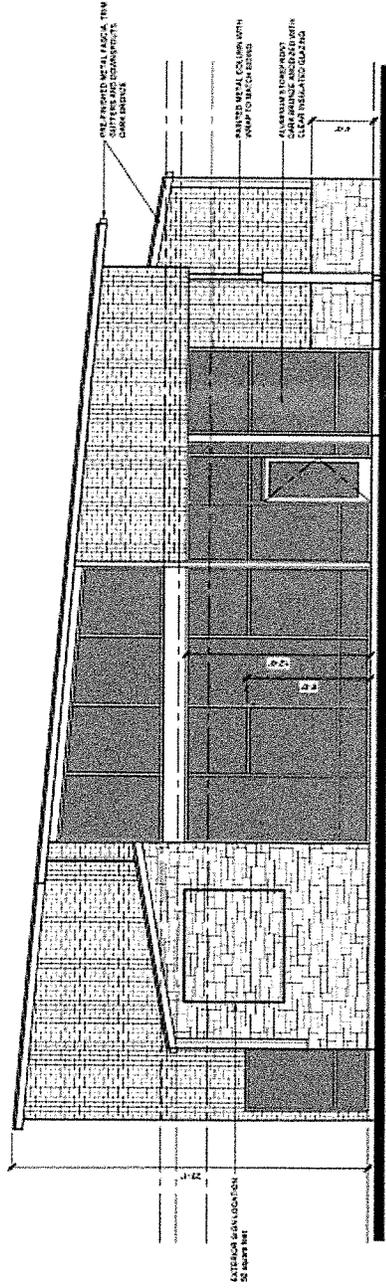
1 NORTH ELEVATION  
SCALE 1/4" = 1'-0"

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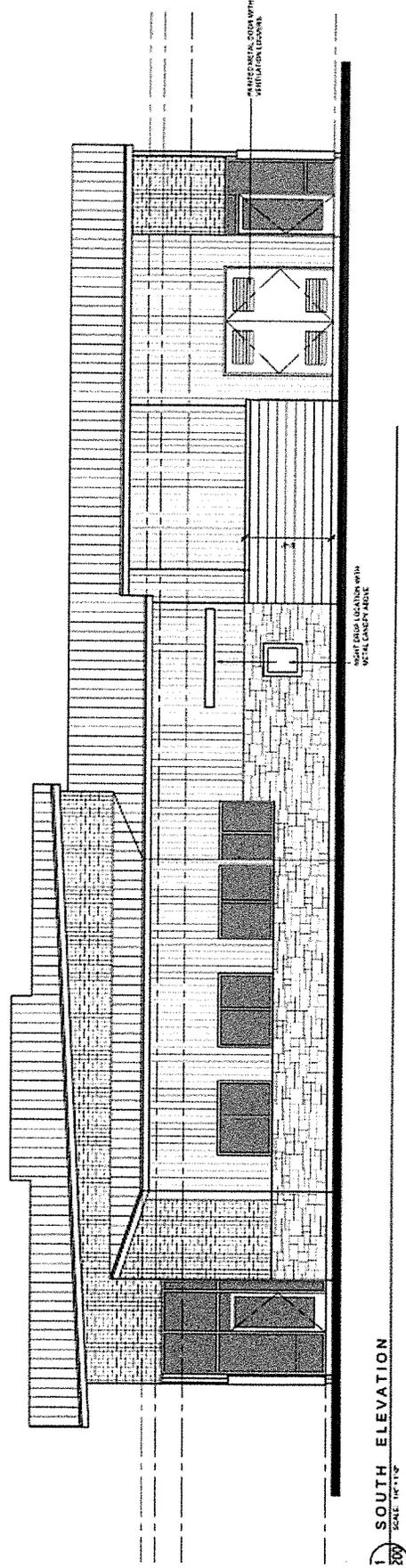
# West & South Building Elevations

**MATERIAL SYMBOLS**

	<b>METAL ROOFING</b> TAYLOR METALS M8150 DARK BRONZE
	<b>METAL SIDING</b> TAYLOR METALS EASY-LOCK DARK BRONZE
	<b>COMPOSITE WOOD SIDING</b> NEW TECH WOOD - ALL WEATHER PERCUTAN T66(14)
	<b>CULTURED STONE VENEER</b> QUINCY CORNING CHARDONWAY LIMESTONE
	<b>PAINTED COLOR TO MATCH</b> METAL SIDING



**WEST ELEVATION**  
SCALE 1/8" = 1'-0"



**SOUTH ELEVATION**  
SCALE 1/8" = 1'-0"

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# North & East Building Elevations



A place where families and businesses thrive.



# West & South Building Elevations



A place where families and businesses thrive.



# South & East Building Elevations



A place where families and businesses thrive.



# North & West Building Elevations



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## Approval Conditions

- Dedicate as needed right-of-way along Pacific Avenue to provide for a right-turn lane to Highway 47.
- Construct a right-turn lane along Pacific Avenue to Highway 47.
- Improve the Pacific Avenue frontage including curbs, gutters, sidewalks, and landscaping.
- Improve the Highway 47 frontage.
- Except where required by other codes (e.g. ADA), reduce the parking stall lengths to 16 ½ feet, provided the adjoining walkways and landscape areas are commensurately enlarged.
- Install a physical barrier on both sides of the access driveway.